



Planning, Zoning and Building Department

BOARD ACTION REPORT – APPEALABLE ITEM

Project Name: Seagate Villas
Project Location: 1017 Bucida Road
Request: Class V Site Plan, Landscape Plan and Architectural Elevation Plan for the demolition of an existing residential structure and construction of a new three-story townhouse building containing three units.

Board: Site Plan Review and Appearance Board
Meeting Date: April 27, 2016

Board Action:

Approved the Site Plan and Architectural Elevation Plan on a 6 to 0 vote (Jim Knight absent) with conditions. The Landscape Plan was approved on a 6 to 0 vote (Jim Knight absent) with no conditions.

Project Description:

The property is located at the northwest corner of Venetian Drive and Bucida Road and measures 0.34 acres. The property contains a single family residence that was constructed in 1950. The subject property is zoned RM (Medium Density Residential) and has a Future Land Use Map Designation of MD (Medium Density Residential 6-12 du/ac).

The development proposal involves the demolition of the existing single family residential structure and construction of a 3-story townhouse structure that accommodates three units. Each individual unit is a four-bedroom townhome (the under air square footages for Unit "A" is 3,720 sq. ft., Unit "B" is 3,996 sq. ft. and Unit "C" is 3,747 sq. ft.). Two of the units will accommodate a two-car garage with the third unit having a three car garage. Each unit will be three stories high with a mean roof height of 34' 11½". Grade level swimming pools will be constructed for the two end units (Units "A" and "C"). A second story plunge pool will be provided for the interior unit (Unit B). Installation of associated landscaping, paverbrick walkways and driveways and the construction of a 5' wide sidewalk along Bucida Road and Venetian Drive are also proposed.

Board Comments:

All Board members liked the architectural elevations and design of the project and felt it would fit nicely with the existing neighborhood. The Board added the following conditions of approval.

- That the proposed generator is removed from the site plan and addressed during the building permit process.
- That the photometric plan is provided and worked out with staff.

Public Comments: There were no public comments.

Associated Actions: All required actions were taken with the Board's approval.

Next Action: The SPRAB action is final unless appealed by the City Commission.

Attachment:

- Site Plan Review and Appearance Board Staff Report-April 27, 2016

SITE PLAN REVIEW AND APPEARANCE BOARD

CITY OF DELRAY BEACH

---STAFF REPORT---

MEETING DATE: April 27, 2016

ITEM: Class V Site Plan, Landscape Plan and Architectural Elevations for Seagate Villas, associated with the demolition of an existing residential structure and construction of a new three-story townhouse building containing three-units.

RECOMMENDATION: Approve

GENERAL DATA:

Owner..... Philip F and Mary Renaud

Applicant..... Seagate Villas

Location..... Northwest corner of Venetian Drive and Bucida Road

Property Size..... 0.3414

Future Land Use Map..... MD (Medium Density Residential 6-12 du/ac)

Current Zoning..... RM (Medium Density Residential)

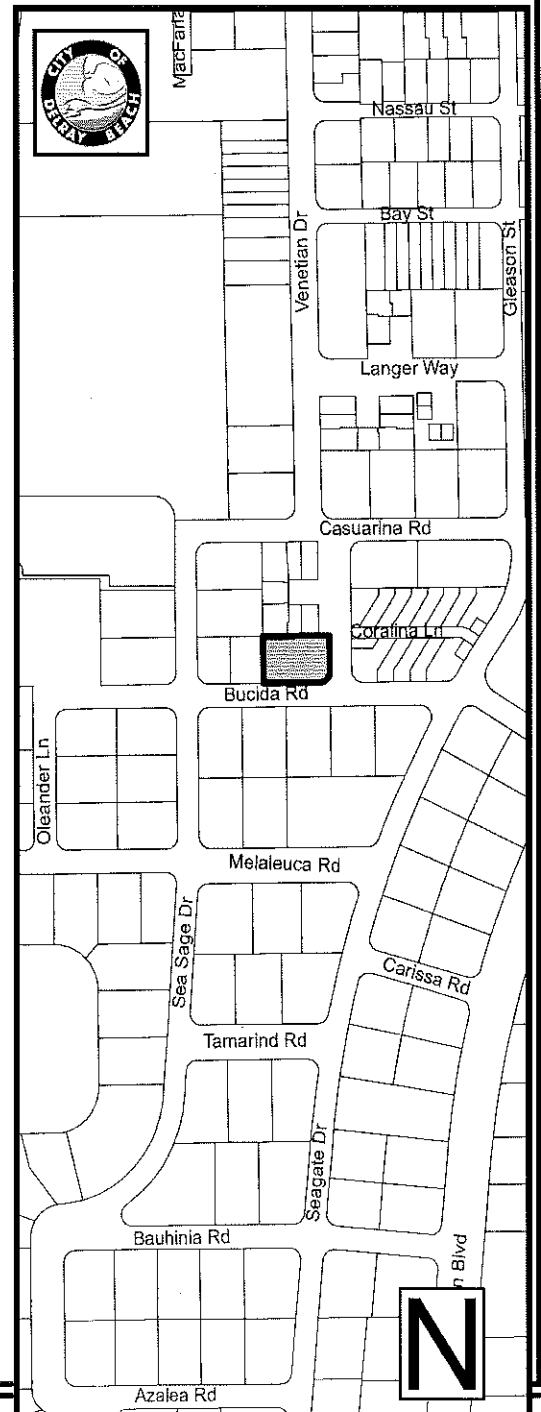
Adjacent Zoning..... North: RM (Medium Density Residential)
East: RM (Medium Density Residential)
West: RM (Medium Density Residential)
South: R-1-AAA (Single Family Residential)

Existing Land Use..... Single Family Residence

Proposed Land Use..... Construct 3 new residential townhouse units with Associated Parking and Landscaping.

Water Service..... Available via connections to an existing 8" water main within Bucida Road.

Sewer Service..... Available via service lateral connections to an existing 6" sewer main within Bucida Road.



ITEM BEFORE THE BOARD

The action before the Board is approval of a Class V site plan request for **Seagate Villas**, pursuant to LDR Section 2.4.5(F). The request involves the following elements:

- ☐ Site Plan;
- ☐ Landscape Plan; and
- ☐ Architectural Elevations.

The property is located at the northwest corner of Venetian Drive and Bucida Road (1017 Bucida Road).

BACKGROUND

The subject property consists of Lot 3, Block 13, Seagate Section "A", according to the Plat thereof as recorded in Plat Book 20, Page 48, of the Public Records of Palm Beach County, Florida. The property contains a single family residence that was constructed in 1950. The overall development measures 0.34 acres. The subject property is zoned RM (Medium Density Residential) and has a Future Land Use Map Designation of MD (Medium Density Residential 6-12 du/ac).

On January 25, 2016, a Class V site plan application was submitted for the construction of a three unit townhouse development and is now before the Board for action.

PROJECT DESCRIPTION

The development proposal consists of the following:

- Demolition of the existing single family residential structure.
- Construction of a 3-story townhouse structure that accommodates three units. Each individual unit is a four bedroom town home (the under air square footages for Unit "A" is 3,720 sq. ft., Unit "B" is 3,996 sq. ft. and Unit "C" is 3,747 sq. ft.). Two of the units will accommodate a two-car garage with the third unit having a three car garage. Each unit will be three stories high with a mean roof height of 34'11 1/2"
- Installation of 6' high privacy walls with aluminum entry gates.
- Grade level swimming pools will be constructed for the two end units. A roof top plunge pool will be provided for the interior unit. The grade level swimming pool is proposed within the side street setback for Unit "A" and within the rear setback for Unit "C".
- Installation of associated landscaping, paverbrick walkways and driveways.
- Construction of a 5' wide sidewalk along Bucida Road and Venetian Drive.

SITE PLAN ANALYSIS

COMPLIANCE WITH THE LAND DEVELOPMENT REGULATIONS:

Items identified in the Land Development Regulations (LDR) shall specifically be addressed by the body taking final action on the site and development application/request.

LDR Section 4.4.6 RM Zone District:

Pursuant to LDR Section 4.4.6 RM Zone District (A) Purpose and Intent: The Medium Density Residential (RM) District provides a residential zoning district with flexible densities having a base of six (6) units per acre and a maximum of twelve (12) units per acre for this property. The actual density of a particular RM development is based upon its ability to achieve certain performance standards which are intended to mitigate the impacts of the increased density and ensure that the project is compatible with surrounding land uses. Further, the Medium Density Residential District provides for implementation of those objectives and policies contained within the Housing Element of the Comprehensive Plan which call for accommodating a variety of housing types.

LDR Section 4.4.6(l) Performance Standards:

Pursuant to LDR Section 4.4.6(l)(1) The following standards shall apply to all site plans approved subsequent to October 7, 1997, and for modifications to existing developments which involve the creation of additional residential units. In order to increase a project density beyond six units per acre, the approving body must make a finding that the development substantially complies with the performance standards listed in this section. The intent of the standards is to mitigate the impacts of the additional density both internal and external to the site. The extent to which a project meets the standards will determine the number of units per acre that will be permitted. For example, if a project meets or exceeds all of the standards, and is otherwise consistent with applicable standards and policies of the City's Comprehensive Plan and Land Development Regulations, the maximum density is permitted. Projects which only partially achieve these standards will be permitted a correspondingly lower density. The performance standards are as follows:

The density of the overall development is 8.82 units per acre, which requires a finding of substantial compliance with all applicable Performance Standards since it exceeds six units per acre. The following are the specific Performance Standards with an analysis of each:

- a) **The traffic circulation system is designed to control speed and reduce volumes on the interior and exterior street network. This can be accomplished through the use of traffic calming devices; street networks consisting of loops and short segments; multiple entrances and exits into the development; and similar measures that are intended to minimize through traffic and keep speeds within the development at or below 20 m.p.h.**

The proposed development does not contain an interior street network and no changes are proposed to the existing surrounding street network. The proposed development would not generate an adverse impact on the surrounding road network. Therefore, this performance standard does not apply.

- b) **Buildings are placed throughout the development in a manner that reduces the overall massing, and provides a feeling of open space.**

The townhouse units are arranged on the lot with the garages fronting on both Venetian Drive and Bucida Road. The proposed units provide significant plane changes, which reduces the massing of the units from the public rights-of-way (see elevation sheet A-301 and A 301F). The proposed variation in roof lines along with the plane changes provide a

feeling of open space and reduce the overall massing of the development. Based on the above, a finding can be made that the proposed in-fill development substantially complies with this performance standard.

- c) **Where immediately adjacent to residential zoning districts having a lower density, building setbacks and landscape materials along those adjacent property lines are increased beyond the required minimums in order to provide a meaningful buffer to those lower density areas. Building setbacks are increased by at least 25% of the required minimum; at least one tree per 30 linear feet (or fraction thereof) is provided; trees exceed the required height at time of planting by 25% or more; and a hedge, wall or fence is provided as a visual buffer between the properties.**

The subject property is bordered on the north, east and west by RM zoning. To the south, (south side of Bucida Road) the properties are zoned R-1-AAA (Single Family Residential). As the subject parcel is not located immediately adjacent to the R-1-AAA but is separated by Bucida Road, this performance standard does not apply.

- d) **The development offers a varied streetscape and building design. For example, setbacks are staggered and offset, with varying roof heights (for multi-family buildings, the planes of the facades are offset to add interest and distinguish individual units). Building elevations incorporate diversity in window and door shapes and locations; features such as balconies, arches, porches, courtyards; and design elements such as shutters, window mullions, quoins, decorative tiles, etc.**

The proposed development consists of a 3 unit townhouse building, designed with various planes on the façades facing the rights-of-way. The applicant has indicated the architectural style of the townhouses is a representation of the transitional Caribbean (tropical) style of architecture with some traditional influence. The roofline is a combination of gambrel and hip roof which includes flat cement tiles. Diversity among the windows will be accomplished with the provision of four different window shapes. The front (east) elevation of the building contains the entry to Unit "A" with the remaining unit entries facing Bucida Road (south elevation). The setback of the third story reduces the massing of the building. The utilization different building materials for the walls (smooth stucco and wall cladding), the upper story balconies (clear glass and aluminum railings), the window treatments (bahama shutters and stucco banding), add interests to the elevations. For an illustration, see sheet A-301 and 301F. Based on the above analysis, a finding can be made that the development is consistent with this performance standard.

- e) **A number of different unit types, sizes and floor plans are available within the development in order to accommodate households of various ages and sizes. Multi-family housing will at a minimum have a mix of one, two and three bedroom units with varying floor plans. Single family housing (attached and detached) will at a minimum offer a mix of three and four bedroom units with varying floor plans.**

As noted previously, the 3-unit fee simple townhouse development offers three options of a 4-bedroom unit. While the in-fill development has provided a variety of floor plans, a mix of one, two and three bedroom units has not been provided. The provision of a mix of bedrooms is problematic for smaller in-fill development. Therefore, this performance standard is not applicable.

- f) The development is designed to preserve and enhance existing natural areas and/or water bodies. Where no such areas exist, new areas which provide open space and native habitat are created and incorporated into the project.**

The site contains an existing single family residence and contains no natural areas on site to be preserved. The proposal involves the redevelopment of property located within an urban in-fill area. Thus, compliance with the standard is not applicable.

It is however noted that the subject property is heavily vegetated and contains a variety of Palm trees including, Coconut Palm, Queen Palm, Sabal Palm, Areca Palm Clusters, Alexander Palm Cluster, Royal Palm, Traveler Palm Clusters, Citrus Species, Hibiscus, Soft Tip Yucca, Weeping Fig and Strangler Fig. All of the trees are to be removed, except the Royal Palm (which will be relocated) and the Alexander Cluster and Strangler Fig (which will remain). Extensive landscaping is proposed and is addressed under the landscape section of the report.

- g) The project provides a convenient and extensive bicycle/pedestrian network, and access to available transit.**

The development proposal includes the construction of public sidewalks along both Venetian Drive and Bucida Road. It should be noted that there are no existing sidewalk immediately to the north along Venetian, however, as redevelopment occurs it is anticipated that the provision of sidewalks will be constructed and ultimately provide access to Atlantic Avenue and mass transit. Based on the above, a finding can be made that the development substantially complies with this performance standard.

Summary

In conclusion, a finding of substantial compliance can be made with regard to performance standards (b), (d), and (g). Performance standard (a), (c), (e) and (f) are not applicable to the proposed development. Therefore based on the above, positive findings can be made with respect to all three of the applicable performance standards and the performance standard requirements of the RM zoning district will have been adequately addressed to support the proposed overall density of 8.82 units per acre.

LDR Section 4.4.6 (H) (Special Regulations - RM zone district):

Pursuant to LDR Section 4.4.6(H)(3) recreational areas shall be required for all new rental apartment developments, and of owner occupied developments which have homeowner associations that must care for retention areas, private streets, or common areas. New developments must include recreational features that are designed to accommodate activities for children and youth of all age ranges. Tot lots are appropriate for toddlers; features such as a basketball court, volleyball court, and open playfields are appropriate for older children. A pool and clubhouse, unless specifically designed for children, is not considered to meet this requirement. Projects having fewer than twenty-five (25) units may be exempted from this standard where it is determined by the approving body that it is not practical or feasible to comply.

This development will consist of three fee-simple townhouses with a homeowners association. As there are only 3 units it is not practical to require a common tot lot or recreation area/swimming pool. It is noted that the proposal includes the construction of individual

swimming or plunge pools for each unit. Based upon the above, compliance with this code requirement has been achieved

LDR Section 4.3.3(O) (Townhouse Design Standards):

As described below, the development proposal complies with the design standards for townhouse developments outlined in LDR Sections 4.3.3(O)(4)(a) through (d):

- a) No more than two townhouses may be constructed without providing a front setback of no less than 4' offset front to rear.**

The purpose of this requirement is to prevent a long linear appearance. The front setback for the proposed development is along Venetian Drive and only Unit "A" utilizes the front setback area. Units "B" and "C" is sited along the side street thus this requirement does not apply.

- b) No townhouse row shall consist of more than 8 units or a length of 200'.**

The townhouse row consists of 3 units and is 99' 4" in length, thus meeting this code requirement.

- c) Service features, garages, parking areas, and entrances to dwelling units shall, whenever possible, be located on a side of the individual lot having access to the interior street. Walkways should be designed to connect dwelling units with each other and connect each dwelling unit with common open space.**

This standard applies to large townhouse developments, and does not apply in this in-fill situation.

- d) Not less than 25% of the total area, less water bodies, shall be usable open space, either for recreational or some other suitable purpose, public or private.**

The basis for this standard is to assure that larger residential developments provide adequate recreational amenities. With smaller developments, provision of these amenities is not considered financially feasible, and the required contribution of \$500.00 per unit (park impact fee) is considered adequate. It is noted, however, that 35% of the site will be landscape/open space area.

Platting:

LDR Section 4.3.3(O)(2) (Plat required) states that each townhouse, or townhouse type development shall be platted. The plat must be recorded prior to issuance of any building permits, and is attached as a condition of approval.

LDR Section 4.3.4(K) Development Standards Matrix:

The following table indicates that the proposal complies with LDR Section 4.3.4(K) as it pertains to the Medium Density Residential (RM) zone district:

		Standard	Provided
Maximum Building Height		35'	34' 11 1/2"
Minimum Building Setbacks	Front (Venetian Dr.-1 st & 2 nd story)	25'	25' 4"
	Front (Venetian Dr.-3 rd story)	30'	30' 2"
	Side Street (Bucida Road-1 st & 2 nd Story)	25'	25' 1"
	Side Street (Bucida Road-3 rd Story)	30'	30' 1"
	Side Interior (north-1 st & 2 nd Story)	15'	15' 1"
	Side Interior (north-3 rd Story)	30'	30' 1"
	Rear (West)	25'	25' 4 1/2"
Minimum Floor Area	Four Bedroom Unit "A"	1,500 sq. ft.	3,720 sq. ft.
	Four Bedroom Unit "B"	1,500 sq. ft.	3,996 sq. ft.
	Four Bedroom Unit "C"	1,500 sq. ft.	3,747 sq. ft.
Maximum Lot Coverage		40%	39.8%
Minimum Open Space		25%	35%

LDR Section 4.6 (Supplemental Regulations):

Parking:

Pursuant to LDR Section 4.6.9(C)(2)(c), 8 parking spaces are required for this 3-unit townhouse development (3 x 2 per unit plus 0.5 space (guest) per unit = 7.5 which is rounded up to 8). A total of 13 parking spaces are provided. Two parking spaces are provided within Units A and B and three parking spaces within Unit C. In addition, 2 tandem parking spaces within each driveway (6 total spaces provided). The depth of the proposed garages varies from 20' to 31' for Unit "A," from 20' to 29' Unit "B" and from 17' to 31' for Unit "C". This additional depth will accommodate customary storage within the garages. Thus, the requirement has been met.

Pool Setbacks:

LDR Sections 4.6.15(G)(1), a minimum ten foot (10') setback is required for swimming pools located in the rear, interior or side street setback areas. Swimming pools shall not extend into the front setback area noted in Section 4.3.4(K). Per the chart below, the proposed development meets this requirement:

	Front Venetian Drive East	Side Street Bucida Road South	Side Interior North	Rear West
Required	25'	10'	10'	10'
Proposed	25' 8"	10' 6"	10'	10'

Sight Visibility Triangles:

Pursuant to LDR Section 4.6.14(B)(1) when an access way intersects a public right-of-way, triangular areas shall provide unobstructed cross-visibility. The area on both sides of a driveway

formed by the intersection of a driveway and a street with a length of ten (10) feet along the driveway, a length of ten (10) feet along the street right-of-way and the third side being a line connecting the ends of the other two lines. Visibility triangles for the three access driveways along the street rights-of-way have been depicted and comply with this LDR requirement.

Additionally, pursuant to LDR Section 4.6.14(B)(6), a 40 foot sight visibility triangle is required at the intersection of Venetian Drive and Bucida Road. The proposed six foot high fence located at the southeast corner of the site encroaches within the required 40' sight visibility triangle. A request to reduce the sight visibility triangle was submitted by the applicant and is attached.

LDR Section 4.6.14 (E) states that requests for a relief from the sight visibility triangle dimensions may be granted by the City Engineer on a case-by-case basis and the review shall take into consideration neighborhood characteristics such as the locations of schools, parks, and other community facilities, and street characteristics such as the pavement width, width of the right-of-way to curb or sidewalk, curvature of the street, one-way directional traffic, presence of on-street parking spaces, speed limit and other similar elements. Denial of relief by the City Engineer may be appealed to the City Commission.

The City Engineer has approved the reduction of the sight visibility from the required 40' to 25'

Other Issues

Right-of-Way:

Pursuant to LDR Section 5.3.1(D)(2), the required right-of-way width for a local street without curb and gutter is 60 feet. The right-of-way width for both Venetian Drive and Bucida Road is 50 feet. The City Engineer has determined that no additional right-of-way dedication is required. A positive finding can be made with respect to right-of-way.

Sidewalk:

Pursuant to LDR Section 6.1.3(B)(1), a 5'-wide sidewalk is required along both Venetian Drive and Bucida Road. The proposed site plan complies with this requirement since 5-foot wide sidewalks are proposed along both rights-of-way and are noted on the site and engineering plans.

Access Easements:

The provision of an access easement to facilitate for the provision of service to the rear yard of the interior unit (Unit "B") has been accommodated with the provision of a 5 foot access easement across Unit "A".

Undergrounding of Utilities:

Pursuant to LDR Section 6.1.8, utility facilities serving the development shall be located underground throughout the development. A note to this effect has been placed on the site plan. Thus this requirement has been met.

LANDSCAPE PLAN ANALYSIS

As previously noted that the subject property is heavily vegetated and contains a variety of Palm trees including, Coconut Palm, Queen Palm, Sabal Palm, Areca Palm Clusters, Alexander Palm Cluster, Royal Palm, Traveler Palm Clusters, Citrus Species, Hibiscus, Soft Tip Yucca, Weeping Fig and Strangler Fig. All of the trees are to be removed, except the Royal Palm (which will be relocated) and the Alexander Cluster and Strangler Fig (which will remain).

A landscape plan has been submitted and evaluated by the Senior Landscape Planner. The landscape plan provides for perimeter and interior landscape areas in addition to foundation planting areas. The proposed landscape plan includes Medjool Date Palm, Single and Triple trunk Alexander Palms, Pigeon Plum, Gumbo Limbo, Bay Rum, Purple Queen, Japanese Pivot, the relocated Royal Palm and the existing Strangler Fig, underplanted with Foxtail Fern, Queen Emma Lilly, Green Island Ficus, Large Fireball Bromeliad, Red-tipped Cocoplum, Thach Palms and Dwarf Yaupon Holly. Foundation and accent plantings at the entry and driveways include Mamey Croton, Yew Podocarpus, Confederate Jasmine, Alcantarea Imperialis, Alexander Palms, Green Island Ficus, Creeping Fig and Thatch Palm. Based on the above analysis, the Landscape Plan complies with LDR Section 4.6.16.

BUILDING ELEVATIONS ANALYSIS

Pursuant to LDR Section 4.6.18(E), the following criteria shall be considered by the Site Plan Review and Appearance Board in the review of plans for building permits. If the following criteria are not met, the application shall be disapproved.

- (1) The plan or the proposed structure is in conformity with good taste, good design, and in general, contributes to the image of the City as a place of beauty, spaciousness, harmony, taste, fitness, broad vistas, and high quality.
- (2) The proposed structure, or project, is in its exterior design and appearance of quality such as not to cause the nature of the local environment or evolving environment to materially depreciate in appearance and value.
- (3) The proposed structure, or project, is in harmony with the proposed developments in the general area, with the Comprehensive Plan, and with the supplemental criteria which may be set forth for the Board from time to time.

The project architect has indicated that the townhomes represents a transitional Caribbean (tropical) style of architecture with some traditional influences. The elevations facing the streetscape incorporate building offsets as well as adequate variations in the roofline. The first floor of the two end units and the third floor of the interior unit will have wall cladding with a smooth stucco finish for the remaining walls of the townhouse building. The roof treatment consisting of flat cement tiles exhibit a combination of gambrel and hip roofs. Diversity among the windows will be accomplished with the provision of four different windows shapes which include: single, double and triple hung aluminum windows. The windows will be distinguished with various elements including aluminum bahama shutters raised stucco banding above or below the windows.

The front elevation of the building along Venetian Drive contains both the garage and main entry for Unit "A". The garage entries for Units "B" and "C" and the main entry for Unit "B" are

located on the south elevation facing Bucida Road. The main entry for Unit "C" is located on the west elevation. The overhead garage doors are metal with a painted wood pergola above the door. The entry doors proposed are stained mahogany doors with wood entry awning supported by wood painted brackets. The architectural elevations also include many features and design elements, such as: glass guardrails for the third story balconies, powder coated aluminum railings with stained wood cap supported by smooth stucco brackets are proposed for the second story balconies, custom aluminum screens, custom aluminum louvers, cantilevered wood trellis, wood stained outriggers and decorative wall mounted light fixtures.

Paver walkways enclosed with a 6' high privacy wall with raised smooth stucco banding and 4'8" white aluminum gates provide access to the units.

The townhouse building features two and three-story roof lines, and will have a combination of smooth stucco and wall cladding finishes. The proposed exterior colors incorporate a color palate that includes a Toque White walls, a Silver Travertine for the wall cladding, a Rookwood Clay (brown) for the garage doors, balcony railings and privacy fence, Interesting Aqua shutters and Charcoal for the flat cement roof tiles. Staff recommends that the color of the privacy walls matches the Toque white walls of the building and this is noted as a condition of approval.

The proposed development will create an interesting streetscape and building design along Venetian Drive and Bucida Road. The proposed development will improve the aesthetics of the vicinity and will be in harmony with developments in the area; thus, a positive finding can be made with respect to LDR Section 4.6.18(E).

REQUIRED FINDINGS

Pursuant Section 3.1.1 Required Findings: Prior to the approval of development applications, certain findings must be made in a form which is a part of the official record. This may be achieved through information on the application, written materials submitted by the applicant, the staff report, or minutes. Findings shall be made by the body which has the authority to approve or deny the development application. These findings relate to the following areas:

Section 3.1.1(A) - Future Land Use Map: The subject property has a Future Land Use Map designation of MD (Medium Density 5-12 du/ac) and is currently zoned RM (Medium Density Residential). The RM zone district is consistent with the MD Future Land Use Map designation, and the proposed multiple family (townhouse) development is allowed as a permitted use in the RM zone district [ref. LDR Section 4.4.6(B)(3)]. Based upon the above, it is appropriate to make a positive finding with respect to consistency with the Future Land Use Map.

Section 3.1.1(B) - Concurrency: As described in Appendix A, a positive finding of concurrency can be made as it relates to water, sewer, streets and traffic, drainage, parks and recreation, open space and solid waste. With respect to school concurrency, a request for concurrency determination was transmitted to Palm Beach County School District. A concurrency determination has not been received and is noted as a condition of approval.

Section 3.1.1 (C) - Consistency (Standards for Site Plan Actions): As described in Appendix B, a positive finding of consistency can be made as it relates to Standards for Site Plan Actions.

Section 3.1.1 (D) - Compliance With the Land Development Regulations: As described under the Site Plan Analysis of this report, a positive finding of compliance with the LDRs can be made, provided the conditions of approval are addressed.

COMPREHENSIVE PLAN POLICIES:

A review of the objectives and policies of the adopted Comprehensive Plan was conducted and the following applicable policy and objective are noted.

Open Space and Recreation Element Policy A-3.1 – Tot lots and recreational areas, serving children from toddlers to teens, shall be a feature of all new housing developments as part of design to accommodate households having a range of ages. This requirement may be waived or modified for residential developments located in the downtown area, and for infill projects having fewer than 25 units. The City may require a monetary contribution in-lieu of the provision of on-site facilities where appropriate.

This item was previously discussed under *Special Regulations - RM zone district*. As there are only 3-units, it is not practical to require a common tot lot or recreation areas/swimming pools. The site plan indicates that all three units will have private swimming/plunge pools.

Future Land Use Element Objective A-1

Property shall be developed or redeveloped, in a manner so that the future use, intensity and density are appropriate in terms of soil, topographic, and other applicable physical considerations; encourage affordable goods and services; are complementary to and compatible with adjacent land uses; and fulfill remaining land use needs.

The existing use of the property is a single family residence. There are no special physical or environmental characteristics of the land that would be negatively impacted by the proposed townhouse development and the development will be complimentary with the surrounding residential neighborhood. The uses surrounding the property are multiple family to the north, east and west. As noted above, compatibility with the adjacent residences is not a concern, as the proposed townhouse development is consistent with the established development pattern of the neighborhood to the north, east and west. The proposed development will have little impact on the single family residential neighborhood to the south given the low traffic generation. The proposed development will be an asset to the neighborhood, therefore, a positive finding with respect to LDR Section 2.4.5(F)(5) can be made.

Housing Policy A-11.3:

In evaluating proposals for new development or redevelopment, the City shall consider the effect that the proposal will have on the stability of nearby neighborhoods. Factors such as noise, odors, dust, traffic volumes and circulation patterns shall be reviewed in terms of their potential to negatively impact the safety, habitability and stability of residential areas. If it is determined that a proposed development will result in a degradation of any neighborhood, the project shall be modified accordingly or denied.

As noted previously, the development proposal involves the demolition of the existing single family residential structure and construction of three townhouse units. Nuisances such as noise, odors, and dust will not be a factor to the adjacent residential uses. The traffic volumes will be

minimal and can be accommodated by the surrounding road network. Thus, the proposal will not result in a degradation of, but will enhance adjacent neighborhoods.

Housing Element Policy B-2.2 - The development of new adult oriented communities within the City is discouraged. New housing developments shall be designed to accommodate households having a range of ages, especially families with children, and shall be required to provide 3 and 4 bedroom units and activity areas for children ranging from toddlers to teens. This requirement may be waived or modified for residential development located in the downtown area, and for infill projects having fewer than 25 units.

The project will contain three 4-bedroom units and as noted above this requirement may be waived or modified for residential developments with fewer than twenty-five (25) units.

Section 2.4.5(F)(5) (Site Plan Findings):

Pursuant to Section 2.4.5(F)(5) (Findings), in addition to provisions of Chapter Three, the approving body must make a finding that the development of the property pursuant to the site plan will be harmonious with the adjacent and nearby properties and the City as a whole, so as not to cause substantial depreciation of property values.

The subject property is bordered by the RM zone district to the north, east and west and R-1-AAA (Single Family Residential) zone district to the south. It is noted that the surrounding uses are a mix of one and two story structures.

Immediately abutting to the west is a two story two unit townhouse development. To the north, is a one story townhouse development. To the east (east side of Venetian Drive), the Carolina Village townhouse units are two story buildings and one-story single family residences are located to the south (south side of Bucida Road).

With the design of the structure, impact on the adjacent property with respect to building height is minimized by increasing the setback dimension of the third floor from the required 15 feet to 30 feet along the north side which directly abuts the one-story multiple family structures. Additionally impact to the single family residence is minimized as the development is separated by Bucida Road. The design of the site plan with access on both abutting streets lessens the number of vehicular trips from the development onto Bucida Road.

Based on the above, the development of the property pursuant to the site plan will be harmonious with the adjacent and nearby properties and the City as a whole, and should not result in the depreciation of property values. Therefore, a positive finding with respect to LDR Section 2.4.5(F)(5) can be made.

REVIEW BY OTHERS

The development proposal is not within a geographical area requiring review by the Community Redevelopment Agency (CRA) or Downtown Development Authority (DDA).

Courtesy Notices:

Special courtesy notices were provided to the following homeowners and civic associations:

- Beach Property Owners Association
 - Delray Citizens Coalition
- Via Marina

Public Notices: Formal public notice is not required for site plan actions.

Letters of objection and/or support, if any, will be presented at the Site Plan Review and Appearance Board meeting.

ASSESSMENT AND CONCLUSION

The proposed townhouse development is compatible with existing multi-family developments on adjacent properties to the north, east and west. The proposed three story structure should not create a negative impact for the one-story townhomes to the north or the single family residences to the south as additional setbacks is provided for the 3rd story. Additionally, the single family residences are buffered by Bucida Road. The design of the project with driveway access for Unit "A" onto Venetian Drive reduces the traffic potential on Bucida Road.

The transitional Caribbean architectural style of the multi-family building will be aesthetically pleasing and a visual asset to the community. The provision of 4 bedroom units addresses housing needs for families in the Delray Beach area. The development substantially meets the performance standards in LDR Section 4.4.6 to allow for the increased density above 6 units per acre. Positive findings can be made with respect to LDR Section 4.4.6(I)[Performance Standards], LDR Sections 4.6.16 [Landscaping] and 4.6.18(E) [Building Elevations], policies of the Comprehensive Plan, Chapter 3 and LDR Section 2.4.5(F)(5).

ALTERNATIVE ACTIONS

- Continue with direction.
- Move approval of the request for a Class V site plan, landscape plan and architectural elevations for **Seagate Villas**, by adopting the findings of fact and law contained in the staff report, and finding that the request and approval thereof is consistent with the Comprehensive Plan and meets criteria set forth in Sections 2.4.5(F)(5), 4.4.6(I)[Performance Standards]; LDR Sections 4.6.16 [Landscaping] and 4.6.18(E) [Building Elevations] and Chapter 3 of the Land Development Regulations, subject to the attached conditions of approval.
- Move denial of the request for a Class V site plan, landscape plan and architectural elevations for the **Seagate Villas**, by adopting the findings of fact and law contained in the staff report, and finding that the request is inconsistent with the Comprehensive Plan and does not meet criteria set forth in Sections 2.4.5(F)(5), 4.4.6(I)[Performance Standards] LDR Sections 4.6.16 [Landscaping] and 4.6.18(E) [Building Elevations] and Chapter 3 of the Land Development Regulations.

STAFF RECOMMENDATION

Site Plan

Move approval of the request for Class V site plan approval for **Seagate Villas**, by adopting the findings of fact and law contained in the staff report, and finding that the request and approval thereof is consistent with the Comprehensive Plan and meets criteria set forth in Section 2.4.5(F)(5); Section 4.4.6(I) [Performance Standards] and Chapter 3 of the Land Development Regulations, subject to the following conditions:

1. That a park impact fee of \$500.00 per unit (\$1,500 total) be paid prior to the issuance of a building permit.
2. Pursuant to LDR Section 4.3.3(O)(2), a plat is required and shall be recorded prior to the issuance of the building permit.
3. That approval of the school concurrency from Palm Beach County School District is submitted prior to the issuance of a building permit.

Landscape Plan

Move approval of the request for landscape plan approval for **Seagate Villas**, by adopting the findings of fact and law contained in the staff report, and finding that the request is consistent with the Comprehensive Plan and meets criteria set forth in Sections 4.6.16 of the Land Development Regulations.

Architectural Elevations

Move approval of the request for architectural elevations for **Seagate Villas**, by adopting the findings of fact and law contained in the staff report, and finding that the request and approval thereof is consistent with the Comprehensive Plan and meets criteria set forth in Section 4.6.18(E) of the Land Development Regulations subject to the following condition:

1. That revised architectural elevation plans are submitted to note that the color of the privacy walls matches the Toque white walls of the building prior to the certification of the site plan.

Attachments:

Appendix A

Appendix B

Site Plan, Floor Plans, Building Elevations, Landscape Plan, Engineering Plans and Location Map

Report prepared by: Jasmin Allen, Senior Planner

APPENDIX A CONCURRENCY FINDINGS

Pursuant to Section 3.1.1(B) Concurrency as defined pursuant to Objective B-2 of the Land Use Element of the Comprehensive Plan must be met and a determination made that the public facility needs of the requested land use and/or development application will not exceed the ability of the City to fund and provide, or to require the provision of, needed capital improvements for the following areas:

Water and Sewer:

- ☐ The property is currently served by an 6" water main along Bucida Road.
- ☐ Adequate fire suppression will be provided via existing fire hydrants on the south side of Bucida Road.
- ☐ Sewer service will be provided via service lateral connection to an existing 8" sewer main along Bucida Road.

Pursuant to the Comprehensive Plan, treatment capacity is available at the City's Water Treatment Plant and the South Central County Waste Water Treatment Plant for the City at build-out. Based upon the above, positive findings can be made with respect to this level of service standard.

Drainage:

Drainage is to be accommodated via sheet flow to catch basins that drain to an exfiltration system. There are no problems anticipated in complying with SFWMD requirements and thus, obtaining a surface water permit.

Streets and Traffic:

The 3-unit townhouse development will generate 21 average daily trips with 3 peak a.m. and 3 peak p.m. vehicle trips onto the surrounding roadway network. There is adequate capacity on the surrounding roadway network to accommodate the additional trips. It is noted that according to the Palm Beach County Traffic Performance Standards Ordinance, residential development east of I-95 are exempt from meeting concurrency. Thus, the proposal complies with the Palm Beach County Traffic Performance Standards Ordinance. Based upon the above, a positive finding can be made with respect to traffic concurrency.

Parks and Recreation Facilities:

The proposed units will not have a significant impact with respect to level of service standards for parks and recreation facilities. However, pursuant to LDR Section 5.3.2, whenever a development is proposed upon land which is not designated for park purposes in the Comprehensive Plan, an in-lieu fee of \$500.00 per dwelling unit will be collected prior to issuance of building permits for each unit. Thus, an in-lieu fee of \$1,500.00 will be required of this development.

Solid Waste:

Trash generated each year by this development will be approximately 1.99 tons per townhouse unit (3 units) for a total of 5.97 tons/year which represents an increase of 3.98 tons from the existing single family residence. The Solid Waste Authority has indicated that its facilities have sufficient capacity to accommodate all development proposals until 2047. Trash collection will be accommodated via roll-out containers.

School:

The proposed development is subject to the Palm Beach County School Concurrency. Approval of the school concurrency has not yet been received and this is noted as a condition of approval.

APPENDIX B
STANDARDS FOR SITE PLAN ACTIONS

- A. Building design, landscaping, and lighting (glare) shall be such that they do not create unwarranted distractions or blockage of visibility as it pertains to traffic circulation.**

Not applicable _____

Meets intent of standard X

Does not meet intent _____

- B. Separation of different forms of transportation shall be encouraged. This includes pedestrians, bicyclists, and vehicles in a manner consistent with policies found under Objectives D-1 and D-2 of the Transportation Element.**

Not applicable _____

Meets intent of standard X

Does not meet intent _____

- C. Open space enhancements as described in Policies found under Objective B-1 of the Open Space and Recreation Element are appropriately addressed.**

Not applicable X

Meets intent of standard _____

Does not meet intent _____

- D. The City shall evaluate the effect that any street widening or traffic circulation modification may have upon an existing neighborhood. If it is determined that the widening or modification will be detrimental and result in a degradation of the neighborhood, the project shall not be permitted.**

Not applicable X

Meets intent of standard _____

Does not meet intent _____

- E. Development of vacant land which is zoned for residential purposes shall be planned in a manner which is consistent with adjacent development regardless of zoning designations.**

Not applicable _____

Meets intent of standard X

Does not meet intent _____

- F. Vacant property shall be developed in a manner so that the future use and intensity are appropriate in terms of soil, topographic, and other applicable physical considerations; complementary to adjacent land uses; and fulfills remaining land use needs.**

Not applicable _____

Meets intent of standard X

Does not meet intent _____

G. Redevelopment and the development of new land shall result in the provision of a variety of housing types which shall continue to accommodate the diverse makeup of the City's demographic profile, and meet the housing needs identified in the Housing Element. This shall be accomplished through the implementation of policies under Objective B-2 of the Housing Element.

Does not meet intent

H. The City shall consider the effect that the proposal will have on the stability of nearby neighborhoods. Factors such as noise, odors, dust, traffic volumes and circulation patterns shall be reviewed in terms of their potential to negatively impact the safety, habitability and stability of residential areas. If it is determined that a proposed development will result in a degradation of any neighborhood, the project shall be modified accordingly or denied.

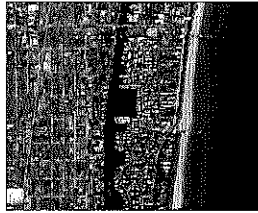
Does not meet intent

I. Development shall not be approved if traffic associated with such development would create a new high accident location, or exacerbate an existing situation causing it to become a high accident location, without such development taking actions to remedy the accident situation.

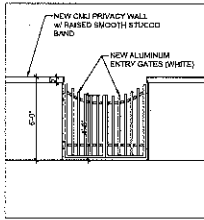
Does not meet intent

J. Tot lots and recreational areas, serving children from toddler to teens, shall be a feature of all new housing developments as part of the design to accommodate households having a range of ages. This requirement may be waived or modified for residential developments located in the downtown area, and for infill projects having fewer than 25 units.

Does not meet intent



2 LOCATION MAP
N.T.S.



3 GATE DETAIL
1/4" = 1'-0"

- NOTES:
1. PER DELRAY BEACH CODE OF ORDINANCES: ALL BUILDING OR STRUCTURES REQUIRING A PERMIT OF CONSTRUCTION WHICH ARE 3 OR MORE FEET TALLER THAN 10 FEET OR MORE INCORPORATED STORIES AS DEMAND REQUIRED BY THE AUTHORITY HAVING JURISDICTION MUST HAVE SPRINKLER PROTECTION.
 2. FIRE CODE REQUIREMENTS: FLORIDA FIRE PREVENTION CODE 2013 EDITION - FLORIDA SPECIFIC (FPPA), FIRE CODE 2012 EDITION & FLORIDA SPECIFIC (FPPA) & LIFE SAFETY CODE, 2012 EDITION.
 3. ALL PROPOSED UTILITY LINES WILL BE PLACED UNDERGROUND.

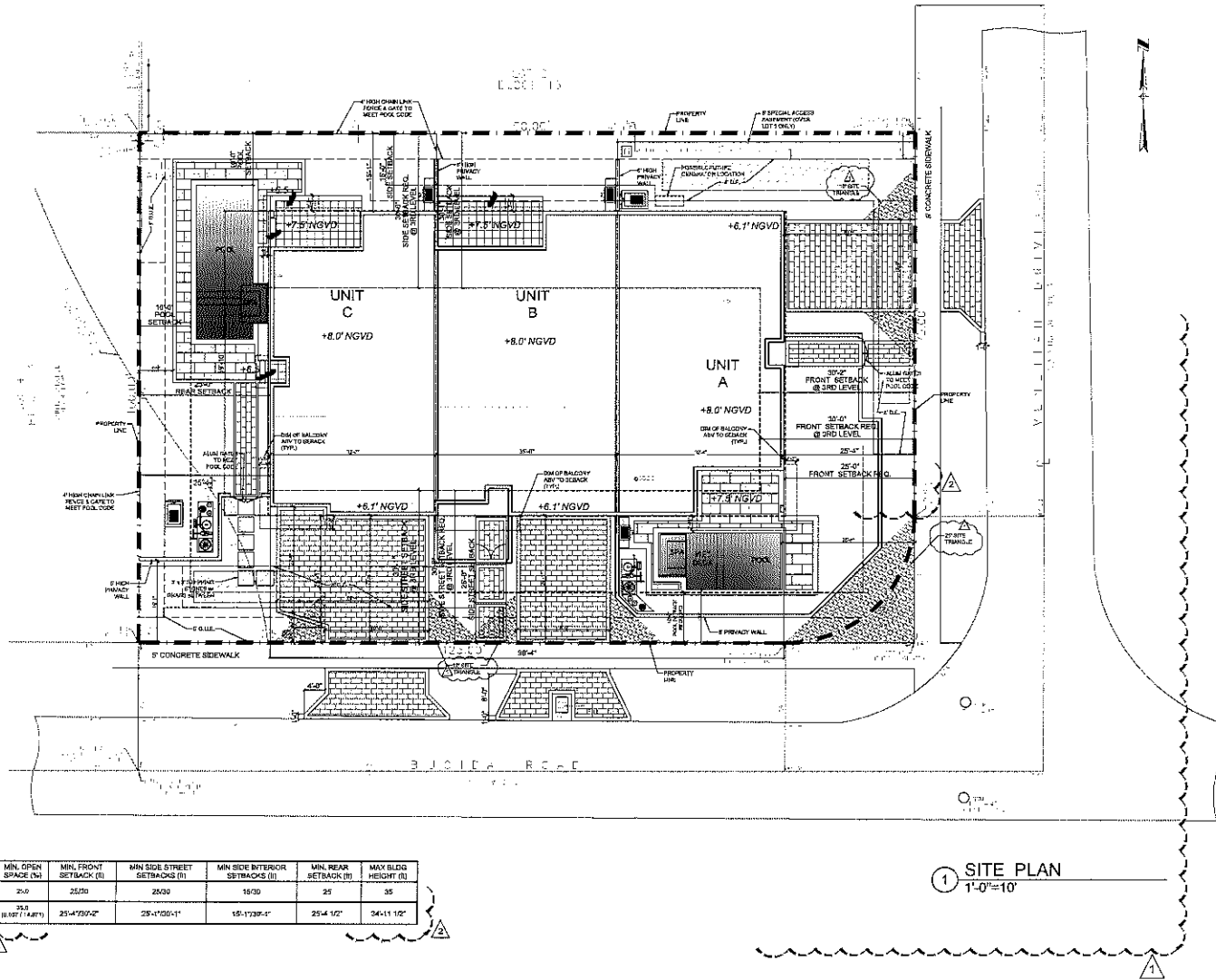
MIN. LOT	MIN. LOT	MIN. LOT	MIN. LOT	MIN. LOT	MAX. LOT	MIN. OPEN	MIN. FRONT	MIN. SIDE STREET	MIN. SIDE INTERIOR	MIN. REAR	MAX. BLDG
REQUIREMENT	SIZE (SQ. FT.)	WIDTH (FT.)	DEPTH (FT.)	FRONTAGE (FT.)	COVERAGE (SQ. FT.)	SPACE (SQ. FT.)	SETBACK (FT.)	SETBACKS (FT.)	SETBACKS (FT.)	SETBACK (FT.)	HEIGHT (FT.)
REQUIRED	8,000	60	100	60	40	21.0	25.00	25.00	15.00	25	35
PROVIDED	14,871	100	260	75	388	11,887 (7,887)	25'-4" (25'-0")	25'-4" (25'-0")	15'-1" (15'-0")	25'-4" (25'-0")	24'-11" (25'-0")

LOT COVERAGE CALCULATION
BUILDING FOOTPRINT: 3,500
INCLUDES COVERED ENTRY, PATIO
(MAX ALLOWED 4% A. 3,500)

IMPERVIOUS CALCULATION
LOT COVERAGE: 8,320
IMPERVIOUS AREAS: 3,740
(PAVEMENT, DRIVEWAY, POOL & POND, DRIVEWAY, ETC.)

IMPERVIOUS DATA
GROUND COVER AREA: 8,320 SF
TOTAL FLOOR AREA: 11,420 SF
PARKING COVERED AREA: 3,740 SF
UPON ESCAPED SPACE: 1,290 SF
WATER RESERVE: 0.50
NO. OF DWELLING UNITS: 5
DWELLING UNIT AREA: 8.0
PARKING REQUIRED: 7.5 SPACES
PARKING PROVIDED: 15.0 SPACES

OPEN SPACE - LOT SIZE - IMPERVIOUS
OPEN SPACE = 14,871 - 3,500 = 11,371



1 SITE PLAN
1'-0" = 1'-0"

PRELIMINARY - NOT FOR CONSTRUCTION 2-25-2016

REVISIONS	BY	DATE
1	CHERRY	02/25/16
2	CHERRY	02/25/16
3	CHERRY	02/25/16
4	CHERRY	02/25/16
5	CHERRY	02/25/16
6	CHERRY	02/25/16
7	CHERRY	02/25/16
8	CHERRY	02/25/16
9	CHERRY	02/25/16
10	CHERRY	02/25/16
11	CHERRY	02/25/16
12	CHERRY	02/25/16
13	CHERRY	02/25/16
14	CHERRY	02/25/16
15	CHERRY	02/25/16
16	CHERRY	02/25/16
17	CHERRY	02/25/16
18	CHERRY	02/25/16
19	CHERRY	02/25/16
20	CHERRY	02/25/16

SEAGATE VILLAS
1017 BUCIDA ROAD
DELRAY BEACH, FL

RANDALL STOFF
ARCHITECTS
distinctive. inspirational. architectural.

SCALE

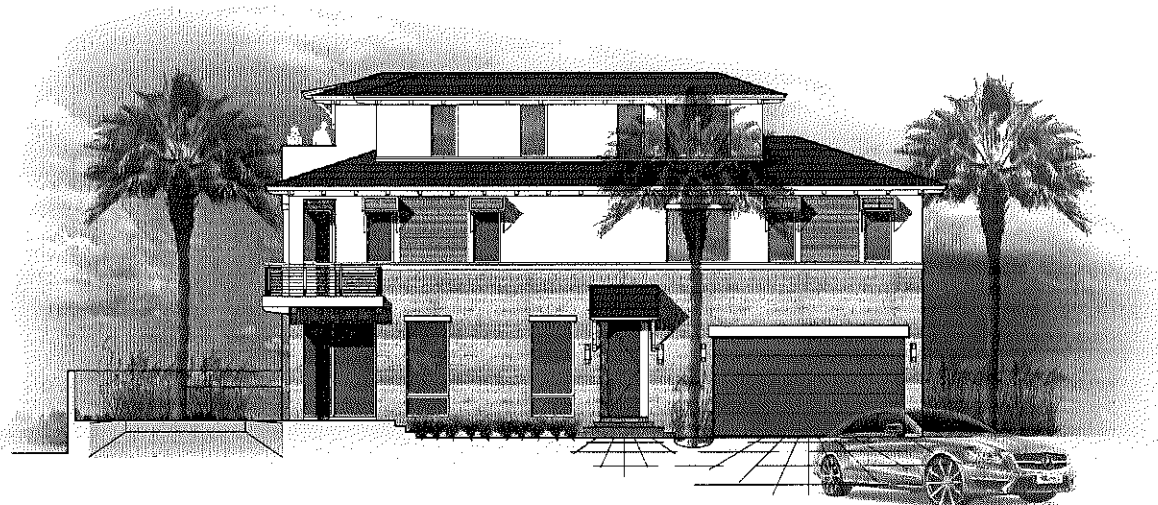
PLAN/SECTION FROM MASTER

SITE PLAN

A101



2 EAST ELEVATION (BUCIDA)
3/16"=1'-0"



1 SOUTH ELEVATION (VENETIAN)
3/16"=1'-0"

PRELIMINARY - NOT FOR CONSTRUCTION 01-22-16

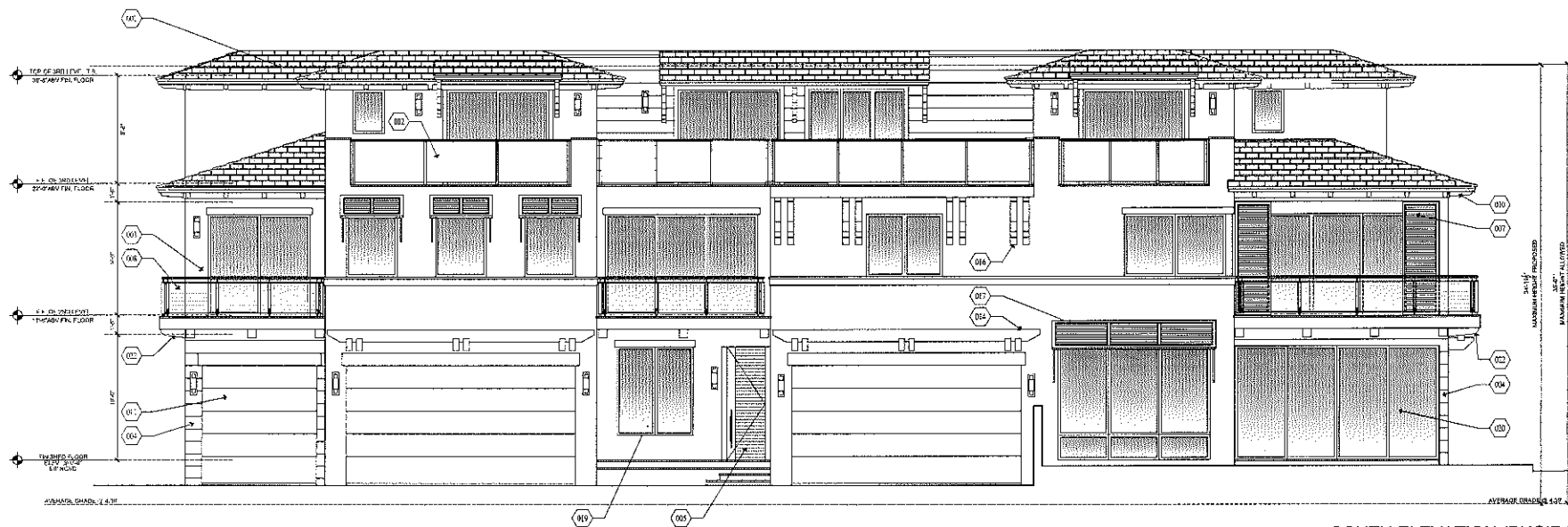
REVISIONS	BY	DATE
1	ME	03 PM 1/22/16

SEAGATE VILLAS
1017 BUCIDA ROAD
DELRAY BEACH, FL

RANDALL STOFFT
ARCHITECTS
distinctive. inspirational. architecture.
1000 W. UNIVERSITY AVENUE, SUITE 100, DEER BEACH, FL 33442 • TEL: 561.892.8000 • FAX: 561.892.8001

SCALE
FT-CMDS: 1/8"=1'-0"
FRONT ELEVATIONS:
COLOR RENDERINGS

A300



2 SOUTH ELEVATION (BUCIDA)
1/4"=1'-0"



1 EAST ELEVATION (VENETIAN)
1/4"=1'-0"

MATERIAL LEGEND			
001	FLAT CEMENT ROOF TILE	009	PAINTED WOOD WALL PERGOLA
002	GLASS GUARDRAIL	010	WOOD STAINED OUTRIGGER
003	SMOOTH STUCCO FINISH	011	OVERHEAD GARAGE DOOR
004	SILVER TRAVERTINE, HONED	012	LIGHTING FIXTURE
005	STAINED MAHOGANY ENTRY DOOR	013	POWER COATED ALUMINUM RAILING
006	CUSTOM ALUMINUM SCREEN	014	CANTILEVER WOOD TRELLIS
007	CUSTOM ALUMINUM LOUVERS	015	WOOD PAINTED OUTRIGGER
008	POWER COATED ALUMINUM RAILING WITH STAINED WOOD CAP	016	WOOD PAINTED BRACKET
017	ALUMINUM BAHAMA SHUTTER		
018	WOOD ENTRY WAINING		
019	ALUMINUM WINDOW		
020	ALUMINUM SLIDING DOOR		
021	RAISED STUCCO BANDING		
022	SMOOTH STUCCO BRACKET		

PRELIMINARY - NOT FOR CONSTRUCTION 01-22-16

REVISIONS	BY	DATE
1	JLB	01-22-16

SEAGATE VILLAS
1017 BUCIDA ROAD
DELRAY BEACH, FL

RANDALL STOFFT
ARCHITECTS
distinctive. inspirational. architecture.
20 N. BENTLEY AVE. DELRAY BEACH, FL 33445 TEL: 561-241-2400 • FAX: 561-241-2401 • WWW.RSARCHT.COM

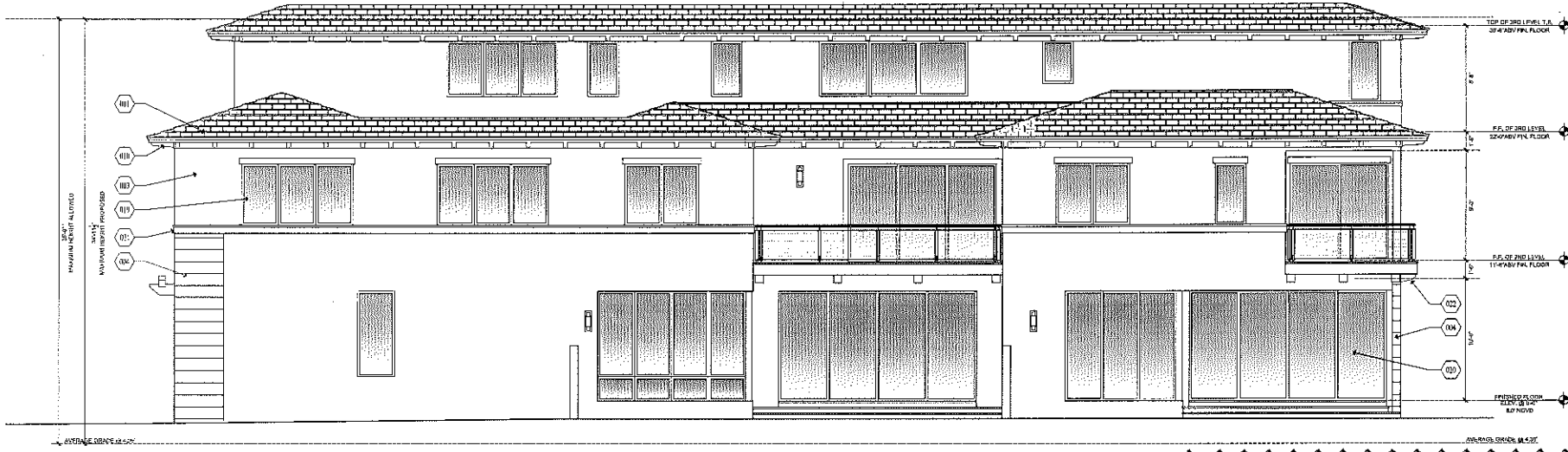
SCALE
PL-001005 PR04-AL000000
SOUTH & EAST ELEVATIONS

A301

MATERIAL LEGEND		
001: FLAT GYPSUM ROOF TILES	008: PAINTED WOOD WALL PERGOLA	017: ALUMINUM BAHAMA SHUTTER
002: GLASS GUARDRAIL	010: WOOD STAINED OUTRIGGERS	018: WOOD ENTRY AWNING
003: SMOOTH STUCCO FINISH	011: OVERHEAD GARAGE DOOR	019: ALUMINUM WINDOW
004: SILVER THERMATEX WIGID	012: LIGHTING FIXTURE	020: ALUMINUM SLIDING DOOR
005: STAINED VARIOUS ENTRY DOOR	013: POWER COATED ALUMINUM RAILING	021: RAISED STUCCO BANDING
006: CUSTOM ALUMINUM SCREEN	014: CANTILEVER WOOD TRELLIS	022: SMOOTH STUCCO BRACKET
007: CUSTOM ALUMINUM LOUNGE	015: WOOD PAINTED OUTRIGGERS	
008: POWER COATED ALUMINUM RAILING WITH STAINED WOOD CAP	016: WOOD PAINTED BRACKET	



2 WEST ELEVATION
1/4"=1'-0"



1 NORTH ELEVATION
1/4"=1'-0"

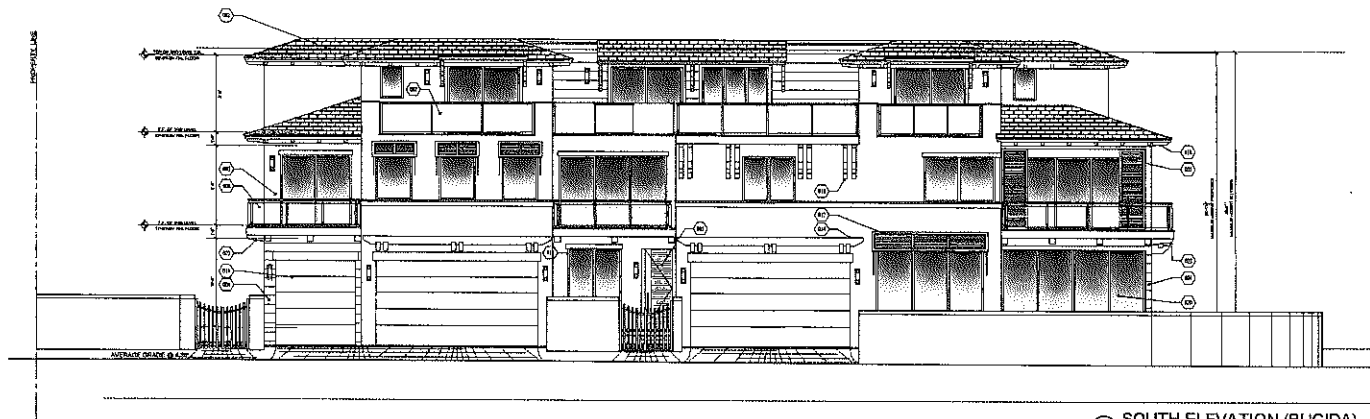
PRELIMINARY - NOT FOR CONSTRUCTION 01-22-16

REVISIONS	BY	DATE
1.0	SDM	01/22/16

SEAGATE VILLAS
1017 BUCIDA ROAD
DELRAY BEACH, FL

RANDALL STOFFT
ARCHITECTS
distinctive. inspirational architecture.
401 N. HUNTER AVE., DELRAY BEACH, FL 33444-4634-0000 • 561.247.2407 • WWW.RSSTOFFT.COM

SCALE
25'-0"=1'-0" FROM ARCHITECT
NORTH & WEST ELEVATIONS
A302



② SOUTH ELEVATION (BUCIDA)
3/16"=1'-0"



① EAST ELEVATION (VENETIAN)
3/16"=1'-0"

MATERIAL LEGEND			
001 FLAT CORNER ROOF TILE	002 PAINTED WOOD WALL PANELING	003 ALUMINUM BRASS DOOR	004 ALUMINUM BRASS DOOR
005 BLANK ALUMINUM	006 WOOD KEANED OUTRIGGER	007 PHOTO ENTRY WINDOW	008 PHOTO ENTRY WINDOW
009 BRIGHT STUCCO FINISH	010 PAINTED ALUMINUM	011 ALUMINUM WINDOW	012 ALUMINUM WINDOW
013 ALUMINUM WINDOW	014 ALUMINUM WINDOW	015 ALUMINUM WINDOW	016 ALUMINUM WINDOW
017 ALUMINUM WINDOW	018 ALUMINUM WINDOW	019 ALUMINUM WINDOW	020 ALUMINUM WINDOW
021 ALUMINUM WINDOW	022 ALUMINUM WINDOW	023 ALUMINUM WINDOW	024 ALUMINUM WINDOW
025 ALUMINUM WINDOW	026 ALUMINUM WINDOW	027 ALUMINUM WINDOW	028 ALUMINUM WINDOW
029 ALUMINUM WINDOW	030 ALUMINUM WINDOW	031 ALUMINUM WINDOW	032 ALUMINUM WINDOW
033 ALUMINUM WINDOW	034 ALUMINUM WINDOW	035 ALUMINUM WINDOW	036 ALUMINUM WINDOW
037 ALUMINUM WINDOW	038 ALUMINUM WINDOW	039 ALUMINUM WINDOW	040 ALUMINUM WINDOW
041 ALUMINUM WINDOW	042 ALUMINUM WINDOW	043 ALUMINUM WINDOW	044 ALUMINUM WINDOW
045 ALUMINUM WINDOW	046 ALUMINUM WINDOW	047 ALUMINUM WINDOW	048 ALUMINUM WINDOW
049 ALUMINUM WINDOW	050 ALUMINUM WINDOW	051 ALUMINUM WINDOW	052 ALUMINUM WINDOW
053 ALUMINUM WINDOW	054 ALUMINUM WINDOW	055 ALUMINUM WINDOW	056 ALUMINUM WINDOW
057 ALUMINUM WINDOW	058 ALUMINUM WINDOW	059 ALUMINUM WINDOW	060 ALUMINUM WINDOW
061 ALUMINUM WINDOW	062 ALUMINUM WINDOW	063 ALUMINUM WINDOW	064 ALUMINUM WINDOW
065 ALUMINUM WINDOW	066 ALUMINUM WINDOW	067 ALUMINUM WINDOW	068 ALUMINUM WINDOW
069 ALUMINUM WINDOW	070 ALUMINUM WINDOW	071 ALUMINUM WINDOW	072 ALUMINUM WINDOW
073 ALUMINUM WINDOW	074 ALUMINUM WINDOW	075 ALUMINUM WINDOW	076 ALUMINUM WINDOW
077 ALUMINUM WINDOW	078 ALUMINUM WINDOW	079 ALUMINUM WINDOW	080 ALUMINUM WINDOW
081 ALUMINUM WINDOW	082 ALUMINUM WINDOW	083 ALUMINUM WINDOW	084 ALUMINUM WINDOW
085 ALUMINUM WINDOW	086 ALUMINUM WINDOW	087 ALUMINUM WINDOW	088 ALUMINUM WINDOW
089 ALUMINUM WINDOW	090 ALUMINUM WINDOW	091 ALUMINUM WINDOW	092 ALUMINUM WINDOW
093 ALUMINUM WINDOW	094 ALUMINUM WINDOW	095 ALUMINUM WINDOW	096 ALUMINUM WINDOW
097 ALUMINUM WINDOW	098 ALUMINUM WINDOW	099 ALUMINUM WINDOW	100 ALUMINUM WINDOW

PRELIMINARY - NOT FOR CONSTRUCTION 01-22-16

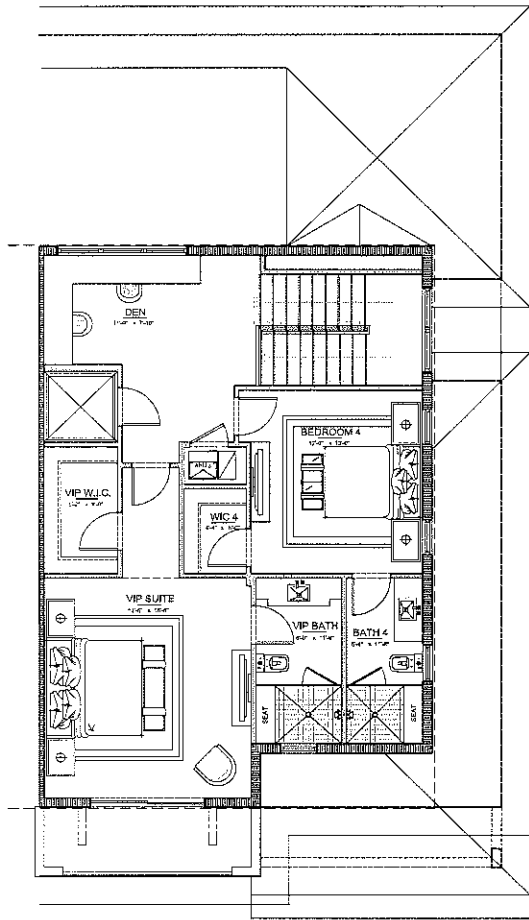
REVISION	BY	DATE
1	ASD	01-22-16

SEAGATE VILLAS
1017 BUCIDA ROAD
DELRAY BEACH, FL

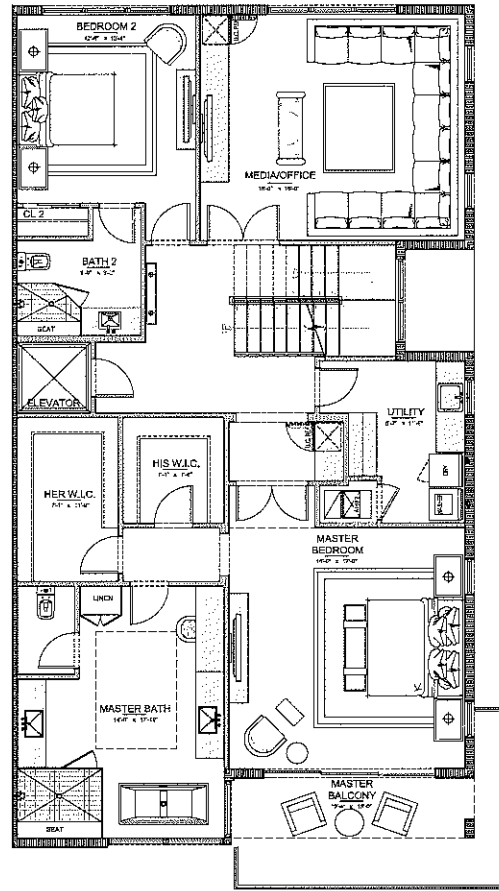
RANDALL STOFF
ARCHITECTS
distinctive. inspirational. architecture.
3141 S. LINDSEY AVE. SUITE 100, DELRAY BEACH, FL 33483-4400
TEL: 561.241.1111 FAX: 561.241.1112 WWW.RSARCHITECTS.COM

15-000000-0000-0000
SOUTH & EAST
ELEVATIONS w/
PRIVATE WALL

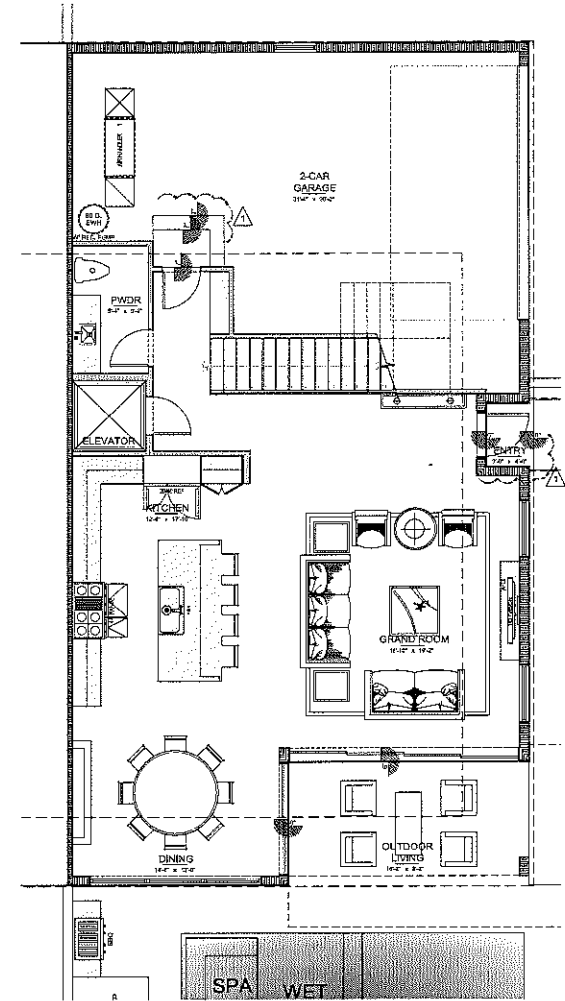
A301-f



3 THIRD FLOOR PLAN
1/4"=1'-0"



2 SECOND FLOOR PLAN
1/4"=1'-0"



1 FIRST FLOOR PLAN
1/4"=1'-0"

A/C AREA	
A/C-1ST FLOOR-UNIT A	1101 SF
A/C-2ND FLOOR-UNIT A	710 SF
A/C-3RD FLOOR-UNIT A	800 SF
	1722 SF

AREA TABULATION	
A/C-1ST FLOOR-UNIT A	1101 SF
A/C-2ND FLOOR-UNIT A	710 SF
A/C-3RD FLOOR-UNIT A	800 SF
2-CAR GARAGE	544 SF
BALCONY	77 SF
MASTER BALCONY	107 SF
OUTDOOR LIVING	424 SF

PRELIMINARY - NOT FOR CONSTRUCTION 01-18-16

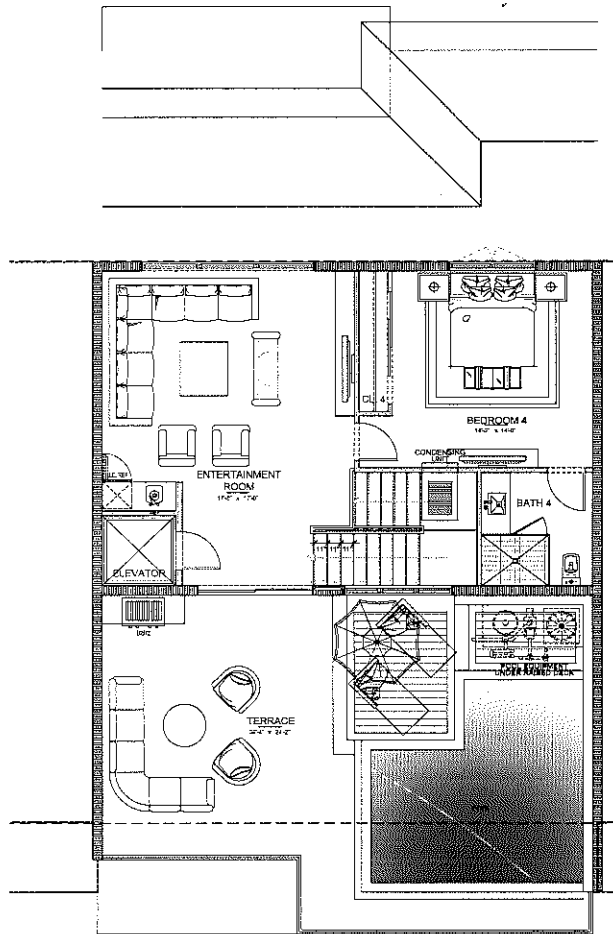
REVISIONS	BY	DATE
1	12.8	2016

SEAGATE VILLAS
1017 BUCIDA ROAD
DELRAY BEACH, FL

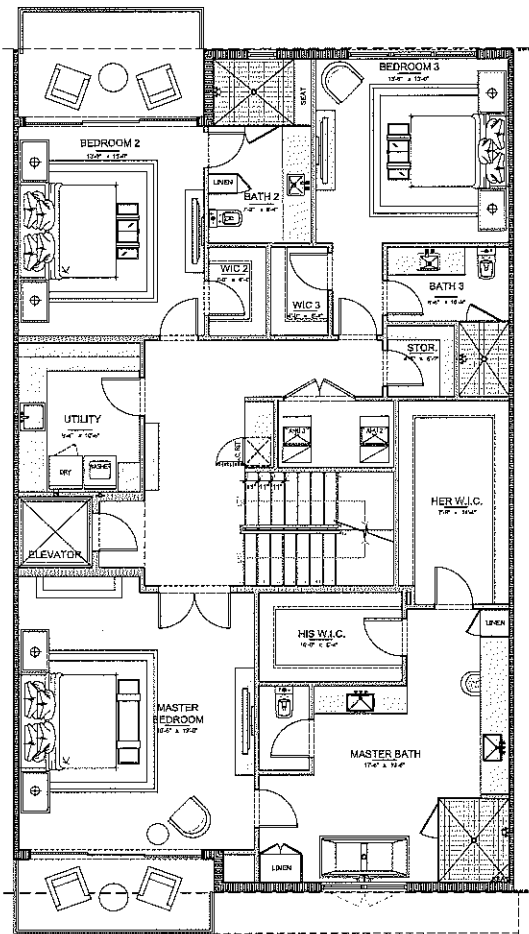
RANDALL STOFFT
ARCHITECTS
distinctive. inspirational architecture.
430 WINDSTAR AVE. DEER BEACH, FL 33442-2008 • NOKIA, FL 33442-2008 • WOODBRIDGE, FL 33442-2008

UNIT A
FLOOR PLANS

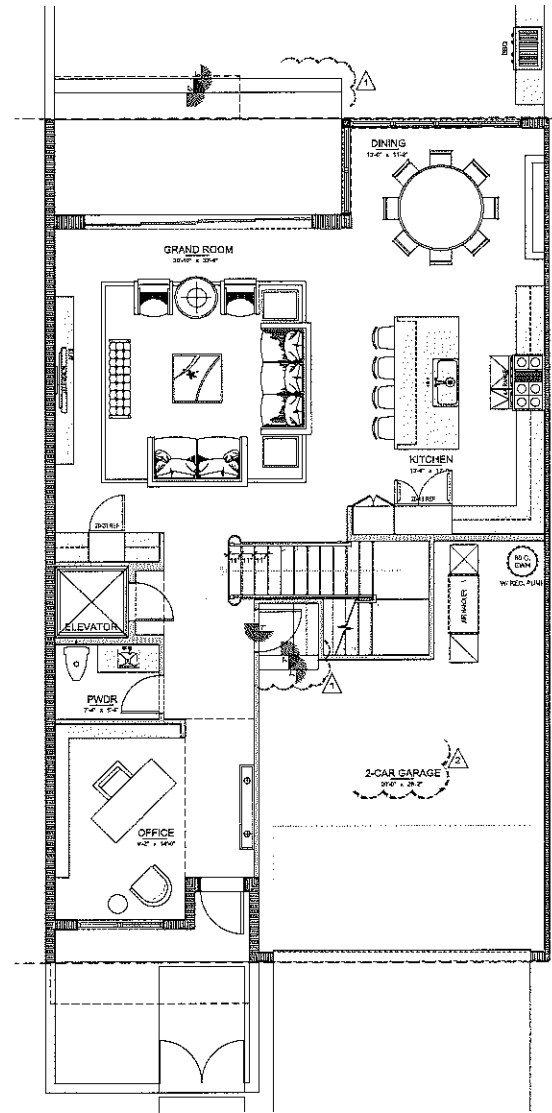
A201



③ THIRD FLOOR PLAN
1/4"=1'-0"



② SECOND FLOOR PLAN
1/4"=1'-0"



① FIRST FLOOR PLAN
1/4"=1'-0"

A/C AREA	
A/C - 1ST FLOOR - UNIT B	1389 SF
A/C - 2ND FLOOR - UNIT B	1885 SF
A/C - 3RD FLOOR - UNIT B	713 SF
A/C - 3RD FLOOR - UNIT B	3893 SF

AREA TABULATION	
A/C - 1ST FLOOR - UNIT B	1389 SF
A/C - 2ND FLOOR - UNIT B	1885 SF
A/C - 3RD FLOOR - UNIT B	713 SF
2-CAR GARAGE	134 SF
COVERED PATIO	134 SF
ENTRY	42 SF
MASTER BALCONY	75 SF
POW. BALCONY	100 SF
TERRACE	713 SF
TOTAL	3941 SF

PRELIMINARY - NOT FOR CONSTRUCTION 2-25-2016

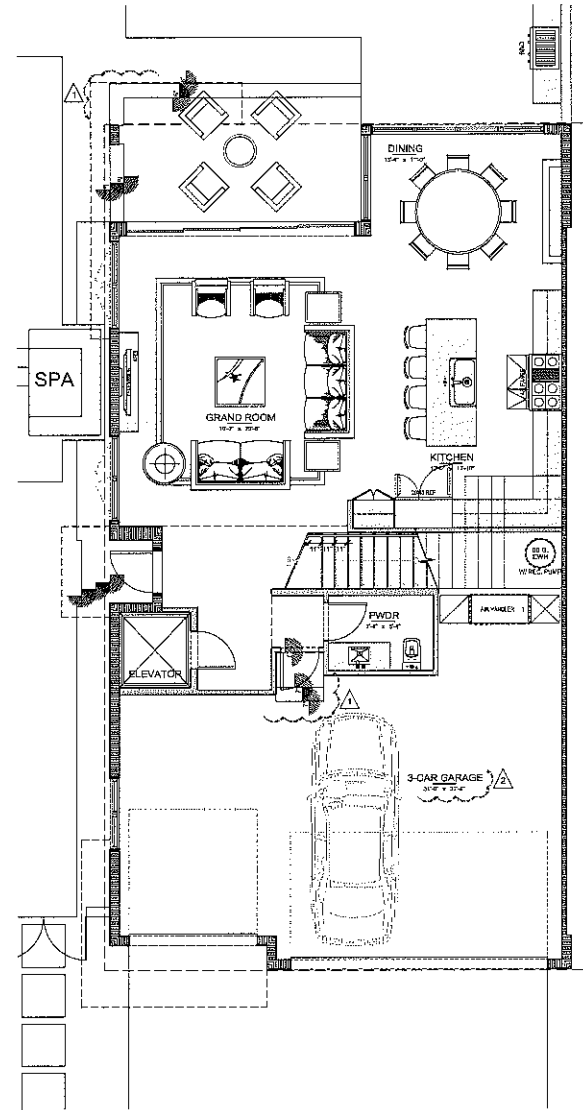
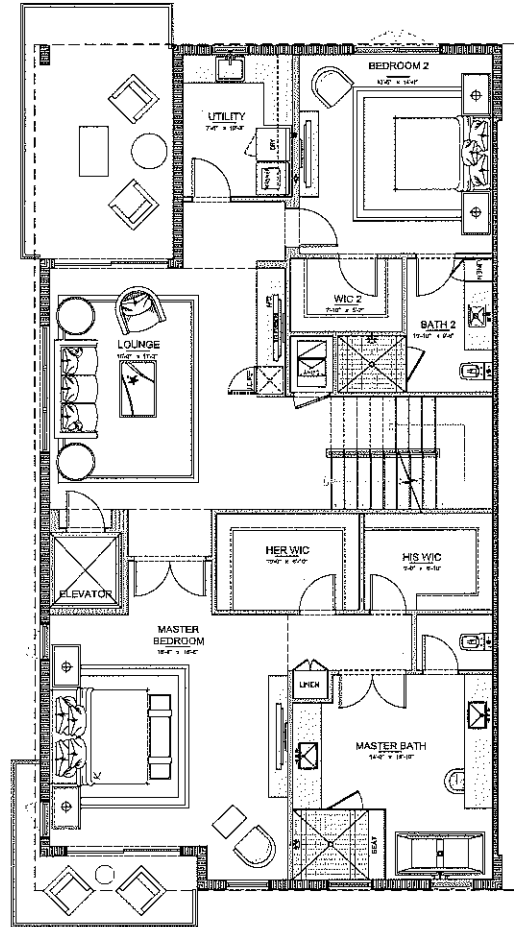
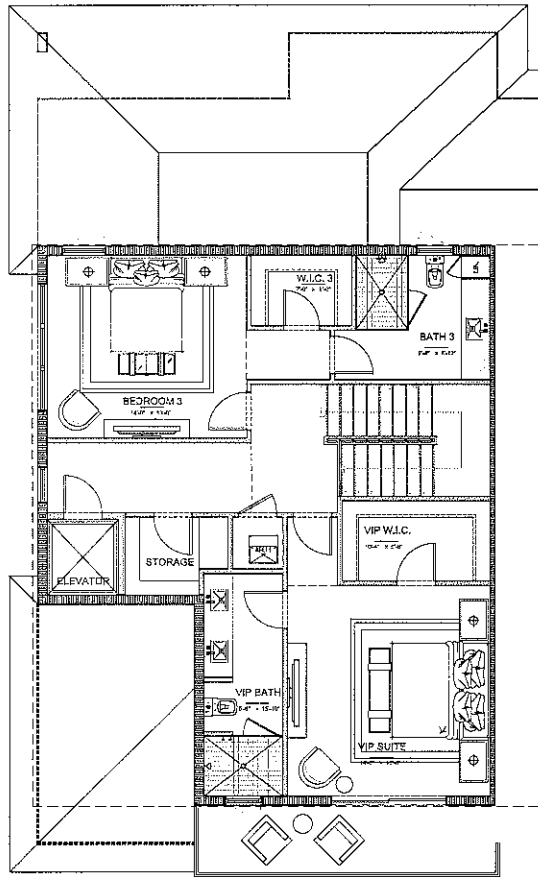
REVISIONS	BY	DATE
1. Change comments	SSPM	02/25/16
2. Change comments	SSPM	02/25/16

SEAGATE VILLAS
1017 BUGIDA ROAD
DELRAY BEACH, FL

RANDALL STOFFT
ARCHITECTS
distinctive, inspirational architecture.
414 N. UNIVERSITY AVE., DELRAY BEACH, FL 33444-4444 • PHONE: 561.363.3733 • WWW.STOFFT.COM

FL-010005 PBD04-A000059
UNIT B
FLOOR PLANS

A202



AC AREA	
AC - 1ST FLOOR - UNIT C	1041 SF
AC - 2ND FLOOR - UNIT C	1791 SF
AC - 3RD FLOOR - UNIT C	681 SF
AC - 3RD FLOOR - UNIT C	3741 SF

AREA TABULATION	
AC - 1ST FLOOR - UNIT C	1041 SF
AC - 2ND FLOOR - UNIT C	1791 SF
AC - 3RD FLOOR - UNIT C	681 SF
ENTRY	14 SF
POWDR BALCONY	150 SF
GAUGE	779 SF
MASTER BALCONY	70 SF
REAR BALCONY	163 SF
	4801 SF

PRELIMINARY - NOT FOR CONSTRUCTION 2-25-2016

REVISIONS	BY	DATE
1. Design complete	SSP	02/11/16
2. Design complete	SSP	02/11/16

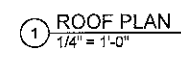
SEAGATE VILLAS
1017 BUCIDA ROAD
DELRAY BEACH, FL

RANDALL STOFFT
ARCHITECTS
distinctive. inspirational. architecture.
400 N. SPENCER AVE. DELRAY BEACH, FL 33445-2020 • PHONE: 561.321.2222 • WWW.RSARCHITECTS.COM

PL-000000 E100-A-000000

UNIT C
FLOOR PLANS

A203

[illegible]

RANDALL STOFFT
ARCHITECTS
distinctive, inspirational architecture.

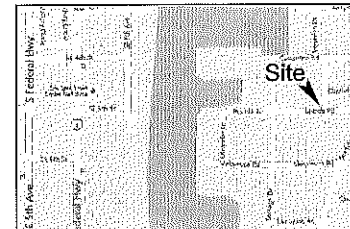
SEAL.

F1-01125 FIRM-A408377

ROOF PLAN

A204

Drawing No.
LP-2
QF 2



LOCATION MAP
N.T.S.



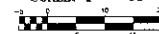
NOT FOR CONSTRUCTION

LEGEND:

- | PROPERTY LINE | |
|---------------|--|
| | SECTION IDENTIFIER |
| | PLAN PAGE |
| | DETAIL PAGE |
| | PROPOSED ELEVATION |
| | DIRECTION OF FLOW |
| | EXISTING ELEVATION |
| | DENOTES AREA OF PROPOSED BRICK PAVING |
| | DENOTES AREA OF PROPOSED CONCRETE PAVING |
| | DENOTES AREA OF EXIST PAVEMENT TO BE REMOVED |
| | DENOTES AREA OF 5' ACCESS EASEMENT |
| | PROPOSED CATCH BASIN |
| | HIGH-DENSITY POLYETHYLENE PIPE |
| | DENOTES 12" PERFORATED PIPE IN 6" WIDE X 3' DEEP ROCK TRENCH (BOTTOM EL. = 1.50) |
| | STRUCTURE NUMBER |
| | RM ELEVATION |
| | INVERT ELEVATION |
| | PROPOSED WATER METER |
| | EXISTING MANHOLE |
| | C.O. |
| | SINGLE SANITARY SEWER WITH CLEANOUT |

CALL 48 HOURS BEFORE YOU D.G.
IT'S THE LAW
1-800-432-4770
SUNSHINE STATE ONE CALL OF FLORIDA, INC.

SCALE: 1" = 10'



© COPYRIGHT 2016 BY ENVIRODESIGN ASSOCIATES, INC.
THIS DRAWING IS PROVIDED FOR INFORMATIONAL PURPOSES ONLY.
UNLESS SIGNED AND SEALED BY A REGISTERED PROFESSIONAL
ENGINEER REPRESENTING ENVIRODESIGN ASSOCIATES, INC.

**NOTE: CONTRACTOR TO FIELD
VERIFY EXACT LOCATION, SIZE, AND
DEPTH OF ALL EXISTING UTILITIES
AT TIME OF CONSTRUCTION AND
REPORT ANY DISCREPANCIES TO
ENVIRODESIGN ASSOCIATES, INC.**



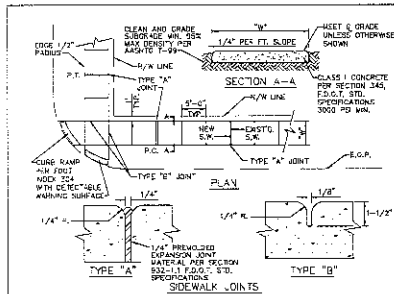
Know what's below.
Call before you dig.

PAVING & DRAINAGE PLAN FOR:

PAVING & DRAINAGE PLAN FOR:

Environ Biol Fish

DRA
T.J.
CRE
J.A.
DAT
4/
JOE
151
SHE

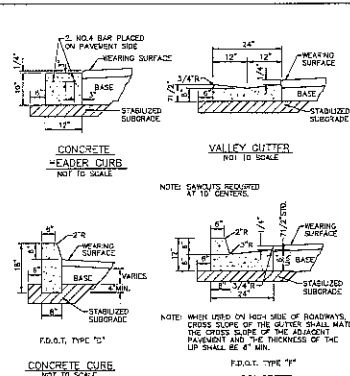


- NOTE: 1. ALL SIDEWALKS SHALL BE CONSTRUCTED FROM DRIVEWAYS.
2. ALL SIDEWALKS SHALL INCLUDE ADA COMPLIANT RAMPS @ INTERSECTIONS.
3. ALL SIDEWALKS SHALL INCLUDE CROSS SLOPE AND RUNNING SLOPE @ INTERSECTIONS WITH ADA HIGHWAYS, ADA COMPLIANT RAMPS @ INTERSECTIONS.
4. 6\"/>

TABLE OF SIDEWALK THICKNESS - "T"
RESIDENTIAL AREAS
WITHIN 10' OF CROSS-STREETS, AT DRIVEWAYS & OTHER AREAS
TABLE OF SIDEWALK WIDTHS - "W"
SINGLE-FAMILY AREAS
MULTI-FAMILY AREAS
OTHER AREAS AS SPECIFIED BY CITY ENGINEER

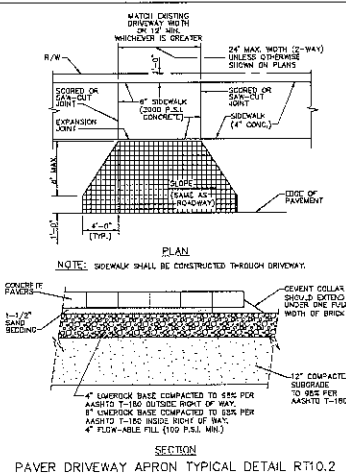
TABLE OF SIDEWALK JOINTS	LOCATION
"A"	P.C. AND P.T. OF CURVES
"B"	6'-0" CENTER TO CENTER ON DRIVEWAYS SCORED DURING PLACEMENT OR SAWCUT WITHIN 24 HOURS OF PLACEMENT
"A"	WHERE SIDEWALK ABUTS CONCRETE CURBS, DRIVEWAYS, AND SIMILAR STRUCTURES

SIDEWALK CONSTRUCTION R-5.1

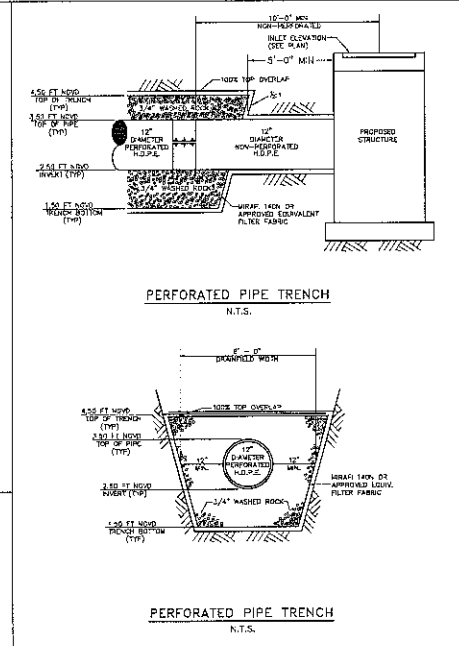


- NOTE: 1. ROADWAY SURFACING SHALL IN ALL CASES EXTEND BELOW CURBING.
2. SAWCUTS AT 10' CENTERS SHALL BE MADE WITHIN 24 HOURS OF CONCRETE PLACEMENT.

CURB AND GUTTER SECTIONS RT 6.1



PAVER DRIVEWAY APRON TYPICAL DETAIL RT10.2



PERFORATED PIPE TRENCH N.T.S.

NOT FOR CONSTRUCTION

PAVING & DRAINAGE DETAILS FOR:
SEAGATE VILLAS
BOCIDA ROAD & CORALITA LANE
DELRAY BEACH, FLORIDA

ENGINEERS - ENVIRONMENTAL CONSULTANTS
240 Ponce de Leon Blvd., Suite 334
Deerfield Beach, Florida 33444
Phone: (561) 271-1250 Fax: (561) 271-0558

DRAWN:
T.J.L.
CHECKED:
J.A.P.
DATE:
5/08/16
JOB NO.
15117-DTL
SHEET NO.
2 OF 5

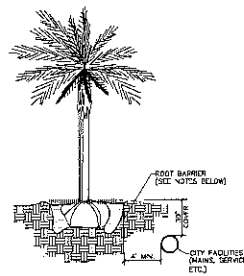
© COPYRIGHT 2016 BY ENVIRONMENTAL ASSOCIATES, INC.
THIS DRAWING IS PROVIDED FOR INFORMATIONAL PURPOSES ONLY.
UNLESS SIGNED AND SEALED BY A REGISTERED PROFESSIONAL
ENGINEER REPRESENTING ENVIRONMENTAL ASSOCIATES, INC.

PAVING, GRADING AND DRAINAGE NOTES

- ALL PAVING AND DRAINAGE WORK IN THE PALM BEACH COUNTY (PBC) RIGHT-OF-WAY SHALL BE CONSTRUCTED IN FULL ACCORDANCE WITH THE PBC LATEST STANDARDS. ALL PAVING AND DRAINAGE WORK IN THE FLORIDA DEPARTMENT OF TRANSPORTATION (FDOT) RIGHT-OF-WAY SHALL BE CONSTRUCTED IN FULL ACCORDANCE WITH THE FDOT LATEST STANDARDS.
- COMPACTED SUBGRADE SHALL BE COMPACTED AND MEET THE DENSITY REQUIREMENTS AS DETERMINED BY THE AASHTO T-99 SPECIFICATIONS. SUBGRADE SHALL EXTEND 12 INCHES BEYOND THE PROPOSED EDGE OF PAVEMENT AND/OR 18" BEYOND VALLEY CUTTER, TYPE "C" CURB & GUTTER, OR TYPE "D" CURB. SUBGRADE SHALL BE 12 INCHES THICK COMPACTED TO 98% ACCORDING TO AASHTO T-180. ALL MUCK, STUMPS, ROOTS OR OTHER DELETERIOUS MATTER ENCOUNTERED IN THE PREPARATION OF THE SUBGRADE SHALL BE REMOVED COMPLETELY FROM THE CENTERLINE OF THE ROADWAY TO A WIDTH OF TEN FEET BEYOND THE EDGE OF PAVEMENT. IF SUBGRADE IS REQUIRED TO BE STABILIZED, THE REQUIRED BEARING VALUE DETERMINATIONS SHALL BE MADE BY THE FLORIDA BEARING TEST, TEST METHOD "C" OF AASHTO T-180 SPECIFICATIONS.
- SHELLROCK BASE SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 330 OF THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION. THE THICKNESS OF THE SHELLROCK BASE SHALL BE AS SHOWN ON THE DETAIL DRAWINGS AND SHALL BE COMPACTED TO A DENSITY OF 98% OF THE MAXIMUM DENSITY AS DETERMINED BY AASHTO T-180. THE SHELLROCK SHALL HAVE A MINIMUM OF 40% CARBONATES OF CALCIUM AND MAGNESIUM IN THE MATERIAL.
- LIMESTONE BASE SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 330 OF THE FOOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION. THE THICKNESS OF THE LIMESTONE BASE SHALL BE AS SHOWN ON THE DETAIL DRAWINGS AND SHALL BE COMPACTED TO A DENSITY OF 98% OF THE MAXIMUM DENSITY AS DETERMINED BY AASHTO T-180.
- MUCK AND PEAT - IF MUCK AND/OR PEAT ARE ENCOUNTERED IN THE ROAD OR PARKING AREA, THEY WILL BE REMOVED COMPLETELY TO A WIDTH OF TEN FEET BEYOND THE EDGE OF PAVEMENT AND SHALL BE BACKFILLED WITH GRANULAR MATERIAL. IF "CARPANS" ARE ENCOUNTERED IN THE SHALE AREA, IT WILL BE REMOVED TO A WIDTH OF TWO FEET AT THE SHALE INVERT AND REPLACED WITH GRANULAR MATERIAL.
- WHY? SO IS DESIRED, LOWER THE GRADE 3 INCHES BELOW THE FINISHED GRADE TO ALLOW FOR THE THICKNESS OF THE SOIL.
- CLEARING AND GRUBBING - WITHIN THE LIMITS OF CONSTRUCTION ALL VEGETATION AND ROOT MATERIAL SHALL BE REMOVED.
- CLIMB - WHERE CLIMB OR OTHER PLASTIC CLAYS ARE ENCOUNTERED, THEY SHALL BE REMOVED WITHIN THE ROADWAY AND PARKING AREAS ONE FOOT BELOW THE SUBGRADE, EXTENDING HORIZONTALLY TO THE OUTSIDE EDGE OF THE SHOULDER AREA.
- PRIME COAT SHALL BE IN ACCORDANCE WITH SECTION 300 OF THE FOOT STANDARD SPECIFICATION FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION. PRIME COAT SHALL BE FC-70 OR ASPHALT EMULSION PRIME (AEP). PRIME COAT SHALL BE APPLIED AT A RATE OF APPLICATION NOT LESS THAN 0.10 GALLONS PER SQUARE YARD FOR LIMELOCK BASES AND NOT LESS THAN 0.15 GALLONS PER SQUARE YARD FOR SHELL ROCK OR COQUINA SHELL BASES.
- TACK COAT - BITUMINOUS TACK COAT SHALL CONFORM WITH THE REQUIREMENTS OF THE FOOT SPECIFICATIONS, LATEST EDITION AND SHALL BE APPLIED AT THE RATE OF 0.08 GALLONS PER SQUARE YARD, UNLESS A VARIATION IS APPROVED BY THE ENGINEER.
- ASPHALTIC CONCRETE SURFACE COURSE SHALL BE IN ACCORDANCE WITH SECTION 334 OF THE FOOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION. THE THICKNESS OF THE SURFACE COURSE SHALL BE AS SHOWN ON THE DETAIL DRAWINGS.
- ALL CONCRETE SHALL DEVELOP 2500 P.S.I. (MINIMUM) 28 DAY COMPRESSIVE STRENGTH OR GREATER WHERE NOTED ON PLANS. CLASS I CONCRETE SHALL CONFORM WITH THE FDOT SPECIFICATIONS, LATEST EDITION. CLASS I CONCRETE USED AS PAVING SHALL DEVELOP 3000 P.S.I. (MINIMUM) 28 DAYS COMPRESSIVE STRENGTH.
- PAVEMENT TRAFFIC STRIPES AND MARKINGS SHALL BE IN ACCORDANCE WITH CITY OF DELRAY BEACH CURRENT STANDARDS AND/OR MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, AND/OR PALM BEACH COUNTY TRAFFIC DEPARTMENT TYPICAL T-F-10-001, WHERE APPLICABLE.
- SEQUENCE OF CONSTRUCTION - THE SEQUENCE OF CONSTRUCTION SHALL BE SUCH THAT ALL UNDERGROUND INSTALLATIONS OF EVERY KIND THAT ARE BENEATH THE PAVEMENT, SHALL BE IN PLACE AND HAVE PROPER DENSITY PRIOR TO THE CONSTRUCTION AND COMPLETION OF THE SUBGRADE.
- INLETS AND MANHOLES - ALL INLETS AND MANHOLES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE FDOT SPECIFICATIONS, LATEST EDITION, AND ROADWAY AND TRAFFIC DESIGN STANDARDS, LATEST EDITION, AND PBC STANDARDS LATEST EDITION, WHERE APPLICABLE.
- GRATE ELEVATIONS REFER TO THE FLOWLINE OF THE GRATE.
- GRADES SHOWN ARE FINISHED GRADES.
- MINIMUM ROAD CROWN ELEVATION SHALL BE 0.1 V/L.G.V.D.
- REINFORCED CONCRETE PIPE - THE PIPE SHALL CONFORM WITH THE REQUIREMENTS OF CLASS II OF AASHTO M-29 AND WITH THE FDOT SPECIFICATIONS, LATEST EDITION. THE FIRST JOINT OF PIPE ENTERING OR LEAVING A MANHOLE OR INLET SHALL BE WRAPPED WITH FILTER FABRIC PER FDOT SPECIFICATIONS.
- CORRUGATED ALUMINUM PIPE SHALL MEET THE REQUIREMENTS OF AASHTO M-198 AND SECTIONS 124, 430 AND 843 OF THE FOOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.
- MEASUREMENT OF THE LENGTH OF PIPE SHALL BE FROM THE CENTER OF THE STRUCTURE TO THE CENTER OF THE STRUCTURE.
- PIPE BACKFILL - REQUIREMENTS FOR PIPE BACKFILL CROSSING ROADS OR PARKING AREAS SHALL BE DEFINED IN THE FDOT SPECIFICATIONS, LATEST EDITION. PIPELINE BACKFILL SHALL BE PLACED IN SIX INCH LIFTS AND COMPACTED TO 100% OF THE STANDARD PROCTOR (AASHTO) T-99 SPECIFICATIONS.
- THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER THREE (3) CERTIFIED COPIES OF A CHEMICAL AND SIEVE ANALYSIS OF THE SELL ROCK BY A STATE OF FLORIDA CERTIFIED LAB WHEN CONSTRUCTING A SELL ROCK BASE FOR ROADWAY.
- PRIOR TO CERTIFICATION OF THE DRAINAGE SYSTEM, THE CONTRACTOR MUST PUMP DOWN AND LAMP THE DRAINAGE SYSTEM FOR INSPECTION BY THE ENGINEER AFTER FINISHED ROCK FOR THE ROADWAYS HAS BEEN INSTALLED.

GENERAL NOTES

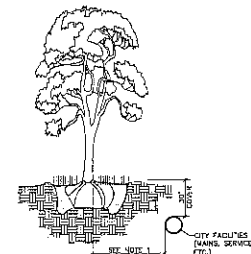
- CONTRACTOR SHALL PROTECT ALL PERMANENT REFERENCE MONUMENTS AND TAKE ALL PRECAUTIONS NECESSARY TO AVOID SURVEY MARKERS DURING CONSTRUCTION. CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACEMENT OF PERMANENT REFERENCE MONUMENTS DISTURBED DURING CONSTRUCTION, AT NO ADDITIONAL COST TO THE OWNER.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UNDERGROUND UTILITIES WHETHER OR NOT SHOWN ON THE PLANS. UTILITIES SHOWN ON THE PLANS ARE FOR REFERENCE ONLY AND MAY NOT INDICATE ACTUAL LOCATIONS. CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES FOR LOCATION AND DEPTH OF THEIR LINES PRIOR TO CONSTRUCTION.
- CONTRACTOR SHALL BE RESPONSIBLE TO COORDINATE AND PAY FOR THE REEMERGING OF POWER LINES AND HOLDING POWER TOLES DURING CONSTRUCTION AT NO ADDITIONAL COST TO THE OWNER.
- CONTRACTOR SHALL BE RESPONSIBLE TO COORDINATE AND PAY FOR THE REPLACEMENT OF TRAFFIC LOOPS IF DAMAGED DURING CONSTRUCTION AT NO ADDITIONAL COST TO THE OWNER.
- ALL ELEVATIONS REFER TO M.G.D. 1928. MINIMUM FINISHED FLOOR ELEVATION FOR ANY RESIDENTIAL STRUCTURE SHALL BE SET AT OR ABOVE ELEVATION 8.0' (WHICH IS ABOVE THE 100-YEAR FREQUENCY STORM).
- SHOP DRAWINGS SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW PRIOR TO CONSTRUCTION OR INSTALLATION AS REQUIRED BY THE ENGINEER OR BY THE APPROPRIATE GOVERNING AGENCIES.
- CONTRACTOR SHALL BE RESPONSIBLE TO SECURE AND PAY FOR, AT NO ADDITIONAL COST TO THE OWNER, ALL NECESSARY PERMITS, FEES AND LICENSES.
- CONTRACTOR SHALL BE RESPONSIBLE FOR GIVING THE REQUIRED NOTICES AND COMPLYING WITH ALL PERMITS CONDITIONS.
- CONTRACTOR SHALL BE RESPONSIBLE TO COORDINATE AND PAY FOR THE HOLDING OF OTHER UTILITY POLES DURING CONSTRUCTION AT NO ADDITIONAL COST TO THE OWNER.
- ALL CONCRETE SHALL DEVELOP 2500 P.S.I. (MINIMUM) 28 DAY COMPRESSIVE STRENGTH OR GREATER WHERE NOTED ON PLANS. CLASS I CONCRETE SHALL CONFORM WITH THE FDOT SPECIFICATIONS, LATEST EDITION. CLASS I CONCRETE USED AS PAVING SHALL DEVELOP 3000 P.S.I. (MINIMUM) 28 DAYS COMPRESSIVE STRENGTH.
- ALL UTILITY CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE LOCAL UTILITY.
- ENVIRONMENTAL ASSOC. INC. SHALL NOT BE RESPONSIBLE FOR THE SAFETY OF THE WORKERS OR THE GENERAL PUBLIC. CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING SAFETY TO THE WORKERS IN ACCORDANCE WITH OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) REQUIREMENTS AND THE SAFETY OF THE GENERAL PUBLIC.
- ALL MATERIALS AND WORKMANSHIP MUST BE IN ACCORDANCE WITH THE LOCAL UTILITY STANDARDS AND SPECIFICATIONS.
- THE CONTRACTOR SHALL CALL SURVEYING 1-800-432-4770 48 HOURS BEFORE DIGGING FOR FIELD LOCATIONS OF UNDERGROUND UTILITIES.
- CORRUGATED ALUMINUM PIPE USED BENEATH PAVEMENT SHALL BE IN CONFORMANCE WITH SEC. 8.2.4.2.2.1.
- ALL CONSTRUCTION ACTIVITY, INCLUDING TRENCHING, IS TO BE A MINIMUM OF SIX FEET FROM THE BASE OF ANY TREE THAT IS DESIGNATED TO REMAIN.
- ALL LIMBROCK AND BASE MATERIALS SHALL BE REMOVED FROM THE PLANTER AREAS/ISLANDS AND REPLACED WITH APPROPRIATE PLANTING SOIL PRIOR TO THE LANDSCAPING OF THE SITE.
- THE REMOVAL OF ANY TREE ON THE SITE IS PROHIBITED WITHOUT THE REQUIRED PERMITS.
- ALL MATERIALS AND CONSTRUCTION WITHIN THE FDOT RIGHT-OF-WAY SHALL CONFORM TO THE FDOT DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM (LATEST ED.).
- LANE CLOSURES WITHIN THE FDOT RIGHT-OF-WAY SHALL BE LIMITED TO THE HOURS OF 5AM-3PM, MONDAY-FRIDAY EXCLUDING GOVERNMENTAL HOLIDAYS. MAINTENANCE OF TRAFFIC SHALL BE PROVIDED IN ACCORDANCE WITH INDEX 613 OF THE FDOT DESIGN STANDARDS (LATEST ED.).



NOTES:

- ALL ROOT BARRIERS SHALL BE 4' MINIMUM FROM ALL CITY FACILITIES.
- THE INSTALLATION OF ROOT BARRIERS SHALL BE COORDINATED WITH CITY AND NOTICED BY CITY PRIOR TO BACKFILLING. ALL ROOT BARRIERS SHALL EXTEND UP TO FINISHED GRADE.
- ROOT BARRIERS SHALL BE MINIMUM 36" DEEP. APPROVED PRODUCTS INCLUDE: ROOT STOP AND ROOT STOP-TECH. FLOODEE BARRIERS SHALL BE OF PANELS MANUFACTURED BY BIOBARRIER.
- ALL ROOT BARRIERS SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S INSTALLATION INSTRUCTIONS.

TYPICAL TREE WITH ROOT BARRIER LD 1.1



NOTES:

- THIS DISTANCE SHALL BE 10' MINIMUM FROM ALL CITY FACILITIES IF NO ROOT BARRIER IS USED.

TYPICAL TREE WITHOUT ROOT BARRIER LD 1.2

NOT FOR CONSTRUCTION

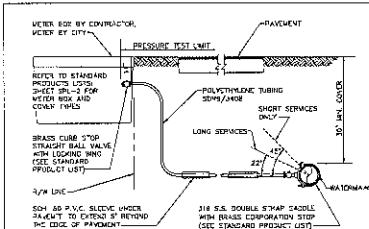
PAVING & DRAINAGE DETAILS FOR:

SEAGATE VILLAS
BOCADA ROAD & CORALITA LANE
DELRAY BEACH, FLORIDA

ENGINEERS - ENVIRONMENTAL CONSULTANTS
2300 Delray Beach Blvd., Suite 101
Delray Beach, FL 33444
Phone: (561) 271-8855 Fax: (561) 271-8858

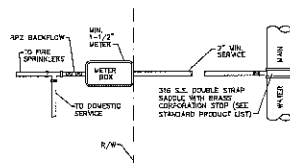
DRAWN:
T.J.L.
CHECKED:
J.A.P.
DATE:
5/09/16
JOB NO.
16117-DTL
SHEET NO.
3 OF 5

© COPYRIGHT 2016 BY ENVIRONMENTAL ASSOCIATES, INC.
THIS DRAWING IS PROVIDED FOR INFORMATIONAL PURPOSES ONLY.
UNLESS SHOWN AND SIGNED BY REGISTERED PROFESSIONAL
ENGINEER REPRESENTING ENVIRONMENTAL ASSOCIATES, INC.



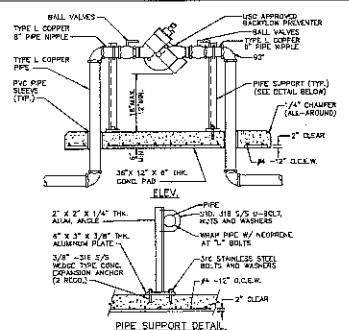
- NOTES:**
- SUCCESSIVE TAPS INTO THE WATER MAIN SHALL BE SPACED A MINIMUM OF 10' ON SICES.
 - SERVICE LINES SHALL NOT BE PLACED UNDER DRIVEWAYS.
 - ALL METERS, REQUIRE A LOCKING BRASS CURB STOP WITH LOCK WING (1" MIN).
 - NO FITTINGS BETWEEN CORPORATION STOP AND BRASS ASSEMBLY.
 - MAXIMUM SERVICE LENGTH TO METER.
 - CASING PIPE I.D. SHALL BE SERVICE O.D. PLUS 1" MINIMUM.
 - ON ALL SERVICES BEING METERS.
 - METER SIZE WILL BE DETERMINED BY PUBLIC UTILITIES DEPT. UPON APPLICATION FOR SERVICE.
 - ALL VALVES TO BE BALL VALVES.
 - METER BOX SHALL BE PROVIDED AND INSTALLED BY CONTRACTOR.

TYPICAL SERVICE CONNECTION PW 5.1c



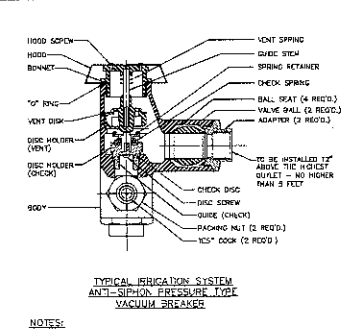
- NOTES:**
- WATER SERVICE LINES TO THE WATER MAIN SHALL BE SPACED A MINIMUM OF 10' ON CENTER.
 - SERVICE LINES SHALL NOT BE PLACED UNDER DRIVEWAYS.
 - ALL SERVICE LINES REQUIRE A LOCKING BRASS CURB STOP WITH LOCK WING (1" MIN).
 - NO FITTINGS BETWEEN CORPORATION STOP AND BRASS ASSEMBLY.
 - MAXIMUM SERVICE LENGTH IS 100' TO METER.
 - CASING PIPE I.D. SHALL BE SERVICE O.D. PLUS 1" MINIMUM.
 - ON ALL SERVICES BEING METERS.
 - METER SIZE WILL BE DETERMINED BY PUBLIC UTILITIES DEPT. UPON APPLICATION FOR SERVICE.
 - ALL VALVES TO BE BALL VALVES.
 - METER BOX SHALL BE PROVIDED AND INSTALLED BY CONTRACTOR.

TYPICAL FIRE SERVICE CONNECTION PW 9.1c



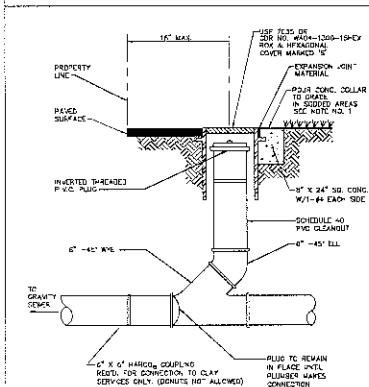
- NOTES:**
- FOR ALL SERVICES LESS THAN OR EQUAL TO 2" DIA.
 - ABOVE GRADE PIPING SHALL BE BRASS OR TYPE "C" COPPER TUBING.
 - ALL COPPER JOINTS SHALL BE MADE WITH 95/5 SOLDER.
 - PIPE BACKFLOW PREVENTER IS REQUIRED IN ACCORDANCE WITH CITY OF DELRAY BEACH CODE OF ORDINANCES TITLE 16, CHAPTER 21.80.
 - USE OF OTHER PIPE MATERIALS WITH APPROVAL OF DEPUTY DIRECTOR OF UTILITIES.
 - PIPE BACKFLOW PREVENTER IS REQUIRED FOR ALL COMMERCIAL PERMITS AND ALL RESIDENTIAL PERMITS WITH THE SPRINKLER SYSTEM.

REDUCED PRESSURE ZONE BACKFLOW PREVENTER PW 10.2



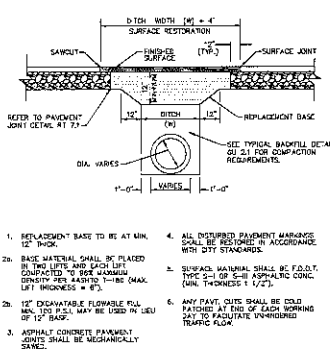
- NOTES:**
- WATTS REGULATOR COMPANY OR EQUIVALENT.
 - VALVE SHALL MEET OR EXCEED THE A.S.S.F., P.S.A. AND U.L.C.I. REQUIREMENTS.
 - SHALL ALSO BE LISTED BY IAPMO (NPS).
 - USE OF OTHER PIPE MATERIALS WITH APPROVAL OF DEPUTY DIR. OF UTILITIES.
 - ABOVE GRADE PIPING SHALL BE P.V.C. OR COPPER.

ANTI-SIPHON PRESSURE TYPE VACUUM BREAKER PW 10.3



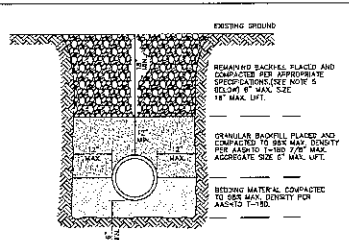
- NOTES:**
- INVERTED BRASS CAP SHALL BE USED IN GRASS AREAS IN LEU OF CASTING AND CONE COLLAR.
 - MINIMUM DEPTH OF SERVICE LATERA, SHALL BE 2 FEET.

TYPICAL COMMERCIAL CLEANOUT VW 5.2



- NOTES:**
- REPLACEMENT BASE TO BE AT MIN. 12" THICK.
 - BASE MATERIAL SHALL BE PLACED IN THE LEFT AND RIGHT LANE COMPACTED TO 95% MAXIMUM DENSITY PER AASHTO T-99 (MIN. 1" THICKNESS = 6").
 - 12" EXCAVATION FILLABLE FULL MIN. 12" P.S.D. MAY BE USED IN LEU OF 12" BASE.
 - ASPHALT CONCRETE PAVEMENT JOINTS SHALL BE NEARLYLY SAME.
 - ALL EXISTING PAVEMENT MATERIALS SHALL BE REMOVED IN ACCORDANCE WITH CITY STANDARDS.
 - SURFACE MATERIAL SHALL BE F.O.D.T. TYPE 2-1.1 OR 5-1.1 ASPHALTIC CONC. (MIN. 1" THICKNESS = 1 1/2").
 - ANY PAINT, CURB SHALL BE COLD PATCHED AT END OF EACH MORNING DAY TO FACILITATE UNHINDERED TRAFFIC FLOW.

PAVEMENT REPAIR DETAIL GU 1.1



- NOTES:**
- UNDESIRABLE IN-SITU MATERIALS SUCH AS MUD, SPILLAGE AND LARGE ROCKS SHALL BE REMOVED. BEDDING MATERIAL AND BACKFILL, CONSISTING OF WASHED AND CRUSHED UNDER 3/4" 7/8" SAND.
 - THE PIPE AND/OR STRUCTURE SHALL BE FULLY SUPPORTED FOR ITS ENTIRE LENGTH WITH APPROPRIATE COMPACTION UNDER THE PIPE HANDHOLES.
 - THE PIPE AND/OR STRUCTURE SHALL BE PLACED IN A DRY TRENCH.
 - BACKFILL SHALL BE FREE OF UNSUITABLE MATERIAL SUCH AS LARGE ROCKS, MUD, AND DEBRIS.
 - COMPACT BACKFILL TO 95% DENSITY UNDER PAVEMENT AND TO 95% DENSITY ELSEWHERE (AASHTO T-99).
 - COMPACTION AND DENSITY TESTS SHALL BE COMPLETED DURING BACKFILL OPERATION. CORRECTIONS NOT FOLLOWING THIS PROCEDURE FOR WATER REDUCTION SHALL BE REQUIRED TO RE-EXCAVATE THE AREA IN QUESTION DOWN TO THE BEDDING MATERIAL, THEN BACKFILL FOLLOWING THE ABOVE PROCEDURES.

TYPICAL BACKFILL DETAIL GU 2.1

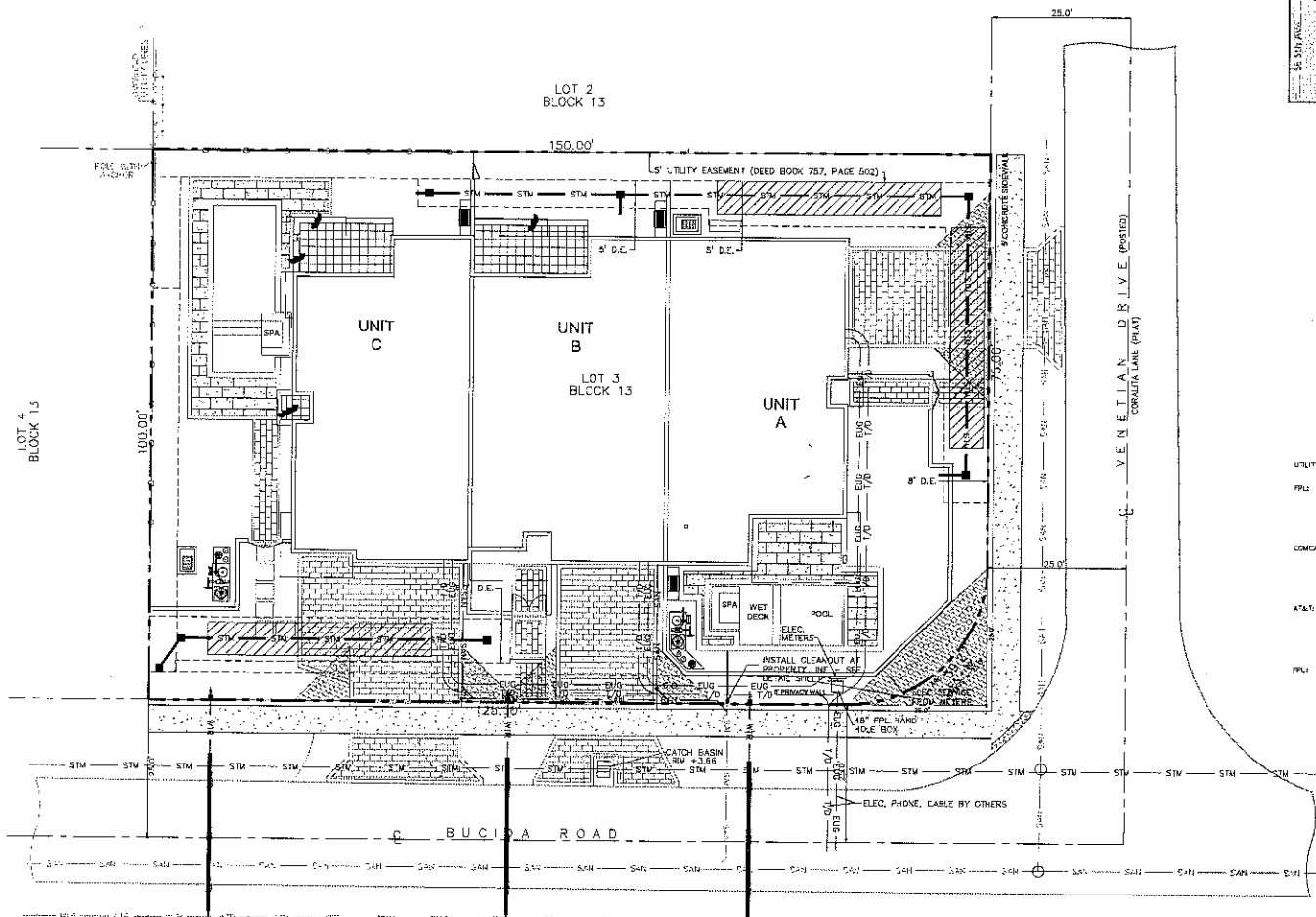
NOT FOR CONSTRUCTION

WATER & WASTEWATER DETAILS FOR:
SEAGATE VILLAS
BOCADA ROAD & CORALITA LANE
DELRAY BEACH, FLORIDA

EnviroDesign Associates, Inc.
ENGINEERS • ENVIRONMENTAL CONSULTANTS
FLORIDA CERTIFICATE OF PROFESSIONAL ENGINEERING NO. 12444
PHONE (561) 274-4000 FAX (561) 274-8558

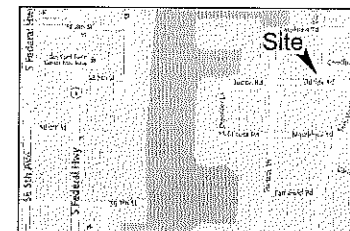
DRAWN:
T.J.L.
CHECKED:
J.A.P.
DATE:
3/09/16
JOB NO.
15117-DTL
SHEET NO.
4 OF 5

© COPYRIGHT 2016 BY ENVIRONMENTAL ASSOCIATES, INC.
THIS DRAWING IS PROVIDED FOR INFORMATIONAL PURPOSES ONLY.
UNLESS NOTED AND SHOWN BY A REGISTERED PROFESSIONAL
ENGINEER REPRESENTING ENVIRONMENTAL ASSOCIATES, INC.



LEGEND:

---	PROPERTY LINE	---	EXISTING STORM LINE
STM	PROPOSED STORM LINE	SAN	EXISTING SANITARY LINE
SAN	PROPOSED SANITARY LINE	WTR	EXISTING WATER LINE
WTR	PROPOSED WATER LINE	---	EXISTING OVERHEAD LINES
EUG	PROPOSED ELECTRIC UNDERGROUND LINE	---	EXISTING GAS LINES
GAS	PROPOSED GAS LINE		



LOCATION MAP
N.T.S.



NOT FOR CONSTRUCTION

UTILITY PROVIDER SIGNATURE:

FPL	SIGNATURE	TITLE
	PRINT NAME	DATE
COMCAST	SIGNATURE	TITLE
	PRINT NAME	DATE
AT&T	SIGNATURE	TITLE
	PRINT NAME	DATE
FPL	SIGNATURE	TITLE
	PRINT NAME	DATE

CALL 48 HOURS BEFORE YOU DIG.
IT'S THE LAW
1-800-432-4770
SUNSHINE STATE ONE CALL OF FLORIDA, INC.

SCALE: 1" = 10'
1 10 20



© COPYRIGHT 2016 BY EXPANSION ASSOCIATES, INC.
THIS DRAWING IS INTENDED FOR INFORMATIONAL PURPOSES ONLY.
UNLESS SHOWN AND SCALED BY A REGISTERED PROFESSIONAL
ENGINEER REPRESENTING EXPANSION ASSOCIATES, INC.

COMPOSITE UTILITY PLAN FOR:
SEAGATE VILLAS
BUCIDA ROAD & CORALITA LANE
DELRAY BEACH, FLORIDA

ENGINEERS • ENVIRONMENTAL CONSULTANTS
EnviroDesign Associates, Inc.
238 Alameda Street, Suite 200, Delray Beach, FL 33444
Phone: (561) 271-1555 Fax: (561) 271-1555

DRAWN:
B.A.B.
CHECKED:
J.A.P.
DATE:
4/11/16
JOB NO.
15117-ENG
SHEET NO.
1 OF 1