

SITE SPECIFIC TRAFFIC ANALYSIS

POMPEY PARK
DELRAY BEACH, FL

PREPARED FOR:
CITY OF DELRAY BEACH

Kimley»»Horn

November 19, 2021
CA 00000696
Kimley-Horn and Associates, Inc.
1920 Wekiva Way
West Palm Beach, Florida 33411
561/845-0665 TEL

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Prepared by:
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Introduction

The Pompey Park site is bordered by NW 2nd Street, NW 10th Avenue, NW 3rd Street, and NW 13th Avenue in Delray Beach, Florida. The existing park is proposed to be redeveloped and will include the expansion of the existing recreation center building. *Figure 1* illustrates the location of the project site. The Parcel Control Numbers (PCNs) for the project site are 12-43-46-17-00-000-3140, 12-43-46-17-00-000-3160, and 12-43-46-17-00-000-3130. The proposed site plan and PCN summary are included in *Appendix A*.

The purpose of this study is to evaluate the impacts of the proposed conversion on the surrounding local City roads and intersection. This site-specific analysis was conducted based on feedback from the City of Delray Beach. This report summarizes the project trip generation, project trip distribution, local roadway analysis, and local intersection analysis.



LEGEND
■ Site Location

FIGURE 1
SITE LOCATION
POMPEY PARK

Inventory and Planning Data

Weekday turning movement counts used in this analysis were conducted during the AM peak hour (7-9am) and PM peak hour (4-6pm) at the following intersections:

- NW 2nd Street & NW 12th Avenue
- NW 2nd Street & NW 10th Avenue
- NW 3rd Street & NW 13th Avenue

Project Traffic

Project traffic used in this analysis is defined as the vehicle trips expected to be generated by the project and the distribution and assignment of that traffic over the study roadway network.

Existing and Proposed Land Uses

As part of the proposed park project, the recreation center building is proposed to be expanded from 21,817 SF to 53,984 SF. No expansion to the existing park acreage is proposed.

Trip Generation

The trip generation potential for the proposed development program was calculated based on rates and equations published by the *Institute of Transportation Engineers (ITE)* in *Trip Generation, 10th Edition*. As shown in *Table 1*, the proposed redevelopment is projected to generate an increase of 927 net new external daily trips, an increase of 57 net new external AM peak-hour trips (38 in, 19 out), and an increase of 75 net new external PM peak-hour trips (35 in, 40 out).

Table 1: Trip Generation

Land Use	Intensity	Daily Trips	AM Peak Hour			PM Peak Hour		
			Total	In	Out	Total	In	Out
Existing Scenario >5 Years								
Recreation Center	21.817 SF	629	38	25	13	50	24	26
	<i>Subtotal</i>	629	38	25	13	50	24	26
Pass-By Capture Recreation Center	0.0%	0	0	0	0	0	0	0
	<i>Subtotal</i>	0	0	0	0	0	0	0
Driveway Volumes		629	38	25	13	50	24	26
Net New External Trips		629	38	25	13	50	24	26
Proposed Scenario								
Recreation Center	53.984 SF	1556	95	63	32	125	59	66
	<i>Subtotal</i>	1556	95	63	32	125	59	66
Pass-By Capture Recreation Center	0.0%	0	0	0	0	0	0	0
	<i>Subtotal</i>	0	0	0	0	0	0	0
Driveway Volumes		1556	95	63	32	125	59	66
Net New External Trips		1556	95	63	32	125	59	66
Net New Driveway Volumes		927	57	38	19	75	35	40
Radius of Development Influence:		1 miles						
<u>Land Use</u>	<u>Daily</u>	<u>AM Peak Hour</u>			<u>PM Peak Hour</u>		<u>Pass-By</u>	
Recreation Center	28.82 trips / 1,000 SF	1.76 trips / 1,000 SF			2.31 trips / 1,000 SF		0.0%	

Traffic Distribution

Traffic distribution is the pairing of trip ends from the subject site with other land uses in the area. These trips were assigned to the surrounding roadways based upon a review of the roadway network proposed to be in place at the time of buildout and its travel time characteristics.

The distribution within the project radius is:

NORTH	-	25 percent
SOUTH	-	15 percent
EAST	-	40 percent
WEST	-	20 percent

Traffic Assignment

The site traffic was assigned to the surrounding roadway network based upon existing travel patterns and the traffic distribution. *Figure 2* illustrates the roadway link assignment. The AM and PM peak hour trips for the project were then assigned to the surrounding roadway network projected to be in place by 2026.



LEGEND
 Site Location
 XX% Trip Assignment

FIGURE 2
 TRIP DISTRIBUTION
 POMPEY PARK
Kimley»Horn

Local Roadway and Intersection Analysis

Capacity analyses were conducted for the directly accessed links and intersections to identify the capacity remaining upon buildout of the project. The turning movement counts conducted in the project vicinity were utilized to determine the existing traffic volumes on the local roadway links and intersections before applying a peak season correction factor (PSCF) to account for seasonal variation in traffic. Net new external project traffic was assigned to the directly accessed roadway links and intersections per direction from the City of Delray Beach. The intersection capacity analysis was conducted using *HCS 7* software.

Table 2 summarizes the roadway link capacity analysis. As shown in *Table 2*, none of the roadway segments are projected to be over capacity upon buildout of the project site. Therefore, no further analysis is required.

Table 3 summarizes the results of the intersection capacity analysis. As shown in *Table 3*, the surrounding local intersections are projected to operate acceptably during the AM and PM peak hours.

Table 2: Local Roadway Capacity Analysis

Roadway	From		To		Existing		Direction	Year 2021 Traffic Volume	Committed Traffic - Option #1		2026 Total Traffic	Meets Standard ??	
	Lanes	Facility Type	LOS D Service Volume	Lanes	Facility Type	Peak Season Adjusted Volumes			1.0% Traffic Growth	Peak Season plus 1.0%			Project Traffic
AM Peak Hour													
NW 2nd Street	NW 13th Ave	NW 12th Ave	2L	Class II	497	NB/EB	95	103	5	108	2	110	Yes
NW 2nd Street	NW 12th Ave	NW 10th Ave	2L	Class II	497	SB/AWB	71	77	4	81	4	85	Yes
NW 12th Ave	Atlantic Ave	NW 2nd Street	2L	Class II	497	NB/EB	78	84	4	88	11	99	Yes
NW 10th Ave	Atlantic Ave	NW 2nd Street	2L	Class II	497	SB/AWB	116	125	6	131	6	137	Yes
NW 10th Ave	Atlantic Ave	NW 2nd Street	2L	Class II	497	NB/EB	110	119	6	125	15	140	Yes
NW 10th Ave	Atlantic Ave	NW 2nd Street	2L	Class II	497	SB/AWB	172	186	9	195	8	203	Yes
NW 10th Ave	NW 2nd Street	NW 3rd Street	2L	Class II	497	NB/EB	64	69	4	73	8	81	Yes
NW 3rd Street	NW 13th Ave	NW 10th Ave	2L	Class II	497	SB/AWB	57	62	3	65	4	69	Yes
NW 3rd Street	NW 2nd Street	NW 3rd Street	2L	Class II	497	NB/EB	121	131	7	138	4	142	Yes
NW 13th Ave	NW 2nd Street	NW 3rd Street	2L	Class II	497	SB/AWB	106	114	6	120	2	122	Yes
NW 13th Ave	NW 2nd Street	NW 3rd Street	2L	Class II	497	NB/EB	41	44	2	46	4	50	Yes
NW 13th Ave	NW 2nd Street	NW 3rd Street	2L	Class II	497	SB/AWB	93	100	5	105	8	113	Yes
NW 13th Ave	NW 2nd Street	NW 3rd Street	2L	Class II	497	NB/EB	45	49	2	51	4	55	Yes
NW 13th Ave	NW 2nd Street	NW 3rd Street	2L	Class II	497	SB/AWB	91	98	5	103	8	111	Yes
PM Peak Hour													
NW 2nd Street	NW 13th Ave	NW 12th Ave	2L	Class II	497	NB/EB	70	76	4	80	4	84	Yes
NW 2nd Street	NW 12th Ave	NW 10th Ave	2L	Class II	497	SB/AWB	151	163	8	171	4	175	Yes
NW 12th Ave	Atlantic Ave	NW 2nd Street	2L	Class II	497	NB/EB	111	120	6	126	11	137	Yes
NW 10th Ave	Atlantic Ave	NW 2nd Street	2L	Class II	497	SB/AWB	126	136	7	143	12	155	Yes
NW 10th Ave	Atlantic Ave	NW 2nd Street	2L	Class II	497	NB/EB	183	198	10	208	15	223	Yes
NW 10th Ave	NW 2nd Street	NW 2nd Street	2L	Class II	497	SB/AWB	117	126	6	132	16	148	Yes
NW 10th Ave	NW 2nd Street	NW 3rd Street	2L	Class II	497	NB/EB	58	63	3	66	8	74	Yes
NW 3rd Street	NW 13th Ave	NW 10th Ave	2L	Class II	497	SB/AWB	74	80	4	84	8	92	Yes
NW 3rd Street	NW 2nd Street	NW 3rd Street	2L	Class II	497	NB/EB	129	139	7	146	4	150	Yes
NW 3rd Street	NW 2nd Street	NW 3rd Street	2L	Class II	497	SB/AWB	101	109	6	115	4	119	Yes
NW 13th Ave	NW 2nd Street	NW 3rd Street	2L	Class II	497	NB/EB	68	73	4	77	8	85	Yes
NW 13th Ave	NW 2nd Street	NW 3rd Street	2L	Class II	497	SB/AWB	82	89	5	94	7	101	Yes
NW 13th Ave	NW 2nd Street	NW 3rd Street	2L	Class II	497	NB/EB	80	86	4	90	8	98	Yes
NW 13th Ave	NW 2nd Street	NW 3rd Street	2L	Class II	497	SB/AWB	70	76	4	80	7	87	Yes

*LOS D Service Volume Calculated based on 2018 FDOT QLOS Handbook Values for Peak Hour Directional 2L Undivided Non-State Roadway with No Exclusive Turn Lanes.

Table 3: Future Total (2026) Intersection Capacity Analysis Summary

AM Peak Hour Delay/LOS					
Intersection	Overall	NB	SB	EB	WB
NW 2nd St & NW 12th Ave	-	10.6/B	-	-	6.3/A
NW 2nd St & NW 10th Ave	8.3/A	8.2/A	8.2/A	8.4/A	8.2/A
NW 3rd St & NW 13th Ave	-	8.9/A	-	-	6.5/A
PM Peak Hour Delay/LOS					
Intersection	Overall	NB	SB	EB	WB
NW 2nd St & NW 12th Ave	-	11.8/B	-	-	4.8/A
NW 2nd St & NW 10th Ave	8.6/A	8.3/A	8.5/A	8.5/A	8.8/A
NW 3rd St & NW 13th Ave	-	9.2/A	-	-	5.1/A

Conclusion

The Pompey Park site is bordered by NW 2nd Street, NW 10th Avenue, NW 3rd Street, and NW 13th Avenue in Delray Beach, Florida. The existing park is proposed to be redeveloped and will include the expansion of the existing recreation center building. Based on the evaluation conducted, all of the roadway links and intersections surrounding the site are anticipated to operate acceptably through buildout of the site.

Appendix A: Project Site Data

PAPA Banner

Location Address 1101 NW 2ND ST
Municipality DELRAY BEACH
Parcel Control Number 12-43-46-17-00-000-3140
Subdivision
Official Records Book 01886 **Page**1162
Sale Date JAN-1971
Legal Description 17-46-43, S 1/2 OF SW 1/4 OF NE 1/4 OF NW 1/4 /LESS E 25 FT RD R/W/

Owners
 DELRAY BEACH CITY OF

Mailing address
 100 NW 1ST AVE
 DELRAY BEACH FL 33444 2612

Sales Date	Price	OR Book/Page	Sale Type	Owner
JAN-1971	\$60,000	01886 / 01162		DELRAY BEACH CITY OF

Exemption Applicant/Owner	Year	Detail
DELRAY BEACH CITY OF	2021	FULL: MUNICIPAL GOVERNMENT

Number of Units 0 ***Total Square Feet** 21817 **Acres** 4.7027
Use Code 8200 - FOREST/PK/REC **Zoning** OSR - OSR - OPEN SPACE & RECREATION (12-DELRAY BEACH)

Tax Year	2021 P	2020	2019
Improvement Value	\$2,688,995	\$2,723,420	\$2,661,228
Land Value	\$864,121	\$864,121	\$822,973
Total Market Value	\$3,553,116	\$3,587,541	\$3,484,201

P = Preliminary

All values are as of January 1st each year

Tax Year	2021 P	2020	2019
Assessed Value	\$3,553,116	\$3,587,541	\$3,484,201
Exemption Amount	\$3,553,116	\$3,587,541	\$3,484,201
Taxable Value	\$0	\$0	\$0

Tax Year	2021 P	2020	2019
Ad Valorem	\$0	\$0	\$0
Non Ad Valorem	\$0	\$0	\$0
Total tax	\$0	\$0	\$0

PAPA Banner

Location Address 240 NW 10TH AVE
Municipality DELRAY BEACH
Parcel Control Number 12-43-46-17-00-000-3160
Subdivision
Official Records Book **Page**
Sale Date
Legal Description 17-46-43, N 1/2 OF SW 1/4 OF NE 1/4 OF NW 1/4

Owners
 DELRAY BEACH CITY OF

Mailing address
 100 NW 1ST AVE
 DELRAY BEACH FL 33444 2612

No Sales Information Available.

Exemption Applicant/Owner	Year	Detail
DELRAY BEACH CITY OF	2021	FULL: MUNICIPAL GOVERNMENT

Number of Units 0	*Total Square Feet 3171	Acres 4.6978
Use Code 8200 - FOREST/PK/REC	Zoning OSR - OSR - OPEN SPACE & RECREATION (12-DELRAY BEACH)	

Tax Year	2021 P	2020	2019
Improvement Value	\$437,367	\$447,321	\$452,969
Land Value	\$863,221	\$863,221	\$822,115
Total Market Value	\$1,300,588	\$1,310,542	\$1,275,084

P = Preliminary

All values are as of January 1st each year

Tax Year	2021 P	2020	2019
Assessed Value	\$1,223,717	\$1,112,470	\$1,011,336
Exemption Amount	\$1,223,717	\$1,112,470	\$1,011,336
Taxable Value	\$0	\$0	\$0

Tax Year	2021 P	2020	2019
Ad Valorem	\$0	\$0	\$0
Non Ad Valorem	\$0	\$0	\$0
Total tax	\$0	\$0	\$0

PAPA Banner

Location Address 240 NW 10TH AVE
Municipality DELRAY BEACH
Parcel Control Number 12-43-46-17-00-000-3130
Subdivision
Official Records Book 01886 **Page**1162
Sale Date JAN-1971
Legal Description 17-46-43, E 1/2 OF SE 1/4 OF NW 1/4 OF NW 1/4 /LESS W 25 FT, S 25 FT & N 25 FT ST R/W/

Owners
 DELRAY BEACH CITY OF

Mailing address
 100 NW 1ST AVE
 DELRAY BEACH FL 33444 2612

Sales Date	Price	OR Book/Page	Sale Type	Owner
JAN-1971	\$60,000	01886 / 01162		

Exemption Applicant/Owner	Year	Detail
DELRAY BEACH CITY OF	2021	FULL: MUNICIPAL GOVERNMENT

Number of Units 0 ***Total Square Feet** 2258 **Acres** 4.5115
Use Code 8200 - FOREST/PK/REC **Zoning** OSR - OSR - OPEN SPACE & RECREATION (12-DELRAY BEACH)

Tax Year	2021 P	2020	2019
Improvement Value	\$209,257	\$211,892	\$207,446
Land Value	\$828,988	\$828,988	\$789,513
Total Market Value	\$1,038,245	\$1,040,880	\$996,959

P = Preliminary

All values are as of January 1st each year

Tax Year	2021 P	2020	2019
Assessed Value	\$993,830	\$903,482	\$821,347
Exemption Amount	\$993,830	\$903,482	\$821,347
Taxable Value	\$0	\$0	\$0

Tax Year	2021 P	2020	2019
Ad Valorem	\$0	\$0	\$0
Non Ad Valorem	\$0	\$0	\$0
Total tax	\$0	\$0	\$0

Appendix B: Turning Movement Counts

Traffic Survey Specialists, Inc.

85 SE 4th Avenue, Unit 109, Delray Beach, Florida 33483
Phone (561) 272-3255

NW 2ND STREET & NW 12TH AVENUE
DELRAY BEACH, FLORIDA
VIDEO COUNT
NOT SIGNALIZED

File Name : 2ND STREET & 12TH AVE
Site Code : 00210102
Start Date : 8/24/2021
Page No : 1

Groups Printed- ALL VEHICLES

Start Time	----- From North				NW 2ND STREET From East				NW 12TH AVENUE From South				NW 2ND STREET From West				Int. Total
	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	
07:00 AM	0	0	0	0	0	15	5	0	0	5	0	6	0	0	2	18	51
07:15 AM	0	0	0	0	0	19	8	0	0	11	0	15	0	0	3	29	85
07:30 AM	0	0	0	0	0	30	9	0	0	16	0	25	0	0	6	19	105
07:45 AM	0	0	0	0	0	29	2	0	0	9	0	16	0	0	4	19	79
Total	0	0	0	0	0	93	24	0	0	41	0	62	0	0	15	85	320
08:00 AM	0	0	0	0	0	15	4	0	0	12	0	6	0	0	3	12	52
08:15 AM	0	0	0	0	0	13	7	0	0	6	0	8	0	0	3	18	55
08:30 AM	0	0	0	0	1	9	4	0	0	5	0	14	0	0	2	14	49
08:45 AM	0	0	0	0	0	13	4	0	0	6	0	6	0	0	0	8	37
Total	0	0	0	0	1	50	19	0	0	29	0	34	0	0	8	52	193
04:00 PM	0	0	0	0	0	23	2	0	0	11	0	13	0	0	4	4	57
04:15 PM	0	0	0	0	0	13	10	0	0	9	0	13	0	0	4	9	58
04:30 PM	0	0	0	0	0	21	14	0	0	15	0	14	0	0	9	14	87
04:45 PM	0	0	0	0	0	15	16	0	0	23	0	12	0	0	4	9	79
Total	0	0	0	0	0	72	42	0	0	58	0	52	0	0	21	36	281
05:00 PM	0	0	0	0	0	20	10	0	0	16	0	9	0	0	4	14	73
05:15 PM	0	0	0	0	0	22	18	0	0	22	0	20	0	0	6	12	100
05:30 PM	0	0	0	0	0	12	6	0	0	34	0	22	0	0	15	7	96
05:45 PM	0	0	0	0	0	19	19	0	0	26	0	34	0	0	1	11	110
Total	0	0	0	0	0	73	53	0	0	98	0	85	0	0	26	44	379
Grand Total	0	0	0	0	1	288	138	0	0	226	0	233	0	0	70	217	1173
Apprch %	0	0	0	0	0.2	67.4	32.3	0	0	49.2	0	50.8	0	0	24.4	75.6	
Total %	0	0	0	0	0.1	24.6	11.8	0	0	19.3	0	19.9	0	0	6	18.5	

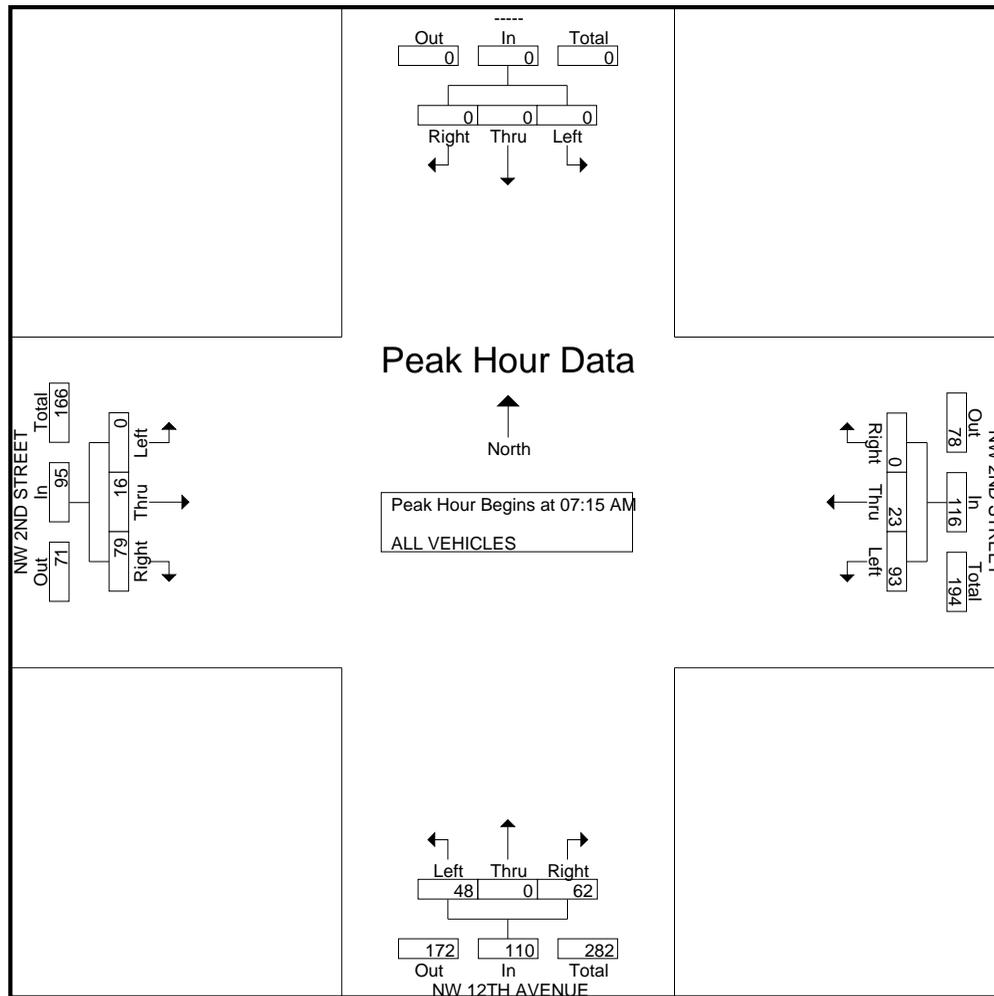
Traffic Survey Specialists, Inc.

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NW 2ND STREET & NW 12TH AVENUE
DELRAY BEACH, FLORIDA
VIDEO COUNT
NOT SIGNALIZED

File Name : 2ND STREET & 12TH AVE
Site Code : 00210102
Start Date : 8/24/2021
Page No : 2

Start Time	----- From North					NW 2ND STREET From East					NW 12TH AVENUE From South					NW 2ND STREET From West					Int. Total
	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:15 AM																					
07:15 AM	0	0	0	0	0	0	19	8	0	27	0	11	0	15	26	0	0	3	29	32	85
07:30 AM	0	0	0	0	0	0	30	9	0	39	0	16	0	25	41	0	0	6	19	25	105
07:45 AM	0	0	0	0	0	0	29	2	0	31	0	9	0	16	25	0	0	4	19	23	79
08:00 AM	0	0	0	0	0	0	15	4	0	19	0	12	0	6	18	0	0	3	12	15	52
Total Volume	0	0	0	0	0	0	93	23	0	116	0	48	0	62	110	0	0	16	79	95	321
% App. Total	0	0	0	0	0	0	80.2	19.8	0		0	43.6	0	56.4		0	0	16.8	83.2		
PHF	.000	.000	.000	.000	.000	.000	.775	.639	.000	.744	.000	.750	.000	.620	.671	.000	.000	.667	.681	.742	.764



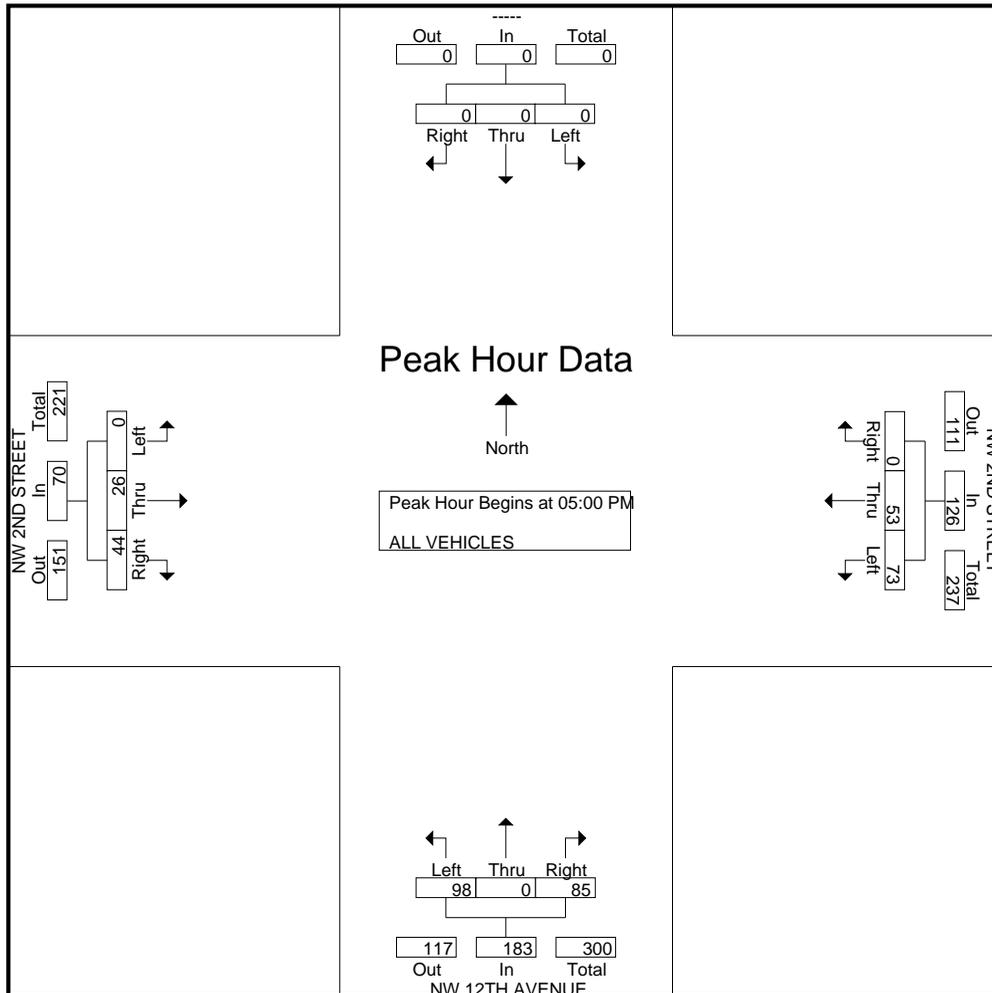
Traffic Survey Specialists, Inc.

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Phone (561) 272-3255

NW 2ND STREET & NW 12TH AVENUE
DELRAY BEACH, FLORIDA
VIDEO COUNT
NOT SIGNALIZED

File Name : 2ND STREET & 12TH AVE
Site Code : 00210102
Start Date : 8/24/2021
Page No : 3

Start Time	----- From North					NW 2ND STREET From East					NW 12TH AVENUE From South					NW 2ND STREET From West					Int. Total
	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 05:00 PM																					
05:00 PM	0	0	0	0	0	0	20	10	0	30	0	16	0	9	25	0	0	4	14	18	73
05:15 PM	0	0	0	0	0	0	22	18	0	40	0	22	0	20	42	0	0	6	12	18	100
05:30 PM	0	0	0	0	0	0	12	6	0	18	0	34	0	22	56	0	0	15	7	22	96
05:45 PM	0	0	0	0	0	0	19	19	0	38	0	26	0	34	60	0	0	1	11	12	110
Total Volume	0	0	0	0	0	0	73	53	0	126	0	98	0	85	183	0	0	26	44	70	379
% App. Total	0	0	0	0	0	0	57.9	42.1	0		0	53.6	0	46.4		0	0	37.1	62.9		
PHF	.000	.000	.000	.000	.000	.000	.830	.697	.000	.788	.000	.721	.000	.625	.763	.000	.000	.433	.786	.795	.861



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NOT SIGNALIZED

File Name : 2ND STREET & 12TH AVE
Site Code : 00210102
Start Date : 8/24/2021
Page No : 1

Groups Printed- PEDESTRIANS & BIKES

Start Time	----- From North				NW 2ND STREET From East				NW 12TH AVENUE From South				NW 2ND STREET From West				Int. Total	
	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right		
07:00 AM	7	0	1	0	0	0	0	0	0	0	0	0	0	1	0	0	0	9
07:15 AM	6	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7
07:30 AM	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
07:45 AM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
Total	15	0	2	0	0	0	0	0	1	0	0	0	0	1	0	0	0	19
08:00 AM	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2
08:15 AM	2	0	1	0	0	0	1	0	0	0	0	0	0	1	0	0	0	5
08:30 AM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	4	0	1	0	1	0	1	0	0	0	0	0	0	1	0	0	0	8
04:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
04:15 PM	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2
04:30 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	2
04:45 PM	2	0	0	0	0	0	0	0	5	0	0	0	0	2	0	0	0	9
Total	4	0	1	0	1	0	0	0	5	0	0	0	0	2	0	1	0	14
05:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
05:30 PM	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
05:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1
Total	0	0	1	0	3	0	0	0	0	0	0	0	0	0	0	1	0	5
Grand Total	23	0	5	0	5	0	1	0	6	0	0	0	0	4	0	2	0	46
Apprch %	82.1	0	17.9	0	83.3	0	16.7	0	100	0	0	0	0	66.7	0	33.3	0	
Total %	50	0	10.9	0	10.9	0	2.2	0	13	0	0	0	0	8.7	0	4.3	0	

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NW 2ND STREET & NW 10TH AVENUE
DELRAY BEACH, FLORIDA
VIDEO COUNT
NOT SIGNALIZED

File Name : 2nd street & 10 ave
Site Code : 00210102
Start Date : 8/24/2021
Page No : 1

Groups Printed- ALL VEHICLES

Start Time	NW 10TH AVENUE From North				NW 2ND STREET From East				NW 10TH AVENUE From South				NW 2ND STREET From West				Int. Total
	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	
07:00 AM	0	7	6	5	0	0	14	3	0	0	8	1	0	5	2	0	51
07:15 AM	0	4	11	2	0	1	17	4	0	2	15	0	0	12	9	0	77
07:30 AM	0	4	9	14	0	2	21	3	0	0	19	3	0	17	14	0	106
07:45 AM	0	5	16	17	0	2	21	10	0	0	13	1	0	13	15	2	115
Total	0	20	42	38	0	5	73	20	0	2	55	5	0	47	40	2	349
08:00 AM	0	6	13	5	0	1	15	2	0	1	10	0	0	3	6	0	62
08:15 AM	0	1	13	6	0	1	16	5	0	3	7	0	0	3	8	0	63
08:30 AM	0	5	6	4	0	0	11	1	0	2	3	1	0	7	10	1	51
08:45 AM	0	0	2	8	0	2	16	0	0	1	8	1	0	1	4	0	43
Total	0	12	34	23	0	4	58	8	0	7	28	2	0	14	28	1	219
04:00 PM	0	4	11	7	0	1	16	2	0	1	5	2	0	10	6	1	66
04:15 PM	0	0	10	8	0	3	17	13	0	1	10	0	0	7	10	1	80
04:30 PM	0	9	13	7	0	3	23	9	0	3	9	1	0	14	12	0	103
04:45 PM	0	4	7	8	0	6	21	9	0	3	5	1	0	6	6	5	81
Total	0	17	41	30	0	13	77	33	0	8	29	4	0	37	34	7	330
05:00 PM	0	6	17	3	0	4	22	17	0	1	13	3	0	7	11	1	105
05:15 PM	0	4	16	7	0	1	35	8	0	2	14	3	0	18	12	1	121
05:30 PM	0	7	8	13	0	5	10	4	0	2	8	3	0	9	13	6	88
05:45 PM	0	2	10	14	0	2	24	4	0	4	11	1	0	11	11	0	94
Total	0	19	51	37	0	12	91	33	0	9	46	10	0	45	47	8	408
Grand Total	0	68	168	128	0	34	299	94	0	26	158	21	0	143	149	18	1306
Apprch %	0	18.7	46.2	35.2	0	8	70	22	0	12.7	77.1	10.2	0	46.1	48.1	5.8	
Total %	0	5.2	12.9	9.8	0	2.6	22.9	7.2	0	2	12.1	1.6	0	10.9	11.4	1.4	

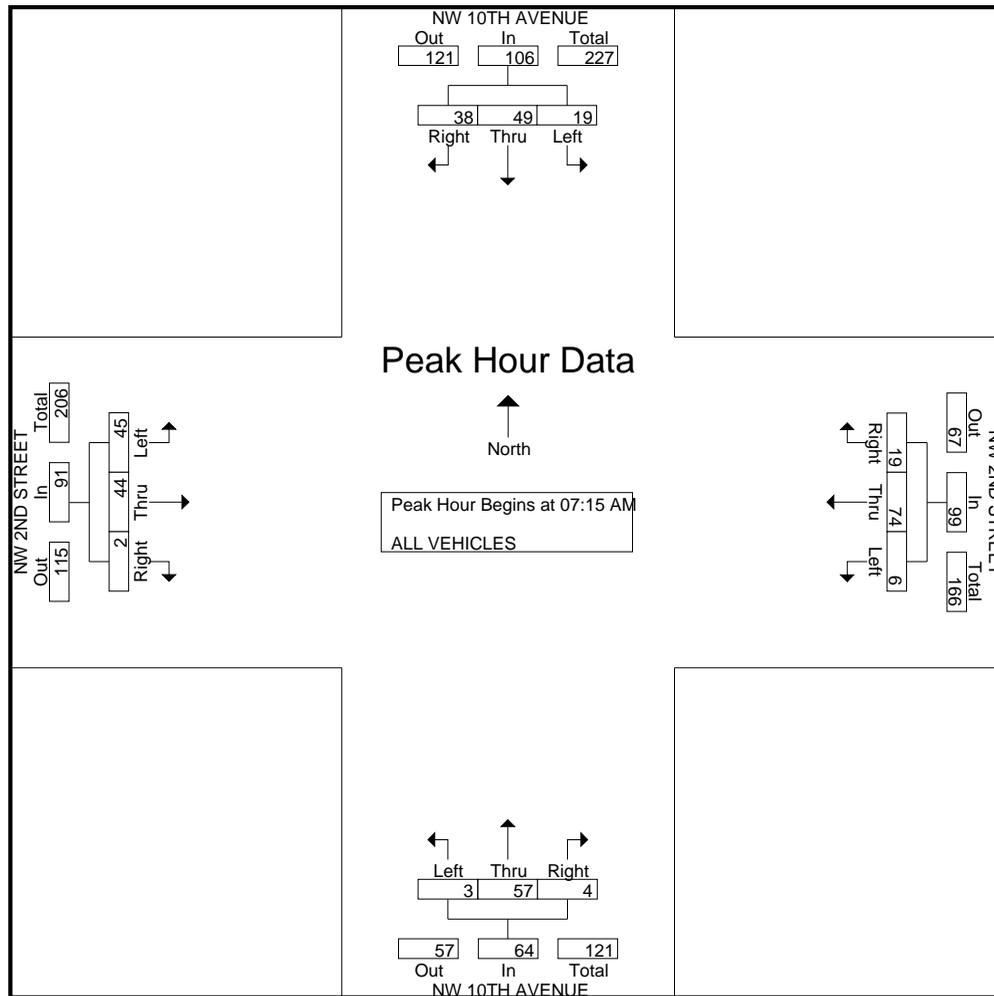
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Site Code : 00210102
Start Date : 8/24/2021
Page No : 2

Start Time	NW 10TH AVENUE From North					NW 2ND STREET From East					NW 10TH AVENUE From South					NW 2ND STREET From West					Int. Total
	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:15 AM																					
07:15 AM	0	4	11	2	17	0	1	17	4	22	0	2	15	0	17	0	12	9	0	21	77
07:30 AM	0	4	9	14	27	0	2	21	3	26	0	0	19	3	22	0	17	14	0	31	106
07:45 AM	0	5	16	17	38	0	2	21	10	33	0	0	13	1	14	0	13	15	2	30	115
08:00 AM	0	6	13	5	24	0	1	15	2	18	0	1	10	0	11	0	3	6	0	9	62
Total Volume	0	19	49	38	106	0	6	74	19	99	0	3	57	4	64	0	45	44	2	91	360
% App. Total	0	17.9	46.2	35.8		0	6.1	74.7	19.2		0	4.7	89.1	6.2		0	49.5	48.4	2.2		
PHF	.000	.792	.766	.559	.697	.000	.750	.881	.475	.750	.000	.375	.750	.333	.727	.000	.662	.733	.250	.734	.783



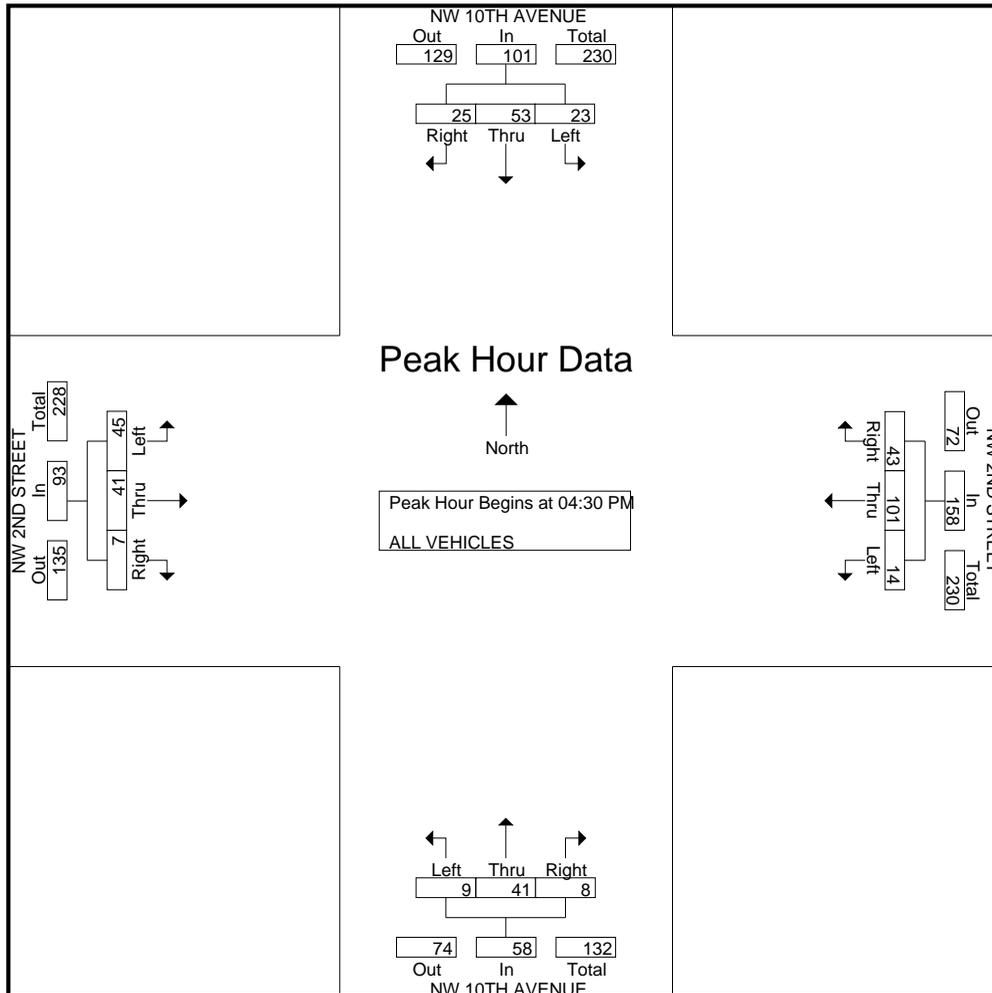
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Page No : 3

Start Time	NW 10TH AVENUE From North					NW 2ND STREET From East					NW 10TH AVENUE From South					NW 2ND STREET From West					Int. Total
	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:30 PM																					
04:30 PM	0	9	13	7	29	0	3	23	9	35	0	3	9	1	13	0	14	12	0	26	103
04:45 PM	0	4	7	8	19	0	6	21	9	36	0	3	5	1	9	0	6	6	5	17	81
05:00 PM	0	6	17	3	26	0	4	22	17	43	0	1	13	3	17	0	7	11	1	19	105
05:15 PM	0	4	16	7	27	0	1	35	8	44	0	2	14	3	19	0	18	12	1	31	121
Total Volume	0	23	53	25	101	0	14	101	43	158	0	9	41	8	58	0	45	41	7	93	410
% App. Total	0	22.8	52.5	24.8		0	8.9	63.9	27.2		0	15.5	70.7	13.8		0	48.4	44.1	7.5		
PHF	.000	.639	.779	.781	.871	.000	.583	.721	.632	.898	.000	.750	.732	.667	.763	.000	.625	.854	.350	.750	.847



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Groups Printed- PEDESTRIANS & BIKES

Start Time	NW 10TH AVENUE From North				NW 2ND STREET From East				NW 10TH AVENUE From South				NW 2ND STREET From West				Int. Total
	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	
07:00 AM	1	0	0	0	0	0	0	0	1	0	0	0	2	0	1	0	5
07:15 AM	4	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	5
07:30 AM	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	2
07:45 AM	1	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	3
Total	6	0	1	0	1	0	2	0	2	0	0	0	2	0	1	0	15
08:00 AM	1	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	3
08:30 AM	1	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	2
08:45 AM	1	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	3
Total	3	0	0	0	0	0	1	0	2	0	1	0	1	0	0	0	8
04:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1
04:30 PM	2	0	0	0	1	0	0	0	3	0	0	0	0	0	0	0	6
04:45 PM	2	0	0	0	0	0	0	0	2	0	1	0	0	0	1	0	6
Total	4	0	1	0	1	0	0	0	5	0	1	0	0	0	1	0	13
05:00 PM	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
05:15 PM	3	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	4
05:30 PM	2	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	3
05:45 PM	1	0	1	0	0	0	0	0	1	0	0	0	0	0	0	0	3
Total	9	0	1	0	0	0	0	0	2	0	1	0	0	0	0	0	13
Grand Total	22	0	3	0	2	0	3	0	11	0	3	0	3	0	2	0	49
Apprch %	88	0	12	0	40	0	60	0	78.6	0	21.4	0	60	0	40	0	
Total %	44.9	0	6.1	0	4.1	0	6.1	0	22.4	0	6.1	0	6.1	0	4.1	0	

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Site Code : 00210102
Start Date : 8/24/2021
Page No : 1

Groups Printed- ALL VEHICLES

Start Time	----- From North				NW 3RD STREET From East				NW 13TH AVENUE From South				NW 3RD STREET From West				Int. Total
	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	UTurn	Left	Thru	Right	
07:00 AM	0	0	0	0	0	20	3	0	0	5	0	8	0	0	0	3	39
07:15 AM	0	0	0	0	0	23	2	0	0	0	0	9	0	0	3	4	41
07:30 AM	0	0	0	0	0	16	3	0	0	2	0	12	0	0	1	1	35
07:45 AM	0	0	0	0	0	21	5	0	0	1	0	8	0	0	0	3	38
Total	0	0	0	0	0	80	13	0	0	8	0	37	0	0	4	11	153
08:00 AM	0	0	0	0	0	12	3	0	0	2	0	8	0	0	4	1	30
08:15 AM	0	0	0	0	0	14	1	0	0	5	0	8	0	0	1	3	32
08:30 AM	0	0	0	0	0	11	2	0	0	2	0	4	0	0	0	5	24
08:45 AM	0	0	0	0	0	8	3	0	0	1	0	5	0	0	1	2	20
Total	0	0	0	0	0	45	9	0	0	10	0	25	0	0	6	11	106
04:00 PM	0	0	0	0	0	11	3	0	0	1	0	9	0	0	4	2	30
04:15 PM	0	0	0	0	0	8	4	0	0	3	0	8	0	0	1	4	28
04:30 PM	0	0	0	0	0	16	4	0	0	4	0	9	0	0	1	1	35
04:45 PM	0	0	0	0	0	7	5	0	0	5	0	15	0	0	4	3	39
Total	0	0	0	0	0	42	16	0	0	13	0	41	0	0	10	10	132
05:00 PM	0	0	0	0	0	14	11	0	0	4	0	11	0	0	3	2	45
05:15 PM	0	0	0	0	0	12	8	0	0	5	0	14	0	0	2	4	45
05:30 PM	0	0	0	0	0	14	7	0	0	8	0	16	0	0	4	7	56
05:45 PM	0	0	0	0	0	13	3	0	0	5	0	17	0	0	1	4	43
Total	0	0	0	0	0	53	29	0	0	22	0	58	0	0	10	17	189
Grand Total	0	0	0	0	0	220	67	0	0	53	0	161	0	0	30	49	580
Apprch %	0	0	0	0	0	76.7	23.3	0	0	24.8	0	75.2	0	0	38	62	
Total %	0	0	0	0	0	37.9	11.6	0	0	9.1	0	27.8	0	0	5.2	8.4	

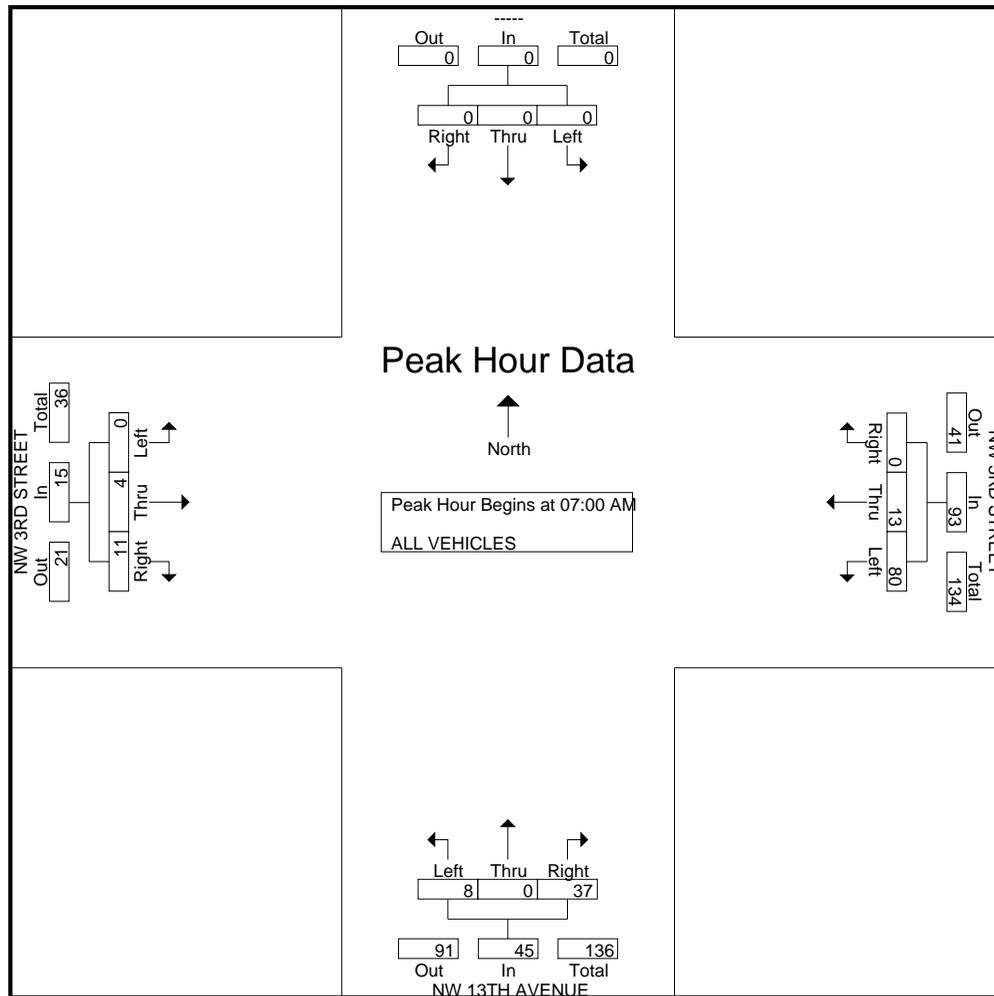
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	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:00 AM																					
07:00 AM	0	0	0	0	0	0	20	3	0	23	0	5	0	8	13	0	0	0	3	3	39
07:15 AM	0	0	0	0	0	0	23	2	0	25	0	0	0	9	9	0	0	3	4	7	41
07:30 AM	0	0	0	0	0	0	16	3	0	19	0	2	0	12	14	0	0	1	1	2	35
07:45 AM	0	0	0	0	0	0	21	5	0	26	0	1	0	8	9	0	0	0	3	3	38
Total Volume	0	0	0	0	0	0	80	13	0	93	0	8	0	37	45	0	0	4	11	15	153
% App. Total	0	0	0	0	0	0	86	14	0		0	17.8	0	82.2		0	0	26.7	73.3		
PHF	.000	.000	.000	.000	.000	.000	.870	.650	.000	.894	.000	.400	.000	.771	.804	.000	.000	.333	.688	.536	.933



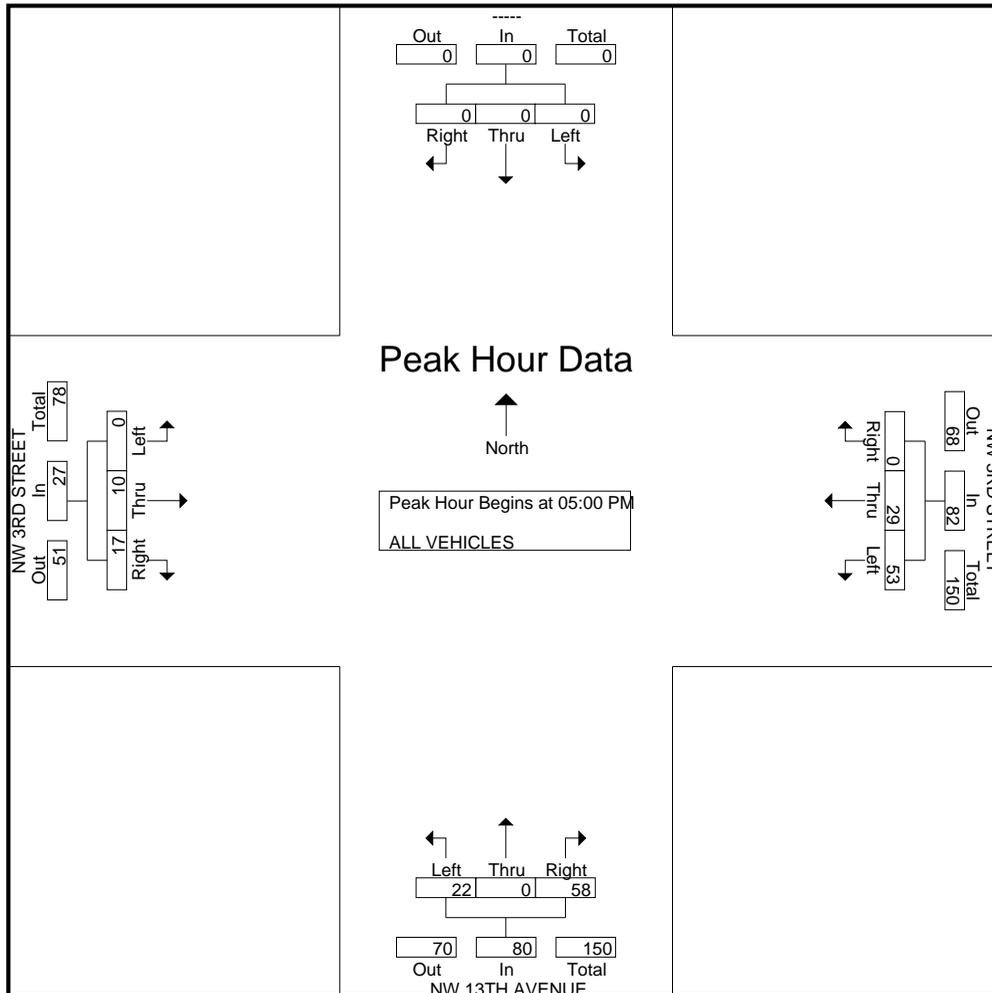
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	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	UTurn	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 05:00 PM																					
05:00 PM	0	0	0	0	0	0	14	11	0	25	0	4	0	11	15	0	0	3	2	5	45
05:15 PM	0	0	0	0	0	0	12	8	0	20	0	5	0	14	19	0	0	2	4	6	45
05:30 PM	0	0	0	0	0	0	14	7	0	21	0	8	0	16	24	0	0	4	7	11	56
05:45 PM	0	0	0	0	0	0	13	3	0	16	0	5	0	17	22	0	0	1	4	5	43
Total Volume	0	0	0	0	0	0	53	29	0	82	0	22	0	58	80	0	0	10	17	27	189
% App. Total	0	0	0	0	0	0	64.6	35.4	0		0	27.5	0	72.5		0	0	37	63		
PHF	.000	.000	.000	.000	.000	.000	.946	.659	.000	.820	.000	.688	.000	.853	.833	.000	.000	.625	.607	.614	.844



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Start Time	----- From North				NW 3RD STREET From East				NW 13TH AVENUE From South				NW 3RD STREET From West				Int. Total
	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	Peds	Left	BIKES	Right	
07:15 AM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
07:30 AM	2	0	0	0	2	0	0	0	3	0	0	0	0	0	0	0	7
Total	3	0	0	0	2	0	0	0	3	0	0	0	0	0	0	0	8
08:00 AM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
08:30 AM	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	2
08:45 AM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	2	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	4
05:00 PM	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	2
Total	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	2
Grand Total	5	0	0	0	2	0	0	0	5	0	0	0	2	0	0	0	14
Apprch %	100	0	0	0	100	0	0	0	100	0	0	0	100	0	0	0	
Total %	35.7	0	0	0	14.3	0	0	0	35.7	0	0	0	14.3	0	0	0	

Appendix C: Intersection Volume Development Sheets

CRITICAL MOVEMENT ANALYSIS
POMPEY PARK
NW 2ND STREET & NW 12TH AVENUE

Growth Rate = 1.00%
Peak Season = 1.08
Buildout Year = 2026 2026
Years = 5 5

AM Peak Hour

	Northbound			Southbound			Eastbound			Westbound		
	LT	Thru	RT	LT	Thru	RT	LT	Thru	RT	LT	Thru	RT
Existing Volume on 08/24/2021	48	0	62	0	0	0	0	16	79	93	23	0
Peak Season Volume	52	0	67	0	0	0	0	17	85	100	25	0
Traffic Volume Growth Committed Development	3	0	3	0	0	0	0	1	4	5	1	0
1.0% Traffic Volume Growth Committed + 1.0% Growth	3	0	3	0	0	0	0	1	4	5	1	0
Max (Committed + 1.0% or Historic Growth)	3	0	3	0	0	0	0	1	4	5	1	0
Background Traffic Volumes	55	0	70	0	0	0	0	18	89	105	26	0
Project Traffic Assignment												
Inbound Traffic Assignment	10.0%		30.0%									
Inbound Traffic Volumes	4		11									
Outbound Traffic Assignment									10.0%	30.0%		
Outbound Traffic Volumes									2	6		
Project Traffic	4	0	11	0	0	0	0	0	2	6	0	0
TOTAL TRAFFIC	59	0	81	0	0	0	0	18	91	111	26	0

PM Peak Hour

	Northbound			Southbound			Eastbound			Westbound		
	LT	Thru	RT	LT	Thru	RT	LT	Thru	RT	LT	Thru	RT
Existing Volume on 08/24/2021	98	0	85	0	0	0	0	26	44	73	53	0
Peak Season Volume	106	0	92	0	0	0	0	28	48	79	57	0
Traffic Volume Growth Committed Development	5	0	5	0	0	0	0	1	2	4	3	0
1.0% Traffic Volume Growth Committed + 1.0% Growth	5	0	5	0	0	0	0	1	2	4	3	0
Max (Committed + 1.0% or Historic Growth)	5	0	5	0	0	0	0	1	2	4	3	0
Background Traffic Volumes	111	0	97	0	0	0	0	29	50	83	60	0
Project Traffic Assignment												
Inbound Traffic Assignment	10.0%		30.0%									
Inbound Traffic Volumes	4		11									
Outbound Traffic Assignment									10.0%	30.0%		
Outbound Traffic Volumes									4	12		
Project Traffic	4	0	11	0	0	0	0	0	4	12	0	0
TOTAL TRAFFIC	115	0	108	0	0	0	0	29	54	95	60	0

**CRITICAL MOVEMENT ANALYSIS
POMPEY PARK
NW 2ND STREET & NW 10TH AVENUE**

Growth Rate = 1.00%
 Peak Season = 1.08 1.08
 Buildout Year = 2026 2026
 Years = 5 5

AM Peak Hour

	Northbound			Southbound			Eastbound			Westbound		
	LT	Thru	RT	LT	Thru	RT	LT	Thru	RT	LT	Thru	RT
Existing Volume on 08/24/2021	3	57	4	19	49	38	45	44	2	6	74	19
Peak Season Volume	3	62	4	21	53	41	49	48	2	6	80	21
Traffic Volume Growth Committed Development	0	3	0	1	3	2	2	2	0	0	4	1
1.0% Traffic Volume Growth Committed + 1.0% Growth	0	3	0	1	3	2	2	2	0	0	4	1
Max (Committed + 1.0% or Historic Growth)	0	3	0	1	3	2	2	2	0	0	4	1
Background Traffic Volumes	3	65	4	22	56	43	51	50	2	6	84	22
Project Traffic Assignment												
Inbound Traffic Assignment	10.0%	10.0%										
Inbound Traffic Volumes	4	4										
Outbound Traffic Assignment					10.0%				10.0%			
Outbound Traffic Volumes					2				2			
Project Traffic	4	4	0	0	2	0	0	0	2	0	0	0
TOTAL TRAFFIC	7	69	4	22	58	43	51	50	4	6	84	22

PM Peak Hour

	Northbound			Southbound			Eastbound			Westbound		
	LT	Thru	RT	LT	Thru	RT	LT	Thru	RT	LT	Thru	RT
Existing Volume on 08/24/2021	9	41	8	23	53	25	45	41	7	14	101	43
Peak Season Volume	10	44	9	25	57	27	49	44	8	15	109	46
Traffic Volume Growth Committed Development	1	2	0	1	3	1	2	2	0	1	6	2
1.0% Traffic Volume Growth Committed + 1.0% Growth	1	2	0	1	3	1	2	2	0	1	6	2
Max (Committed + 1.0% or Historic Growth)	1	2	0	1	3	1	2	2	0	1	6	2
Background Traffic Volumes	11	46	9	26	60	28	51	46	8	16	115	48
Project Traffic Assignment												
Inbound Traffic Assignment	10.0%	10.0%										
Inbound Traffic Volumes	4	4										
Outbound Traffic Assignment					10.0%				10.0%			
Outbound Traffic Volumes					4				4			
Project Traffic	4	4	0	0	4	0	0	0	4	0	0	0
TOTAL TRAFFIC	15	50	9	26	64	28	51	46	12	16	115	48

CRITICAL MOVEMENT ANALYSIS
POMPEY PARK
NW 3RD STREET & NW 13TH AVENUE

Growth Rate = 1.00%
Peak Season = 1.08 1.08
Buildout Year = 2026 2026
Years = 5 5

AM Peak Hour

	Northbound			Southbound			Eastbound			Westbound		
	LT	Thru	RT	LT	Thru	RT	LT	Thru	RT	LT	Thru	RT
Existing Volume on 08/24/2021	8	0	37	0	0	0	0	4	11	80	13	0
Peak Season Volume	9	0	40	0	0	0	0	4	12	86	14	0
Traffic Volume Growth Committed Development	0	0	2	0	0	0	0	0	1	4	1	0
1.0% Traffic Volume Growth Committed + 1.0% Growth	0	0	2	0	0	0	0	0	1	4	1	0
Max (Committed + 1.0% or Historic Growth)	0	0	2	0	0	0	0	0	1	4	1	0
Background Traffic Volumes	9	0	42	0	0	0	0	4	13	90	15	0
Project Traffic Assignment												
Inbound Traffic Assignment										20.0%		
Inbound Traffic Volumes										8		
Outbound Traffic Assignment			20.0%									
Outbound Traffic Volumes			4									
Project Traffic	0	0	4	0	0	0	0	0	0	8	0	0
TOTAL TRAFFIC	9	0	46	0	0	0	0	4	13	98	15	0

PM Peak Hour

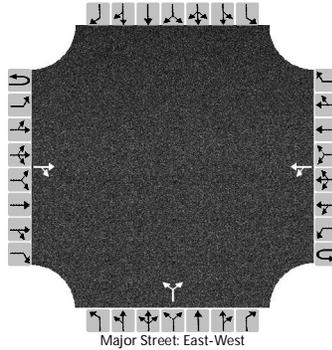
	Northbound			Southbound			Eastbound			Westbound		
	LT	Thru	RT	LT	Thru	RT	LT	Thru	RT	LT	Thru	RT
Existing Volume on 08/24/2021	22	0	58	0	0	0	0	10	17	53	29	0
Peak Season Volume	24	0	63	0	0	0	0	11	18	57	31	0
Traffic Volume Growth Committed Development	1	0	3	0	0	0	0	1	1	3	2	0
1.0% Traffic Volume Growth Committed + 1.0% Growth	1	0	3	0	0	0	0	1	1	3	2	0
Max (Committed + 1.0% or Historic Growth)	1	0	3	0	0	0	0	1	1	3	2	0
Background Traffic Volumes	25	0	66	0	0	0	0	12	19	60	33	0
Project Traffic Assignment												
Inbound Traffic Assignment										20.0%		
Inbound Traffic Volumes										7		
Outbound Traffic Assignment			20.0%									
Outbound Traffic Volumes			8									
Project Traffic	0	0	8	0	0	0	0	0	0	7	0	0
TOTAL TRAFFIC	25	0	74	0	0	0	0	12	19	67	33	0

Appendix D: HCS Output Sheets

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	SK			Intersection	NW 2nd St & NW 12th Ave		
Agency/Co.	Kimley-Horn			Jurisdiction	Delray Beach		
Date Performed	11/19/2021			East/West Street	NW 2nd Street		
Analysis Year	2026			North/South Street	NW 12th Avenue		
Time Analyzed	AM Peak			Peak Hour Factor	0.95		
Intersection Orientation	East-West			Analysis Time Period (hrs)	0.25		
Project Description	Pompey Park						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Priority																
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume (veh/h)			18	91		111	26			59		81				
Percent Heavy Vehicles (%)						3				3		3				
Proportion Time Blocked																
Percent Grade (%)									0							
Right Turn Channelized																
Median Type Storage	Undivided															

Critical and Follow-up Headways

Base Critical Headway (sec)						4.1					7.1		6.2			
Critical Headway (sec)						4.13					6.43		6.23			
Base Follow-Up Headway (sec)						2.2					3.5		3.3			
Follow-Up Headway (sec)						2.23					3.53		3.33			

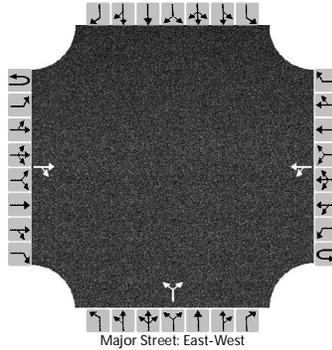
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						117						147				
Capacity, c (veh/h)						1468						786				
v/c Ratio						0.08						0.19				
95% Queue Length, Q ₉₅ (veh)						0.3						0.7				
Control Delay (s/veh)						7.7						10.6				
Level of Service (LOS)						A						B				
Approach Delay (s/veh)					6.3				10.6							
Approach LOS									B							

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	SK			Intersection	NW 2nd St & NW 12th Ave		
Agency/Co.	Kimley-Horn			Jurisdiction	Delray Beach		
Date Performed	11/19/2021			East/West Street	NW 2nd Street		
Analysis Year	2026			North/South Street	NW 12th Avenue		
Time Analyzed	PM Peak			Peak Hour Factor	0.95		
Intersection Orientation	East-West			Analysis Time Period (hrs)	0.25		
Project Description	Pompey Park						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Priority																
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume (veh/h)			29	54		95	60			115		108				
Percent Heavy Vehicles (%)						3				3		3				
Proportion Time Blocked																
Percent Grade (%)									0							
Right Turn Channelized																
Median Type Storage	Undivided															

Critical and Follow-up Headways

Base Critical Headway (sec)						4.1					7.1		6.2			
Critical Headway (sec)						4.13					6.43		6.23			
Base Follow-Up Headway (sec)						2.2					3.5		3.3			
Follow-Up Headway (sec)						2.23					3.53		3.33			

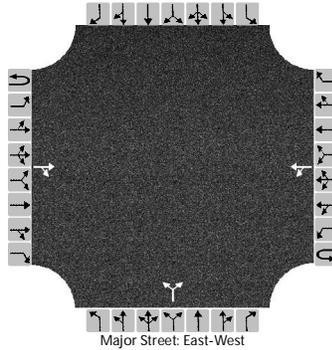
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						100						235				
Capacity, c (veh/h)						1502						764				
v/c Ratio						0.07						0.31				
95% Queue Length, Q ₉₅ (veh)						0.2						1.3				
Control Delay (s/veh)						7.6						11.8				
Level of Service (LOS)						A						B				
Approach Delay (s/veh)					4.8				11.8							
Approach LOS									B							

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	SK			Intersection	NW 3rd St & NW 13th Ave		
Agency/Co.	Kimley-Horn			Jurisdiction	Delray Beach		
Date Performed	11/19/2021			East/West Street	NW 3rd Street		
Analysis Year	2026			North/South Street	NW 13th Avenue		
Time Analyzed	AM Peak			Peak Hour Factor	0.95		
Intersection Orientation	East-West			Analysis Time Period (hrs)	0.25		
Project Description	Pompey Park						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Priority																
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume (veh/h)			4	13		98	15			9		46				
Percent Heavy Vehicles (%)						3				3		3				
Proportion Time Blocked																
Percent Grade (%)									0							
Right Turn Channelized																
Median Type Storage	Undivided															

Critical and Follow-up Headways

Base Critical Headway (sec)						4.1					7.1		6.2			
Critical Headway (sec)						4.13					6.43		6.23			
Base Follow-Up Headway (sec)						2.2					3.5		3.3			
Follow-Up Headway (sec)						2.23					3.53		3.33			

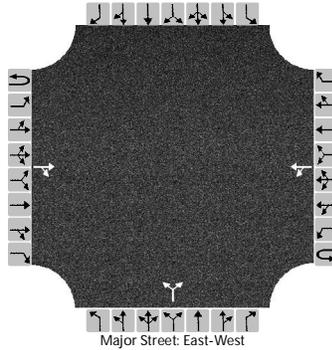
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						103						58				
Capacity, c (veh/h)						1592						984				
v/c Ratio						0.06						0.06				
95% Queue Length, Q ₉₅ (veh)						0.2						0.2				
Control Delay (s/veh)						7.4						8.9				
Level of Service (LOS)						A						A				
Approach Delay (s/veh)					6.5				8.9							
Approach LOS					A				A							

HCS7 Two-Way Stop-Control Report

General Information				Site Information			
Analyst	SK			Intersection	NW 3rd St & NW 13th Ave		
Agency/Co.	Kimley-Horn			Jurisdiction	Delray Beach		
Date Performed	11/19/2021			East/West Street	NW 3rd Street		
Analysis Year	2026			North/South Street	NW 13th Avenue		
Time Analyzed	PM Peak			Peak Hour Factor	0.95		
Intersection Orientation	East-West			Analysis Time Period (hrs)	0.25		
Project Description	Pompey Park						

Lanes



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Priority																
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume (veh/h)			12	19		67	33			25		74				
Percent Heavy Vehicles (%)						3				3		3				
Proportion Time Blocked																
Percent Grade (%)									0							
Right Turn Channelized																
Median Type Storage	Undivided															

Critical and Follow-up Headways

Base Critical Headway (sec)						4.1					7.1		6.2			
Critical Headway (sec)						4.13					6.43		6.23			
Base Follow-Up Headway (sec)						2.2					3.5		3.3			
Follow-Up Headway (sec)						2.23					3.53		3.33			

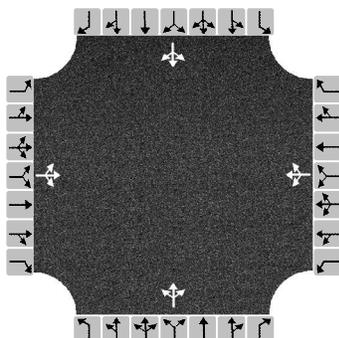
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)						71						104				
Capacity, c (veh/h)						1573						955				
v/c Ratio						0.04						0.11				
95% Queue Length, Q ₉₅ (veh)						0.1						0.4				
Control Delay (s/veh)						7.4						9.2				
Level of Service (LOS)						A						A				
Approach Delay (s/veh)					5.1				9.2							
Approach LOS					A				A							

HCS7 All-Way Stop Control Report

General Information		Site Information	
Analyst	SK	Intersection	NW 2nd St & NW 10th Ave
Agency/Co.	Kimley-Horn	Jurisdiction	Delray Beach
Date Performed	11/19/2021	East/West Street	NW 2nd Street
Analysis Year	2026	North/South Street	NW 10th Avenue
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.95
Time Analyzed	AM Peak		
Project Description	Pompey Park		

Lanes



Vehicle Volume and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement												
Volume	51	50	4	6	84	22	7	69	4	22	58	43
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR			LTR			LTR			LTR		
Flow Rate, v (veh/h)	111			118			84			129		
Percent Heavy Vehicles	2			2			2			2		

Departure Headway and Service Time

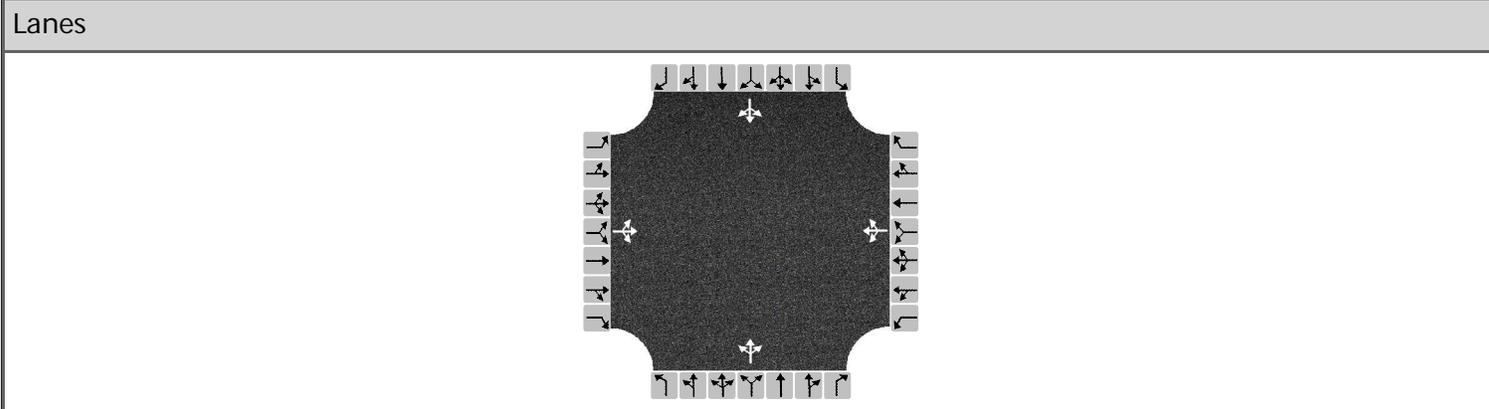
Initial Departure Headway, hd (s)	3.20			3.20			3.20			3.20		
Initial Degree of Utilization, x	0.098			0.105			0.075			0.115		
Final Departure Headway, hd (s)	4.65			4.47			4.62			4.41		
Final Degree of Utilization, x	0.143			0.146			0.108			0.159		
Move-Up Time, m (s)	2.0			2.0			2.0			2.0		
Service Time, ts (s)	2.65			2.47			2.62			2.41		

Capacity, Delay and Level of Service

Flow Rate, v (veh/h)	111			118			84			129		
Capacity	774			806			779			817		
95% Queue Length, Q ₉₅ (veh)	0.5			0.5			0.4			0.6		
Control Delay (s/veh)	8.4			8.2			8.2			8.2		
Level of Service, LOS	A			A			A			A		
Approach Delay (s/veh)	8.4			8.2			8.2			8.2		
Approach LOS	A			A			A			A		
Intersection Delay, s/veh LOS	8.3						A					

HCS7 All-Way Stop Control Report

General Information		Site Information	
Analyst	SK	Intersection	NW 2nd St & NW 10th Ave
Agency/Co.	Kimley-Horn	Jurisdiction	Delray Beach
Date Performed	11/19/2021	East/West Street	NW 2nd Street
Analysis Year	2026	North/South Street	NW 10th Avenue
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.95
Time Analyzed	PM Peak		
Project Description	Pompey Park		



Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement												
Volume	51	46	12	16	115	48	15	50	9	26	64	28
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR			LTR			LTR			LTR		
Flow Rate, v (veh/h)	115			188			78			124		
Percent Heavy Vehicles	2			2			2			2		

Departure Headway and Service Time												
Initial Departure Headway, hd (s)	3.20			3.20			3.20			3.20		
Initial Degree of Utilization, x	0.102			0.167			0.069			0.110		
Final Departure Headway, hd (s)	4.68			4.43			4.78			4.65		
Final Degree of Utilization, x	0.149			0.232			0.103			0.160		
Move-Up Time, m (s)	2.0			2.0			2.0			2.0		
Service Time, ts (s)	2.68			2.43			2.78			2.65		

Capacity, Delay and Level of Service												
Flow Rate, v (veh/h)	115			188			78			124		
Capacity	769			813			754			774		
95% Queue Length, Q ₉₅ (veh)	0.5			0.9			0.3			0.6		
Control Delay (s/veh)	8.5			8.8			8.3			8.5		
Level of Service, LOS	A			A			A			A		
Approach Delay (s/veh)	8.5			8.8			8.3			8.5		
Approach LOS	A			A			A			A		
Intersection Delay, s/veh LOS	8.6						A					