



CITY OF DELRAY BEACH
DEPARTMENT OF DEVELOPMENT SERVICES
100 N.W. 1ST AVENUE • DELRAY BEACH • FLORIDA 33444 • (561) 243-7040



TO: DEVELOPMENT SERVICES MANAGEMENT GROUP (DSMG)

FROM: Jennifer Buce, Planner

DATE: June 26, 2025

REQUEST: Consideration of a request to reduce the side street setback from the minimum 15 feet to 10 feet associated with the redevelopment at 212 Kings Lynn.

PROPERTY INFORMATION

PCN: 12-43-46-08-06-000-0570

Zoning:

Single-family Residential (R-1-AA)
Lake Ida Neighborhood Overlay District

Prior Use:

Single-Family Residence, ca. 1956

Proposed Use:

Single-Family Residence

BACKGROUND:

The request is associated with the redevelopment of the property. The original structure, built in 1956, will be demolished and replaced with a new single-family residence.

The subject property is located on the southwest corner of Aylesbury Road and Kings Lynn. The property consists of Lot 57 and a portion of Lot 58, Plat of Chevy Chase, recorded in 1953.

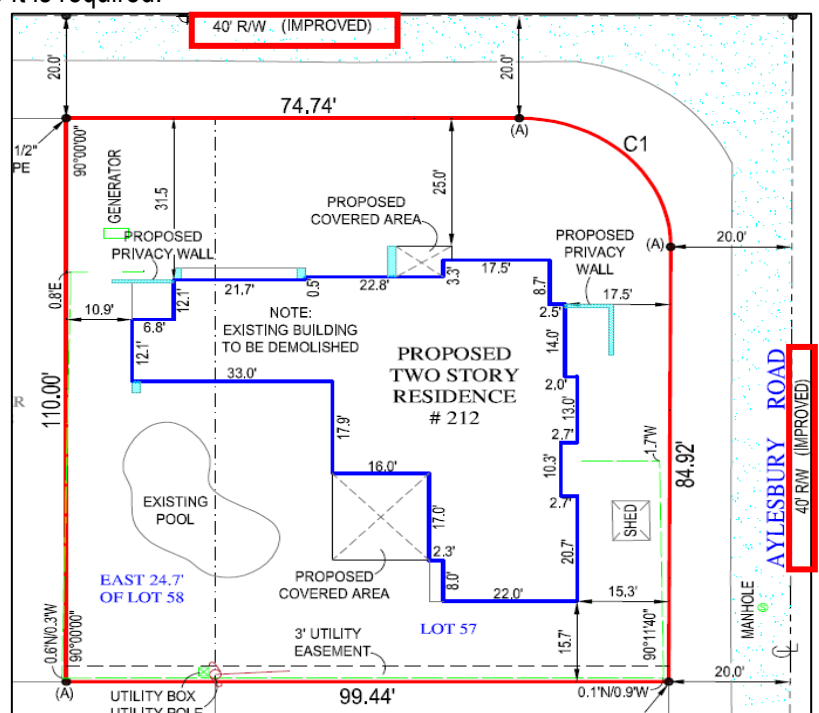


The property is located within the Lake Ida Neighborhood Overlay District where additional developmental provisions have been established to address the neighborhood's concerns regarding the mass and bulk of new construction within the neighborhood. The provisions encourage one-story structures and strive to mitigate impacts of upper stories onto adjacent properties, while accommodating the generally larger configuration of properties fronting onto Lake Ida.

<u>Lot Coverage (max.):</u>	1-story – 40%	More than 1-story – 30%
<u>Floor Area Ratio:</u>	1-story – 0.40	More than 1-story – 0.35
		0.40 with frontage on Lake Ida

Upper Story Limitations: 75% of the first floor
100% of the first floor with frontage on Lake Ida

<u>R-1-AA Setbacks:</u>	Front	1-story – 30 ft	More than 1-story – 25 ft
	Rear	10 ft	15 ft



STAFF REVIEW:

Pursuant to **LDR Section 2.1.2(B)**, the Development Services Management Group (DSMG) has the authority to grant administrative relief to adjustments to setback requirements for detached single-family and duplex structures resulting from right-of-way dedications, equal to the amount of the dedication, but no more than five feet.

A review of the existing conditions adjacent to the subject property (outlined in blue) indicates that only one property has been redeveloped (2010); the property is immediately to the north and has similar street frontage conditions. Based on the Property Appraiser's aerial, it does not appear that a right of way dedication was required. The PAPA measuring tool indicates a side-street setback of at least 20ft was provided. The property to the south was built in 1983 and fronts onto both Aylesbury Road and Beverly Drive, with Aylesbury Road as the side-street. The balance of the properties adjacent to the subject property front onto Aylesbury Road.

As redevelopment occurs along Aylesbury Road, the same right of way dedication will be required and a front setback of either 30 ft or 25 ft will be required, depending on the number of stories. Those requests will also have the ability to request a setback reduction along Aylesbury Road, assuming the current provisions remain in the LDR at the time of application.



APPLICANT JUSTIFICATION

The applicant's request is provided as an attachment.

FINDINGS

LDR Section 2.4.11(D)(5), Findings, Prior to granting administrative relief, the administrative official or body shall find:

- (a) That the relief sought is consistent with the specific authorization provided for in these regulations;
- (b) That the intent of the affected regulation is preserved;
- (c) That the action will not be detrimental to the public health, safety, or welfare; and,
- (d) The relief is consistent with the established character of the surrounding neighborhood.

While this is the first residence along Aylesbury Road and Kings Lynn to provide a right of way dedication, the LDR specifies that when new development is proposed and there is no dedication, it must meet the goals and objectives in *Table MBL – Street Network Classification and Improvements*. The prescribed right of way widths have been determined as appropriate as the best way to provide and promote public safety and welfare; provide stormwater management, adequate area for street trees; and to assure adequate access, circulation, and parking in high intensity use areas.

Consideration of the whether the intent of the affected regulations (side-street setback requirement) is preserved is necessary. The setback requirement along side-streets is smaller than those for the front, as both can be utilized for access, but is intended to maintain appropriate visibility along the streetscape. Visibility is imperative to minimize conflicts between vehicles entering or existing a property and other vehicles, bicyclists, or pedestrians. The LDR requires a minimum sight visibility triangle of 10 feet along the driveway and property line. LDR Section 4.6.14, Sight Visibility, specifies, “the objective of these requirements is to provide sufficient view corridors for safe interaction of vehicles and pedestrians at intersections.” Therefore, when parked in the garage, the vehicle will only have ten feet before crossing into the right of way. Lake Ida residents commonly utilize the road network for recreational purposes, including walking and biking. However, it is an area where there is not a sidewalk network, requiring pedestrians, bicyclists, and vehicles, to share the same travel way.

Further, consideration regarding consistency with the established character of the surrounding neighborhood is to be considered. The intent of the additional regulations when the Lake Ida Neighborhood Overlay District was established was to manage the mass and scale of new construction that was impacting the neighborhood character. Reducing the side-street setback, although consistent with the existing setback, and the required right-of-way dedication, is contrary to the intent of the additional provisions required for the neighborhood, particularly on a parcel that exceeds the minimum lot dimensions for the R-1-AA zoning district.

REVIEW PROCESS

If the request is approved and no other relief is requested, the property owner may proceed with the submittal of a building permit application illustrating a side-street setback of 10 feet along Aylesbury Road.

If the request is denied, the plans must reflect both the minimum required side-street setback of 15 feet and the required five-foot right-of-way dedication or an appeal can be requested in accordance with LDR Article 2.5, Appeals. Appeals of the Development Services Management Group (DSMG) are considered by the City Commission for final determination.