



DEVELOPMENT SERVICES

BUILDING | HISTORIC PRESERVATION | PLANNING & ZONING

PLANNING AND ZONING BOARD STAFF REPORT

Delray Hyundai Genesis

Meeting	File No.	Application Type
October 20, 2025	2025-065	Level 3 Site Plan
Property Owner	Authorized Agent	
ABC JS Auto Imports LLC	Covelli Design Associates	

Request

Consideration of a Level 3 Site Plan Application with Architectural Elevations and Landscape Plan for an approximately 167,000 square foot full-service automotive dealership at 2612-2650 N. Federal Highway.

Site Data & Information

Location: 2612 – 2650 N. Federal Highway

PCN: 12-43-46-04-08-000-0160 + 12-43-46-04-08-000-0142

Property Size: 3.22 acres / 140,227 sq. ft.

Land Use: General Commercial (GC)

Zoning: Automotive Commercial (AC)

Adjacent Zoning:

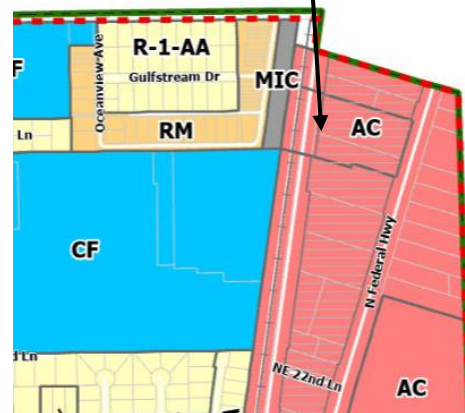
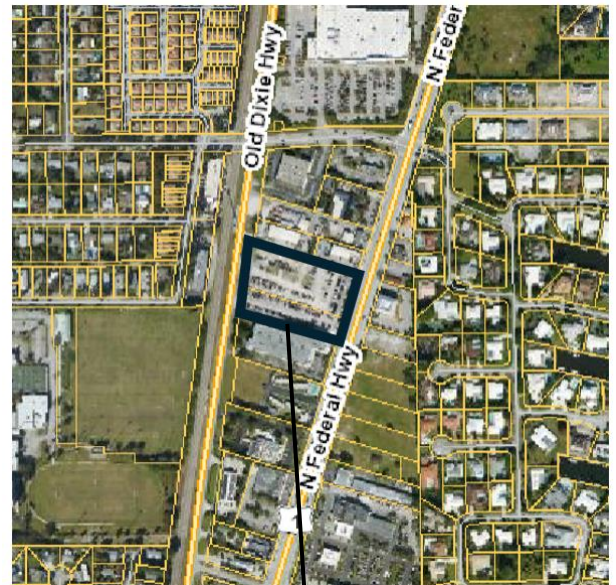
- **North, South, East:** General Commercial (GC)
- **West:** Mixed Industrial and Commercial (MIC)

Existing Use: Vacant; previously a used automotive sales and auto storage lot

Proposed Land Use: Commercial – full-service automotive dealership

Floor Area Ratio (FAR):

- **Existing:** NA
- **Proposed:** 1.19 FAR
- **Maximum Allowed:** 3.0 FAR



Background Information

The subject property - currently vacant - previously consisted of an automotive dealership known as Delray Auto Mall, which ceased operation at the location in 2019. Subsequently, the property was used as inventory storage for neighboring dealerships; and, most recently, a Zoning Certificate of Use (ZCU) was approved for an Automotive Dealership at the subject property in 2019. The previously existing structures on site were demolished in 2024. The property is zoned Automotive Commercial (AC) with an underlying Land Use Map (LUM) designation of General Commercial (GC). The zoning designation and compatible land use classification indicate that the subject property has been identified as an appropriate location for further development of auto-oriented commercial uses.

Project Description



The proposed development is a two-story full-service automotive dealership with a four-story parking garage. The total gross square footage is 167,032 square feet. Components of the use include showrooms and service areas for two distinct brands (Genesis and Hyundai), administrative offices, and vehicle inventory bullpens.

The project is well within intensity thresholds with a proposed FAR of 1.19, whereas a maximum of 3.0 is permitted. Likewise, the development complies with height and setback regulations as established in the AC district – except where specifically discussed in the following sections.

The architectural style proposed is contemporary and is reflective of the national brand prototyping. The centralized building is wrapped on all sides by vehicular use area (parking, drive-aisles, etc.) and the minimum required landscape buffering adjacent to vehicular use area, including the increased 10-foot buffer adjacent to Federal Highway, is provided along the full extent of the site.

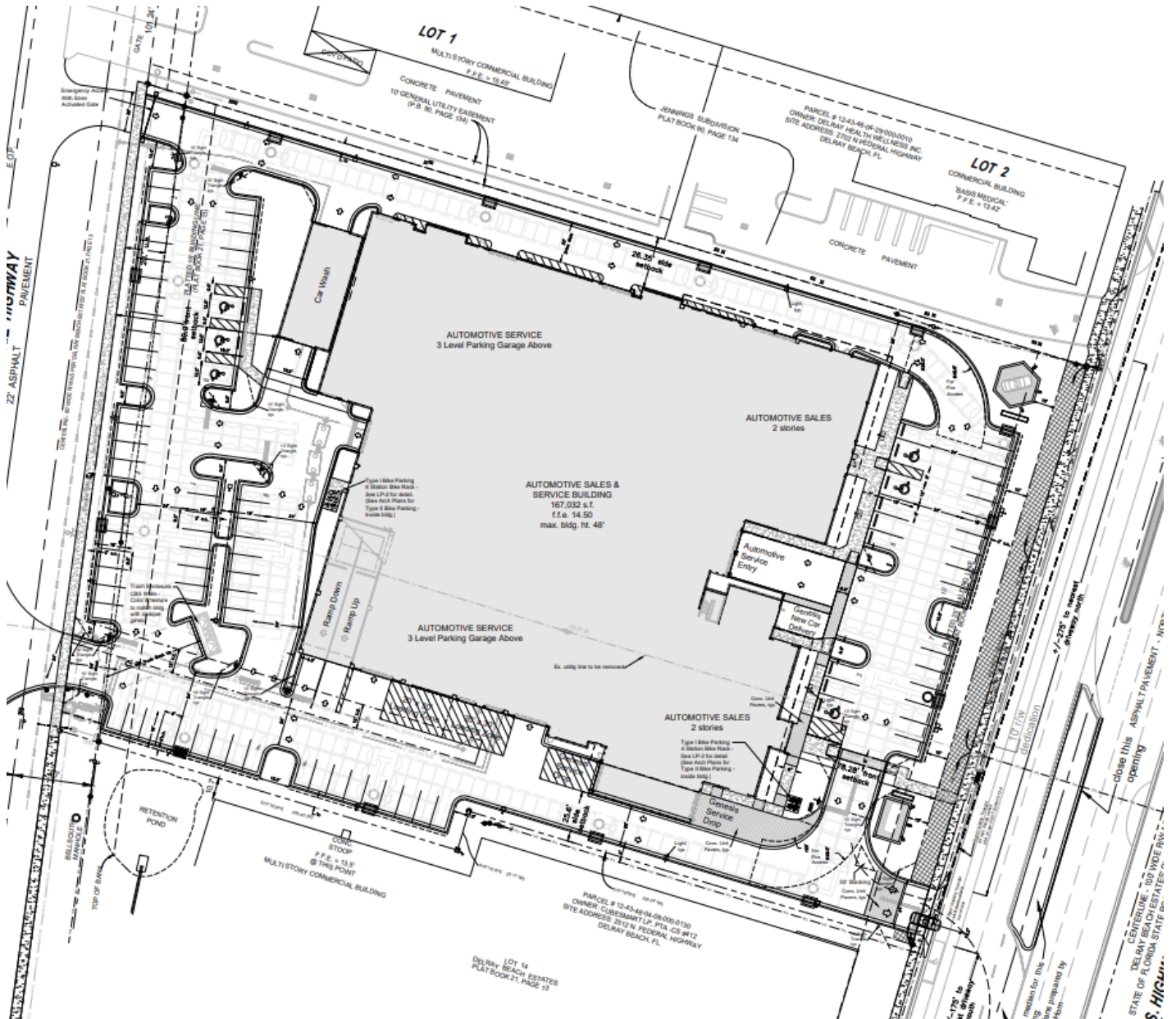
Parking is provided via a surface lot (88 spaces) and parking garage (237 spaces) for a total of 325 spaces. 153 of these spaces are required to remain allocated for customer and employee parking and any surplus parking may be used for vehicle inventory storage.

Relief is also requested as part of this application:

1. Relief from LDR 4.4.10(F)(3) to allow a front setback of 78 feet in the North Federal Highway Overlay District, whereas the maximum allowable setback is 15 feet.
2. Relief from LDR Section 4.3.4(K) to allow 11 percent open space, whereas a minimum of 25 percent is required.

A full analysis of the relief will be discussed in subsequent sections.

Review and Analysis: Site Plan



(Full-sized site plan available as an attachment)

LDR Section 2.4.10(A)(3), Findings. All site plan applications require compliance with the applicable regulations and review criteria and shall be consistent with the Comprehensive Plan and other local ordinances.

(b) Level 2, Level 3, and Level 4 Site Plan applications require compliance with the findings in Chapter 3, Performance Standards.

The Site Plan was reviewed according to the performance standards for site plan actions listed in **LDR Section 3.2.3** and these standards have been factored into the technical review of the application. The intent of the standards is to ensure that certain essential criteria and a level of quality are adhered to with regard to new development. The project complies with the performance standards.

LDR Section 3.1.1, Required Findings. Prior to the approval of development applications, certain findings must be made in a form which is part of the official record. This may be achieved through information on the application, the staff report, or minutes. Findings shall be made by the body which has the authority to approve or deny the development application.

These findings relate to the following four areas:

(A) Land Use Map. *The resulting use of land or structures must be allowed in the zoning district within which the land is situated and said zoning must be consistent with the applicable land use designation as shown on the Land Use Map.*

AC is a zoning district deemed compatible with the GC land use designation per the comprehensive plan. The intent for this property to be developed as an automotive-oriented commercial use is clearly consistent and compatible with the land use designation.

(B) Concurrency. *Concurrency as defined by Objective NDC 3.1 of the Neighborhoods, Districts, and Corridors Element of the adopted Comprehensive Plan must be met and a determination made that the public facility needs, including public schools, of the requested land use and/or development application will not exceed the ability of the City and The School District of Palm Beach County to fund and provide, or to require the provision of, needed capital improvements in order to maintain the Levels of Service Standards established in Table CIE, Level of Service Standards, of the Capital Improvements Element of the adopted Comprehensive Plan of the City of Delray Beach.*

Water and Sewer. The development will connect to existing water and sewer networks as approved by the City Utilities Department.

Drainage. Drainage will be accommodated on site. All stormwater runoffs will be collected and contained within the subject property through a drainage system approved by the City Engineer. A significant network of underground drainage storage containers is proposed below the surface parking area (reference Civil sheet C4)

Transportation. The provided Palm Beach County TPS approval letter indicates the proposed project will not have a significant impact on the existing road network. Therefore, the project meets transportation concurrency. An increase of 72 AM and 94 PM peak hour trips is anticipated.

Solid Waste. The Solid Waste Authority has indicated that its facilities have sufficient capacity to accommodate all development proposals until 2054.

Schools. Not Applicable.

(C) Consistency. *A finding of overall consistency may be made even though the action will be in conflict with some individual performance standards contained within Article 3.2, provided that the approving body specifically finds that the beneficial aspects of the proposed project (hence compliance with some standards) outweighs the negative impacts of identified points of conflict.*

LDR Section 3.2.1, Basis for determining consistency.

The performance standards set forth in this Article either reflect a policy from the adopted Comprehensive Plan or a principle of good planning practice. The performance standards set forth in the following sections as well as compliance with items specifically listed as required findings in appropriate portions of Section 2.4.5 shall be the basis upon which a finding of overall consistency [Section 3.1.1(C)] is to be made. However, exclusion from this Article shall not be a basis for not allowing consideration of other objectives and policies found in the adopted Comprehensive Plan in the making of a finding of overall consistency.

The following Comprehensive Plan objectives and policies are relevant to the request:

Neighborhoods, Districts, and Corridors Element

Objective NDC 1.3 *Apply the mixed-use land use designations of Commercial Core, General Commercial, Transitional, Congress Mixed-Use, and Historic Mixed-Use to accommodate a wide range of commercial and residential housing opportunities appropriate in scale, intensity, and density for the diverse neighborhoods, districts, and corridors in the city.*

Policy NDC 1.1.2 *Provide a complementary mix of land uses, including residential, office, commercial, industrial, recreational, and community facilities, with design characteristics that provide: Similar uses, intensity, height, and development patterns facing each other, especially in residential neighborhoods. Uses that meet the daily needs of residents. Public open spaces that are safe and attractive.*

Objective 2.5 Automotive Dealerships *Recognize that automotive dealers and auto-related uses are local legacy industries with unique impacts that require appropriate and strategic locations.*

Policy NDC 2.5.1 *Accommodate automotive dealerships and auto-related uses on land with a General Commercial land use designation, zoned Automotive Commercial, and located north of George Bush Boulevard, between North Federal and Dixie Highways.*

The proposed development maintains consistency and compatibility with the comprehensive plan being that it is located on a parcel specifically in an area where policy NDC 2.5.1 identifies as most appropriate for accommodating auto dealerships.

LDR Section 3.2.3, Standards for Site Plan Action

The standards for site plan action include adequate traffic circulation and accessibility for vehicles and pedestrians, compatibility with adjacent land uses, intensity thresholds, and energy efficiency and sustainability requirements, such that the proposed project will not result in the degradation of the surrounding area. The development proposes a use that is contextually appropriate for the area and aligns with the intent and permitted uses of the Automotive Commercial zoning district and the comprehensive plan; the proposed site plan meets the standards for site plan action.

(D) Compliance with the LDR. *Whenever an item is identified elsewhere in these Land Development Regulations (LDRs), it shall specifically be addressed by the body taking final action on a land development application/request. Such items are found in Section 2.4.5 and in special regulation portions of individual zoning district regulations.*

LDR Section 4.4.10, Automotive Commercial; and 4.3.4, Base District Development Standards

Standard/Regulation	Review	
Height 4.3.4(K)	Maximum: 48 feet Proposed: 45 feet	
Setbacks 4.3.4(K)	Minimum Required Front – 5 feet minimum; 15 feet maximum Rear – 10 feet Interior – 0 feet	Provided: Front – 78 feet; <u>relief required</u> Rear – 81 feet Interior – 26 feet
Landscape Buffer 4.3.4(H)(6)(b)(2.)	Minimum Required Federal Hwy: 10 feet	Provided: Federal Hwy: 10 feet

Other Development Regulations

Standard/Regulation	Review	
4.6.9 – Off-Street Parking (Four Spaces per 1000 sq. ft. of gross building area less display area)	Required: 192 Provided: 325	
4.6.9, Bicycle Parking Table 4.6.9(C)-1	Required: Type I spaces – 5 Type II spaces – 4 Provided: Type I spaces – 10 Type II spaces with showers – 4 spaces and 4 showers	
	Type I Bicycle Parking. Intended to be used for short term bicycle parking by visitors. Six spaces per 25,000 sq. ft. of building and parking area are required.	

	Type II Bicycle Parking. Required at certain thresholds and provides long term storage options for people who reside, work, or are visiting for an extended duration. Two plus two spaces per 50,000 sq. ft. of gross floor area, and two showers plus two per 50,000 sq. ft. of gross floor area.
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Front Setback Relief

The North Federal Highway Overlay District establishes a minimum and maximum setback to facilitate the redevelopment of a commercial corridor where the buildings frame the street for a more pedestrian-scaled appearance. This standard is intended to deter strip-style commercial centers with parking in front of buildings, which creates a corridor that is better framed for the pedestrian and less dominated by automotive aspects of the use. As such, a minimum and maximum setback are established for the district ensuring that buildings are no closer than 5 feet or further than 15 feet from the front property line.

However, it is understood that in certain contexts it may be difficult for certain development typologies (including automotive dealerships) to adhere to this setback requirement. Therefore, **LDR section 4.4.10(F)(3)** allows for the approving body to determine that a setback greater than 15 feet may be proposed if it *can be demonstrated to the approving body that it is not feasible to comply with this standard*.

The applicant proposes a front setback of 78 feet. The area within the front setback is used for additional parking, a drive aisle for vehicle circulation, and ingress to the service center. While the parking located in this area is surplus to LDR requirement, the drive aisle provides necessary circulation and navigability intra-site (including required Fire accessibility).

The Board shall consider the Applicant's justification (attached) when evaluating this request. Staff acknowledges that when a use is entirely auto-oriented in its function, it is difficult to mitigate for the visual impacts of the automotive use that would otherwise be avoided with typical commercial development (i.e. parking located behind buildings, etc.). Likewise, it is customary and expected for vehicle display areas to be located within clear visibility of the primary commercial corridor adjacent to a dealership.

Open Space Reduction

LDR Section 4.3.4(K) requires that development sites in AC provide a minimum of 25 percent of the total site area as open space. The Applicant instead proposes an open space amounting to 11 percent of the total site area. However, **LDR Section 4.4.10(H)(4)** allows for a reduction in open space in specific contexts.

Specifically, Whenever the requirement to bring an existing site into full compliance triggers a requirement to increase the percentage of open space pursuant to Section 4.3.4(K), a reduction of that requirement may be approved by the approving body; however, in no instance shall the percentage be reduced below the existing percentage of open space. Concurrent with granting a reduction, the Board must find that such reduction does not diminish the practical application of the requirement, particularly as it relates to the perimeter of the site.

Even though this is a new construction application, because the previous use existing on site was an automotive dealership, the provision in 4.4.10(H)(4) has been determined to be applicable. As such, the Board holds the ability to grant a reduction in open space below 25 percent, so long as the reduction is not greater than the previously existing condition. The Applicant has provided exhibits documenting the existing open space on site as being approximately 1 percent of the overall site area. Therefore, because the requested relief is still greater than what previously existed, the Board may approve pursuant to the referenced code section if it is deemed that the practical application of the regulation is still sufficiently met. It is important to note that the minimum required perimeter buffer is provided along all sides of the property.

Review & Analysis: Landscape Plan

LDR Section 2.4.10(A)(3)(c), Findings

Landscape Plans, including modifications to existing landscaping, shall be consistent with Section 4.6.16, Landscape Regulations.

A technical review of this site plan has been performed, and a determination has been made that the project complies with all applicable landscape regulations. Landscaping is interspersed throughout the site in a way that is appropriate for the use and contextual setting,

where foundation landscaping, landscape islands, and a robust landscape buffer help beautify and soften the starkness and vastness of the automotive use.

Nine existing trees will be removed including seven palms replaced on a one-to-one basis, one invasive species tree that is exempt from mitigation, and one 4" dbh canopy tree that is replaced with another 4" dbh tree. In total, 80 new trees and 24 new palms are proposed, well exceeding the amount of mitigation required.

A full landscape plan including the tree disposition plan is available as an attachment.

Review & Analysis: Architectural Elevations

LDR Section 2.4.10(A)(3)(d), Findings

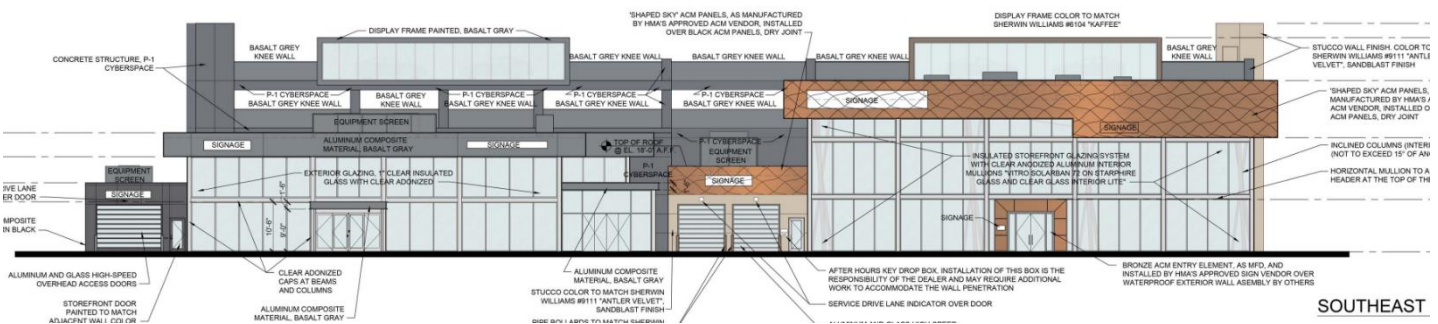
Architectural Elevations, including modifications to existing building facades, require an overall determination of consistency with the objectives and standards of Section 4.6.18, Architectural Elevations and Aesthetics, and any adopted architectural design guidelines and standards, as applicable.

LDR Section 4.6.18, Architectural Elevations and Aesthetics

(E), Criteria for board action

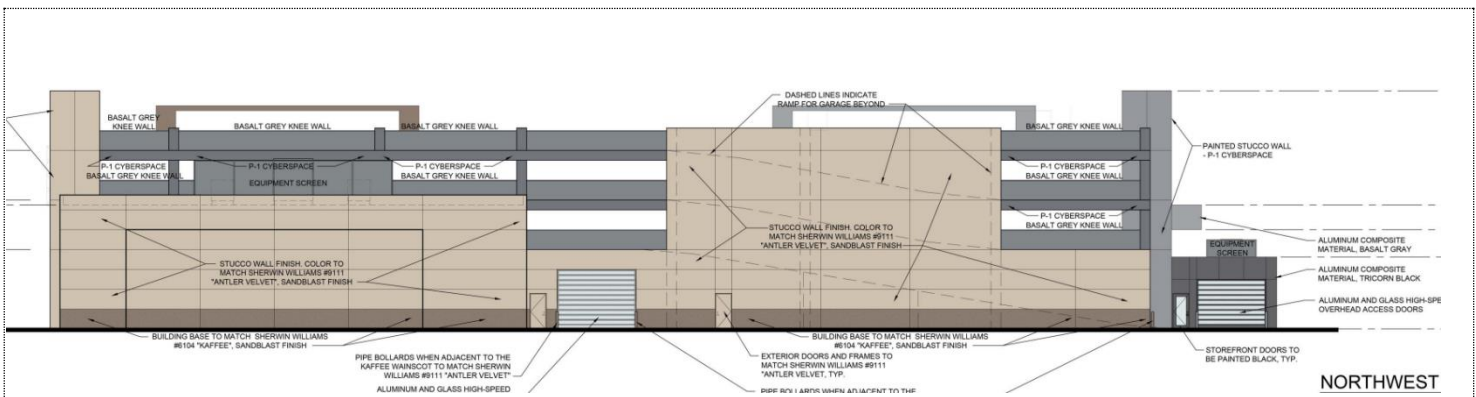
The following criteria shall be considered, by the Site Plan Review and Appearance Board or Historic Preservation Board, in the review of plans for building permits. If the following criteria are not met, the application shall be disapproved.

- 1. The plan or the proposed structure is in conformity with good taste, good design, and in general contributes to the image of the City as a place of beauty, spaciousness, harmony, taste, fitness, broad vistas, and high quality.*
- 2. The proposed structure, or project, is in its exterior design and appearance of quality such as not to cause the nature of the local environment or evolving environment to materially depreciate in appearance and value.*
- 3. The proposed structure, or project, is in harmony with the proposed developments in the general area, with the Comprehensive Plan, and with the supplemental criteria which may be set forth for the Board from time to time.*



Staff finds that the design criteria are met. The contemporary design is aligned with the national brand prototype. The façade integrates material and texture variation to create interest, and there are various points of vertical articulation and shifts in the massing that move the design beyond what could otherwise be a repetitive and bland product.

Staff worked with the Applicant during technical review to facilitate an improved exterior to the parking garage, and while improvements have been made through the introduction of raised stucco and scoring, as well as color palette variation, more interesting screening materials could still be introduced to the garage to allow it to match the quality of the front façade.



Options for Board Action

- Move **approval** of a Level 3 Site Plan Application request for **2612 – 2650 N Federal Hwy**, including Architectural Elevations and Landscape Plan, to construct an approximately 167,000 sq. ft. full-service automotive sales use, by finding that the request is consistent with the Land Development Regulations and the Comprehensive Plan.
- Move **approval, as amended** of a Level 3 Site Plan Application request for **2612 – 2650 N Federal Hwy**, including Architectural Elevations and Landscape Plan, to construct an approximately 167,000 sq. ft. full-service automotive sales use, by finding that the request is consistent with the Land Development Regulations and the Comprehensive Plan, **subject to conditions**.
- Move **denial** of a Level 3 Site Plan Application request for **2612 – 2650 N Federal Hwy**, including Architectural Elevations and Landscape Plan, to construct an approximately 167,000 sq. ft. full-service automotive sales use, by finding that the request is inconsistent with the Land Development Regulations and the Comprehensive Plan.
- Continue with direction.**

Public and Courtesy Notices

No Public Notice was required by the LDR for this application.

TAC Review Timeline

Review No.	Submittal Date	TAC Comments Transmitted	Total time with Applicant: 101 days Total time under review: 117 days
1	11/25/2024	1/10/2025	
2	1/28/2025	2/26/2025	
3	4/28/2025	5/20/2025	
4	6/11/2025	7/2/2025	