



Kiewit



RFQ 2025-023

CMAR

TROPIC ISLE NEIGHBORHOOD
IMPROVEMENTS

PROJECT NO. 19-015

CITY OF DELRAY BEACH, FL

Kiewit Infrastructure South Co.
6301 NW 5th Way, Suite 3200
Ft Lauderdale, FL 33309

TAB 1 4.2.1. INTRODUCTION

April 9, 2025

Casetra Thompson

City of Delray Beach Purchasing and Contract Administration Division
100 NW 1st Avenue
Delray Beach, FL 33444

RE: RFQ NO.: 2025-023 Tropic Isle Neighborhood Improvements Project No. 19-015 Construction Manager at Risk

Dear Casetra Thompson and members of the Selection Committee:

Kiewit Infrastructure South Co. (KISC), a wholly owned subsidiary of Kiewit Corporation (Kiewit), is eager for the opportunity to partner with the City of Delray Beach (the City) to deliver improvements to the Tropic Isle neighborhood. Given the unique challenges posed by underlying muck soils, flood-prone conditions, outfall reconstruction, and the need to balance pavement rehabilitation with stormwater and utility upgrades within a residential community, the Construction Manager at-Risk (CMAR) delivery model is the most effective approach. As a trusted partner delivering successful CMAR projects in Florida and throughout North America, KISC offers the City a **transparent and collaborative partner** with the experience and resources necessary to maximize the return on investment to the City through the CMAR process. By integrating construction expertise early in the design phase, we can integrate with the City and the Kimley-Horn and Associates Inc. (Kimley-Horn) design team to develop cost-effective solutions that minimize muck disturbance, optimize roadway elevations, minimize stakeholder inconvenience, and ensure long-term resilience against rising sea levels and high tides. Our team's experience with CMAR projects in established communities, coupled with a consistent public outreach approach, will ensure the successful execution of this critical Project while maintaining safe access through construction zones and minimizing impacts to residents.

UNWAVERING COMMITMENT TO THE COMMUNITY

The Tropic Isle Neighborhood Improvements Project (the Project) is in a high-end residential community where maintaining home access and protecting the waterways from runoff are critical. At KISC, we don't just build infrastructure—we build trust, with a personal stake in delivering the highest quality results. Our approach goes beyond traditional collaboration, using both City coordination and a boots-on-the-ground, door-to-door effort to ensure residents stay informed. We plan carefully, allocate resources strategically, and implement creative solutions to minimize disruptions. When you choose KISC, you're selecting a team that prioritizes both public and project success—one that delivers on commitments, communicates clearly, and leaves a lasting positive impact.

WHY KIEWIT? Our team brings **CMAR best practices, urban Florida experience, and schedule accuracy** to meet the City's goals on the Tropic Isle Neighborhood Improvements Project.

ESTIMATES AND SCHEDULES BASED ON:

>150
KIEWIT FLORIDA PROJECTS

\$5.7 B
IN VALUE OVER
THE PAST 10 YEARS

680+
CMAR & CM/GC
NORTH AMERICA PROJECTS

\$2.2 B
FLORIDA CMAR
PROJECTS

ENR 2024 RANKINGS

#1 DOMESTIC HEAVY
& TRANSPORTATION

#2 OVERALL TOP
CONTRACTOR

#24 TOP CMAR FIRM

PRECONSTRUCTION AND CONSTRUCTION MANAGEMENT SERVICES

In 2023, we delivered nearly \$9.5B in projects using the CMGC/DB/PDB methods. While Kiewit is often known for delivering major infrastructure projects, nearly a quarter of our annual work falls in the \$50M and under range in contract value, of a similar size and complexity to the City's Project. Many of these projects require managing residential access, drainage, and minimizing impacts to surrounding utilities. Through our extensive CMAR experience, we have developed several innovative approaches to the preconstruction phases of projects that help exceed our clients' goals and ensure the successful delivery during the construction phases of these projects. **First, our team utilizes a fully open-book approach regarding cost estimating, innovation tracking, and risk management.** From the onset, our team will develop a production-based, bottoms-up estimate and focus on areas where we can make the most significant impact on cost. **Next, our team will concentrate on innovation and risk mitigation by utilizing detailed tracking logs and tools that aid in decision-making.** As a true differentiator, our builders will be involved in every step of preconstruction and roll into the construction phase, making a seamless transition. Their critical right-time constructability input will reduce cost, improve quality, reduce maintenance, and aid on-time delivery of a successful Project.

COMMITMENT TO PROVIDING REQUIRED SERVICES

KISC understands that our role as the CMAR contractor is not to dictate design decisions but to serve as an essential bridge between the City, design team, and real-world construction execution. Given the neighborhood's coastal setting, variable soil conditions, and susceptibility to seasonal flooding, our input will ensure that design choices align with constructability, cost efficiency, and long-term performance. By focusing on where the highest costs and most construction risks are, we will help prioritize design elements that maximize value without compromising quality. Our extensive experience with similar projects allows us to anticipate potential material availability issues, permitting challenges, and sequencing constraints, offering practical solutions that keep the Project on schedule and within budget. Through close collaboration with the Kimley-Horn team and the City, we will help refine construction methodologies, mitigate risks, and ensure that the final design is not only feasible but optimized for a seamless and efficient build.

COMMITMENT TO RESULTS

KISC recognizes the critical importance of this Project and is fully committed to delivering the necessary services with efficiency, precision, and minimal disruption to the community. We understand the City's urgency in addressing aging infrastructure, and we bring the expertise to achieve schedule certainty without compromising quality. Our proven approach—rooted in transparent, open-book information sharing, proactive risk mitigation, and seamless stakeholder engagement—provides schedule certainty, reducing the potential for delays and rework. With a seasoned management team and a workforce that has successfully executed complex infrastructure projects across Florida and North America, we will apply best practices that drive efficiency, enhance public trust, and deliver a lasting solution. KISC is dedicated to making this a smooth, well-coordinated project that meets the City's needs while preserving the character and accessibility of the Tropic Isle neighborhood.

We understand that this Project is about more than just updating aging infrastructure—it's about trust, reliability, and the ability to deliver without disruption. KISC is ready to serve as the City's construction partner to deliver a project that is seamless, efficient, and built to serve the community for decades to come.

Please contact me at Tim.Cleary@kiewit.com or 770-487-2300 if you any questions while evaluating our proposal. Thank you for your time and consideration.

Sincerely,

Kiewit Infrastructure South Co.



Timothy J. Cleary
Senior Vice President

TAB 2 4.2.2. TABLE OF CONTENTS

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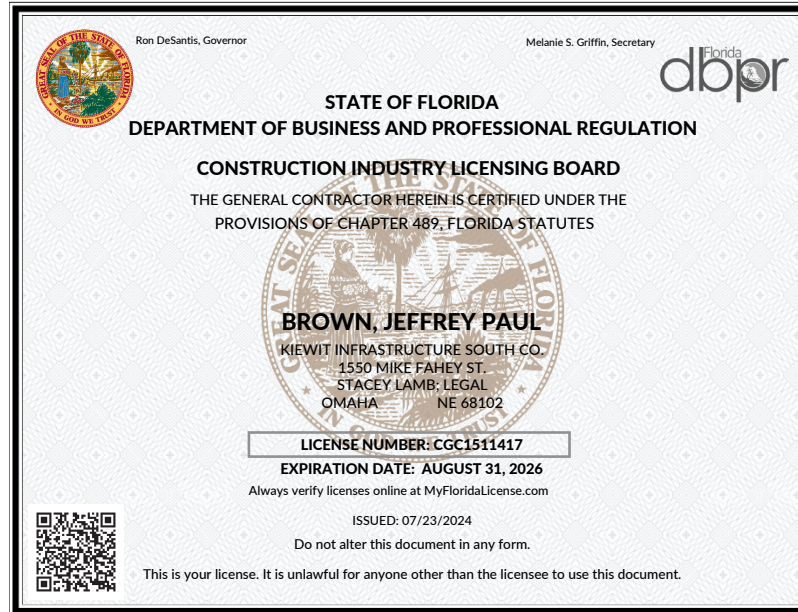
TAB 3
4.2.3. MINIMUM QUALIFICATIONS

4.2.3. MINIMUM QUALIFICATIONS

3.A. Affirmative Statement And Submission Of Evidence

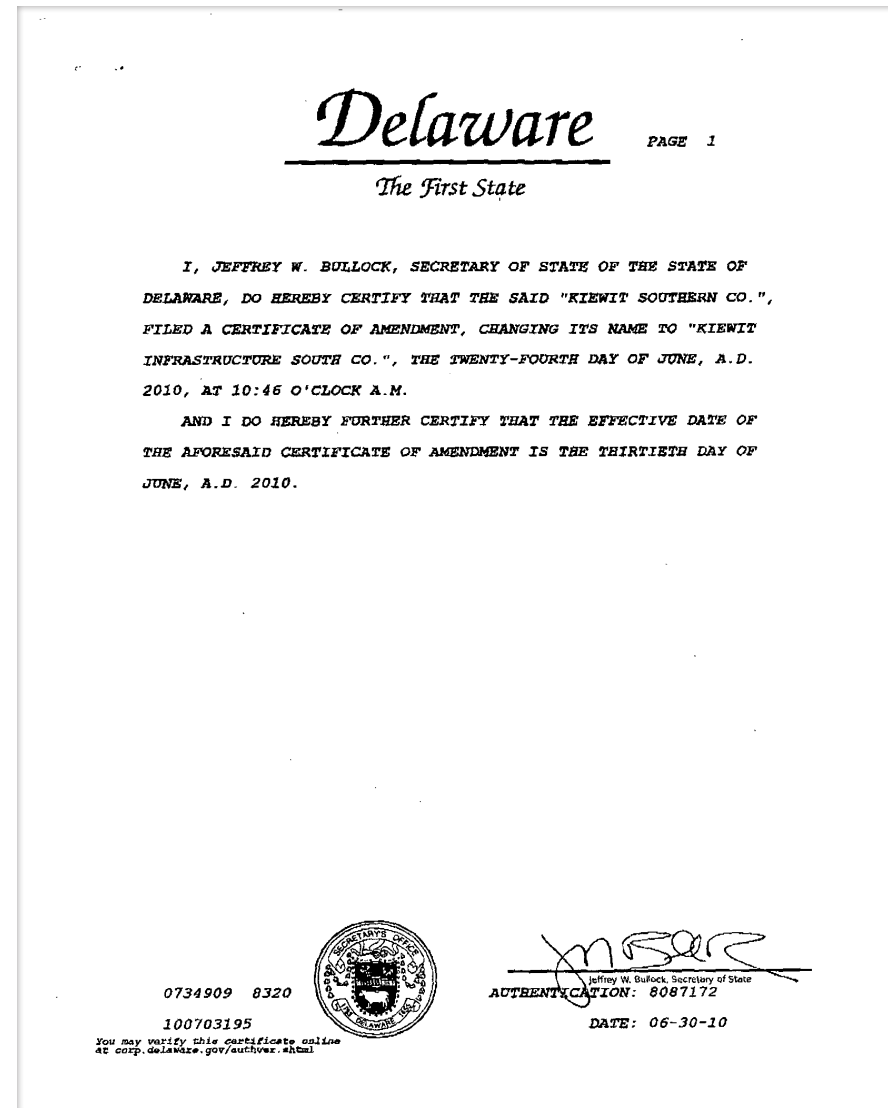
Kiewit Infrastructure South Co. confirms that all assigned key professional staff possess all licenses and certification required to undertake and complete the project. Please see 3.B, below, for General Contractor's License. We will provide additional licenses if requested.

3.B. Florida State General Contractor's License



3.C. Confirmation Of Business

Kiewit Infrastructure South Co. has worked under its current vendor name since 2010.



3.D. Bonding Capacity

Please see page 5.

3.E. Conflicts Of Interest

Kiewit Infrastructure South Co. does not have any conflicts of interest in relation to the RFQ.

3.F. AIA Document A305-2020 And All Exhibits

Please see pages 7-26.

3.G. State of Florida Division of Corporations

Kiewit Infrastructure South Co. is registered with the State of Florida Division of Corporations.

Hyperlink:

<https://search.sunbiz.org/Inquiry/CorporationSearch/SearchResultDetail?inquirytype=EntityName&directionType=Initial&searchNameOrder=KIEWITINFRASTRUCTURESOUTH%208373503&aggregateId=forp-837350-a6b21e4f-319f-41b5-b1dd-cf912d5593bc&searchTerm=Kiewit%20Infrastructure%20South%20Co.&listNameOrder=KIEWITINFRASTRUCTURESOUTH%208373503>

3.H. Florida State Board of Administration Scrutinized List.

Kiewit Infrastructure South Co. is not on the Florida State Board of Administration Scrutinized List of Prohibited Companies.



Travelers
Bond, Home Office
(860) 277-9355
(860) 277-3931 (fax)

One Tower Square
Hartford, CT 06183

March 24, 2025

City of Delray Beach
100 Northwest 1st Avenue
Delray Beach, FL 33444

RE: Tropic Isle Neighborhood Improvements
Kiewit Infrastructure South Co., Bonding Capacity

Dear Sir or Madam:

We have had the pleasure of extending surety credit to the Kiewit companies since 1958 in connection with contracts aggregating billions of dollars. As a Kiewit operating subsidiary, it is our considered opinion that Kiewit Infrastructure South Co. is one of the outstanding and reputable construction organizations in North America. Its skill, integrity, and financial responsibility are unquestioned.

As part of an overall work program commitment, we have authorized Kiewit Infrastructure South Co. to bid individual contracts up to \$350 million in size. The total program capacity for all Kiewit companies is \$15 billion. It is our intention to furnish Kiewit Infrastructure South Co. with Performance and Labor and Material Payment Bonds if awarded the above referenced project.

This commitment is subject to our standard underwriting at the time of the bond request, including a review of acceptable bond forms, contract financing and our standard underwriting considerations.

If you have any other questions, please feel free to contact me at (402) 271-2956.

Travelers Casualty and Surety Company of America,
A.M. Best Rating A++, XV

Traci Sutton
Attorney-in-Fact





**Travelers Casualty and Surety Company of America
Travelers Casualty and Surety Company
St. Paul Fire and Marine Insurance Company**

POWER OF ATTORNEY

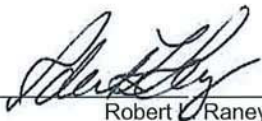
KNOW ALL MEN BY THESE PRESENTS: That Travelers Casualty and Surety Company of America, Travelers Casualty and Surety Company, and St. Paul Fire and Marine Insurance Company are corporations duly organized under the laws of the State of Connecticut (herein collectively called the "Companies"), and that the Companies do hereby make, constitute and appoint **Philip G. Dehn, Tammy Pike, Paul A. Foss, Marie Huggins, Traci Sutton, and Deanne Jones of Omaha, Nebraska**, their true and lawful Attorney (s)-in-Fact to sign, execute, seal and acknowledge any and all bonds, recognizances, conditional undertakings and other writings obligatory in the nature thereof on behalf of the Companies in the, r business of guaranteeing the fidelity of persons, guaranteeing the performance of contracts and executing or guaranteeing bonds and undertakings required or permitted in any actions or proceedings allowed by law.

IN WITNESS WHEREOF, the Companies have caused this instrument to be signed, and their corporate seals to be hereto affixed, this **21st** day of **April**, 2021.



State of Connecticut

City of Hartford ss.

By: 
Robert L. Raney, Senior Vice President

On this the **21st** day of **April**, 2021, before me personally appeared **Robert L. Raney**, who acknowledged himself to be the Senior Vice President of each of the Companies, and that he, as such, being authorized so to do, executed the foregoing instrument for the purposes therein contained by signing on behalf of said Companies by himself as a duly authorized officer.

IN WITNESS WHEREOF, I hereunto set my hand and official seal.

My Commission expires the **30th** day of **June**, 2026




Anna P. Nowik, Notary Public

This Power of Attorney is granted under and by the authority of the following resolutions adopted by the Boards of Directors of each of the Companies, which resolutions are now in full force and effect, reading as follows:

RESOLVED, that the Chairman, the President, any Vice Chairman, any Executive Vice President, any Senior Vice President, any Vice President, any Second Vice President, the Treasurer, any Assistant Treasurer, the Corporate Secretary or any Assistant Secretary may appoint Attorneys-in-Fact and Agents to act for and on behalf of the Company and may give such appointee such authority as his or her certificate of authority may prescribe to sign with the Company's name and seal with the Company's seal bonds, recognizances, contracts of indemnity, and other writings obligatory in the nature of a bond, recognizance, or conditional undertaking, and any of said officers or the Board of Directors at any time may remove any such appointee and revoke the power given him or her; and it is

FURTHER RESOLVED, that the Chairman, the President, any Vice Chairman, any Executive Vice President, any Senior Vice President or any Vice President may delegate all or any part of the foregoing authority to one or more officers or employees of this Company, provided that each such delegation is in writing and a copy thereof is filed in the office of the Secretary; and it is

FURTHER RESOLVED, that any bond, recognizance, contract of indemnity, or writing obligatory in the nature of a bond, recognizance, or conditional undertaking shall be valid and binding upon the Company when (a) signed by the President, any Vice Chairman, any Executive Vice President, any Senior Vice President or any Vice President, any Second Vice President, the Treasurer, any Assistant Treasurer, the Corporate Secretary or any Assistant Secretary and duly attested and sealed with the Company's seal by a Secretary or Assistant Secretary; or (b) duly executed (under seal, if required) by one or more Attorneys-in-Fact and Agents pursuant to the power prescribed in his or her certificate or their certificates of authority or by one or more Company officers pursuant to a written delegation of authority; and it is

FURTHER RESOLVED, that the signature of each of the following officers: President, any Executive Vice President, any Senior Vice President, any Vice President, any Assistant Vice President, any Secretary, any Assistant Secretary, and the seal of the Company may be affixed by facsimile to any Power of Attorney or to any certificate relating thereto appointing Resident Vice Presidents, Resident Assistant Secretaries or Attorneys-in-Fact for purposes only of executing and attesting bonds and undertakings and other writings obligatory in the nature thereof, and any such Power of Attorney or certificate bearing such facsimile signature or facsimile seal shall be valid and binding upon the Company and any such power so executed and certified by such facsimile signature and facsimile seal shall be valid and binding on the Company in the future with respect to any bond or understanding to which it is attached.

I, **Kevin E. Hughes**, the undersigned, Assistant Secretary of each of the Companies, do hereby certify that the above and foregoing is a true and correct copy of the Power of Attorney executed by said Companies, which remains in full force and effect.

Dated this **24th** day of **March**, 2025 .




Kevin E. Hughes, Assistant Secretary

To verify the authenticity of this Power of Attorney, please call us at 1-800-421-3880.
Please refer to the above-named Attorney(s)-in-Fact and the details of the bond to which this Power of Attorney is attached.

DRAFT AIA® Document A305® – 2020

Contractor's Qualification Statement

THE PARTIES SHOULD EXECUTE A SEPARATE CONFIDENTIALITY AGREEMENT IF THEY INTEND FOR ANY OF THE INFORMATION IN THIS A305-2020 TO BE HELD CONFIDENTIAL.

SUBMITTED BY:

(Organization name and address.)
« Kiewit Infrastructure South Co.
6301 NW 5th Way
Suite 3200
Ft Lauderdale, FL 33309 »

SUBMITTED TO:

(Organization name and address.)
« City of Delray Beach
100 NW 1st Avenue
Delray Beach, FL 33444 »

ADDITIONS AND DELETIONS:

The author of this document has added information needed for its completion. The author may also have revised the text of the original AIA standard form. An *Additions and Deletions Report* that notes added information as well as revisions to the standard form text is available from the author and should be reviewed.

This document has important legal consequences. Consultation with an attorney is encouraged with respect to its completion or modification.

TYPE OF WORK TYPICALLY PERFORMED

(Indicate the type of work your organization typically performs, such as general contracting, construction manager as constructor services, HVAC contracting, electrical contracting, plumbing contracting, or other.)
« General Contractor »

THIS CONTRACTOR'S QUALIFICATION STATEMENT INCLUDES THE FOLLOWING:

(Check all that apply.)

- [« X »] Exhibit A – General Information
- [« X »] Exhibit B – Financial and Performance Information
- [« X »] Exhibit C – Project-Specific Information
- [« X »] Exhibit D – Past Project Experience
- [« X »] Exhibit E – Past Project Experience (Continued)

CONTRACTOR CERTIFICATION

The undersigned certifies under oath that the information provided in this Contractor's Qualification Statement is true and sufficiently complete so as not to be misleading.

Timothy J. Cleary
Organization's Authorized Representative
Signature

« 04/09/2025 »
Date

« Timothy J. Cleary | Senior Vice President »
Printed Name and Title

NOTARY

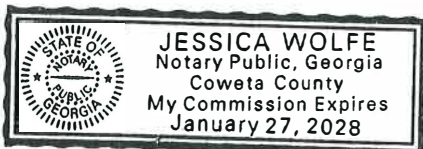
State of: « Georgia »

County of: « Fayette »

Signed and sworn to before me this « 9th » day of « April », « 2025 »

Jessica Wolfe
Notary Signature

My commission expires: « 1.27.2028 »



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Exhibit A

General Information

This Exhibit is part of the Contractor's Qualification Statement, submitted by «Kiewit Infrastructure South Co.» and dated the « 9th » day of « April » in the year « 2025 » (In words, indicate day, month and year.)

§ A.1 ORGANIZATION

§ A.1.1 Name and Location

§ A.1.1.1 Identify the full legal name of your organization.

«Kiewit Infrastructure South Co. »

§ A.1.1.2 List all other names under which your organization currently does business and, for each name, identify jurisdictions in which it is registered to do business under that trade name.

« Kiewit Infrastructure South Co. »

§ A.1.1.3 List all prior names under which your organization has operated and, for each name, indicate the date range and jurisdiction in which it was used.

« **Kiewit Southern Co.** | 2006-2010 | Florida, Tennessee, Utah, Virginia, Alabama, Georgia, South Carolina, West Virginia, Mississippi, North Carolina, Iowa, Louisiana, Kentucky, Arkansas, Kansas, Oklahoma, South Dakota, Maryland, District of Columbia, Texas, Nebraska, and New Mexico

Gilbert Southern Corp. | 1991-2006 | Florida, Tennessee, Utah, Virginia, Alabama, Georgia, South Carolina, West Virginia, Mississippi, North Carolina, Iowa, Louisiana, Kentucky, Arkansas, Kansas, Oklahoma, and South Dakota

Gilbert Corporation of Delaware, Inc. | 1969-1991 | Florida, Tennessee, Utah, Virginia, Alabama, Georgia, South Carolina, West Virginia, Mississippi, and North Carolina »

§ A.1.1.4 Identify the address of your organization's principal place of business and list all office locations out of which your organization conducts business. If your organization has multiple offices, you may attach an exhibit or refer to a website.

«District Office: 450 Dividend Drive, Peachtree City, GA 30269

Regional Offices:

Florida - Tampa: 5405 Cypress Center Dr. Suite 150, Tampa, FL 33609

Florida - Orlando: 6200 Lee Vista Blvd., Suite 700, Orlando, FL 32822

Florida - Ft. Lauderdale: 6301 NW 5th Way, Suite 3200, Ft. Lauderdale, FL 33309

South Carolina - 5617 North Rhett Avenue, Suite 125 D & E, North Charleston, SC 29406

Maryland - 6994 Columbia Gateway Drive, Suite 200, Columbia, MD 21046

»

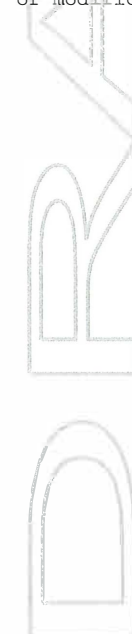
§ A.1.2 Legal Status

§ A.1.2.1 Identify the legal status under which your organization does business, such as sole proprietorship, partnership, corporation, limited liability corporation, joint venture, or other.

ADDITIONS AND DELETIONS:

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« Corporation »

- .1 If your organization is a corporation, identify the state in which it is incorporated, the date of incorporation, and its four highest-ranking corporate officers and their titles, as applicable.

« State of Incorporation: Delaware
Date of incorporation: November 28, 1969
President's Name: David Miles
Sr. Vice President's Name: Timothy J. Cleary
Secretary's Name: Allison M. Hardy
Treasurer's Name: Stephen S. Thomas »

- .2 If your organization is a partnership, identify its partners and its date of organization.

« N/A »

- .3 If your organization is individually owned, identify its owner and date of organization.

« N/A »

- .4 If the form of your organization is other than those listed above, describe it and identify its individual leaders:

« N/A »

§ A.1.2.2 Does your organization own, in whole or in part, any other construction-related businesses? If so, identify and describe those businesses and specify percentage of ownership.

« The following companies are wholly owned subsidiaries of Kiewit Infrastructure South Co.:

1. Kiewit Louisiana Co.- Infrastructure Construction
2. Kiewit Texas Co.- Infrastructure Construction
3. Kiewit Frontier Inc.- Infrastructure Construction
4. Consolidated Oilfield Services, Inc.- Infrastructure Construction
5. Kiewit Texas Construction LP- Infrastructure Construction »

§ A.1.3 Other Information

§ A.1.3.1 How many years has your organization been in business?

« 55 Years »

§ A.1.3.2 How many full-time employees work for your organization?

« 1,131 »

§ A.1.3.3 List your North American Industry Classification System (NAICS) codes and titles. Specify which is your primary NAICS code.

« 237310, Highway, Street, and Bridge Construction »

§ A.1.3.4 Indicate whether your organization is certified as a governmentally recognized special business class, such as a minority business enterprise, woman business enterprise, service disabled veteran owned small business, woman owned small business, small business in a HUBZone, or a small disadvantaged business in the 8(a) Business Development Program. For each, identify the certifying authority and indicate jurisdictions to which such certification applies.

« N/A »

§ A.2 EXPERIENCE

§ A.2.1 Complete Exhibit D to describe up to four projects, either completed or in progress, that are representative of your organization’s experience and capabilities.

§ A.2.2 State your organization’s total dollar value of work currently under contract.

« \$5,812,774,699 »

§ A.2.3 Of the amount stated in Section A.2.2, state the dollar value of work that remains to be completed:

« \$1,969,691,085 »

§ A.2.4 State your organization’s average annual dollar value of construction work performed during the last five years.

« \$2,356,462,000 »

§ A.3 CAPABILITIES

§ A.3.1 List the categories of work that your organization typically self-performs.

« Trench Excavation and Backfill, General Embankment, Roadway Reconstruction, Pipe Installation, Drainage & Structures Installation, MOT »

§ A.3.2 Identify qualities, accreditations, services, skills, or personnel that you believe differentiate your organization from others.

« Company qualities and accreditations:

Kiewit Infrastructure South Co. has an EMR of 0.56 for the year of 2025 and is ISO 9001 Certified.

Key Personnel:

- Brad Williamson – Project Principal | Industry: 18 Years
- Joe Pescatore – Project Manager | Industry: 15 Years
- Fabricio Cedillo – Construction Manager | Industry: 18 Years
- Haider Cortez – Project Engineer | Industry: 8 Years
- Abby Conner – Lead Estimator | Industry: 19 Years
- Mark Thomas – Drainage/Roadway Superintendent | Industry: 30 Years
- Alberto Baca – Outfall/Structures Superintendent | Industry: 24 Years
- Ryan Conner – Sanitary Superintendent | Industry: 21 Years

Please refer to Tab 6. which demonstrates the unique qualities. accreditations, services, and skills of our key personnel. »

§ A.3.3 Does your organization provide design collaboration or pre-construction services? If so, describe those services.

« Yes. Kiewit provides both design collaboration and pre-construction services including, but not limited to, civil, Structural, geotechnical, drainage, and temporary structures design; value engineering; constructability reviews; GMP development; subcontractor and supplier procurement; schedule development; utility coordination; estimating; bid packaging; and permitting, »

§ A.3.4 Does your organization use building information modeling (BIM)? If so, describe how your organization uses BIM and identify BIM software that your organization regularly uses.

« Yes, Kiewit uses building information modeling (BIM) software such as Bentley and Autodesk. Kiewit utilizes BIM software to create detailed 3D models, coordinate multi-discipline designs, and streamline data exchange, enhancing project accuracy and efficiency from design through construction. »

§ A.3.5 Does your organization use a project management information system? If so, identify that system.

« Yes. Kiewit uses a propriety project management information system powered by InEight. This tool seamlessly

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User Notes:

integrates with other accounting, estimating, scheduling, and document management systems, allowing unprecedented visibility to data and ensuring all parties are making decisions based on a single version of the truth. »

§ A.4 REFERENCES

§ A.4.1 Identify three client references:

(Insert name, organization, and contact information)

« Brad Baird | City of Tampa, Florida | Brad.Baird@tampagov.net, (813) 274-7883
Edwin Dendor | Clay County BOCC | edwindendor@claycountygov.com, (904) 529-4260
George Gault | AECOM | George.gault@aecom.com, (407) 718-0907 »

§ A.4.2 Identify three architect references:

(Insert name, organization, and contact information)

« Trent Dreese | Gannett Fleming Inc. | tdreese@gfnet.com, (717) 763-7211
Josh Wade | Parsons | joshua.s.wade@parsons.com, (571) 842-6300
Chris McGuire | AECOM | Chris.McGuire@aecom.com, (443) 386-6286 »

§ A.4.3 Identify one bank reference:

(Insert name, organization, and contact information)

« Mukesh Singh | Bank of America | msingh64@bofa.com, (646) 743-2640 »

§ A.4.4 Identify three subcontractor or other trade references:

(Insert name, organization, and contact information)

« Mike Adams | Gulf Coast Rebar Inc. | Mike@gulfcoastrebar.com, (561) 723-1456
Dennis Storm | ABC Cutting Contractors of Atlanta, Inc | dstorm@abccuttinginc.com, (404) 768-0965
Jonethan Biasatti | Johnson Laux | jonb@johnson-laux.com, (407) 770-2180 »

DRAFT AIA® Document A305® – 2020
Exhibit B

Financial and Performance Information

This Exhibit is part of the Contractor's Qualification Statement, submitted by « Kiewit Infrastructure South Co. » and dated the « 9th » day of « April » in the year « 2025 » *(In words, indicate day, month and year.)*

§ B.1 FINANCIAL

§ B.1.1 Federal tax identification number:

« 47-0530367 »

§ B.1.2 Attach financial statements for the last three years prepared in accordance with Generally Accepted Accounting Principles, including your organization's latest balance sheet and income statement. Also, indicate the name and contact information of the firm that prepared each financial statement.

« As allowed by the Q&A, our Financial Statements have been provided on a separate PDF named "Confidential Financial Statements".

Contact Information and Firm that prepared each statement:

Matt Maiers

KPMG LLP

(402) 661-5285 »

§ B.1.3 Has your organization, its parent, or a subsidiary, affiliate, or other entity having common ownership or management, been the subject of any bankruptcy proceeding within the last ten years?

« No. »

§ B.1.4 Identify your organization's preferred credit rating agency and identification information.

(Identify rating agency, such as Dun and Bradstreet or Equifax, and insert your organization's identification number or other method of searching your organization's credit rating with such agency.)

« Dun and Bradstreet – 05-507-6616 »

§ B.2 DISPUTES AND DISCIPLINARY ACTIONS

§ B.2.1 Are there any pending or outstanding judgments, arbitration proceedings, bond claims, or lawsuits against your organization, its parent, or a subsidiary, affiliate, or other entity having common ownership or management, or any of the individuals listed in Exhibit A, Section 1.2, in which the amount in dispute is more than \$75,000?

(If the answer is yes, provide an explanation.)

« Yes, please see attachment B.1. »

§ B.2.2 In the last five years has your organization, its parent, or a subsidiary, affiliate, or other entity having common ownership or management:

(If the answer to any of the questions below is yes, provide an explanation.)

.1 failed to complete work awarded to it?

ADDITIONS AND DELETIONS:

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This document has important legal consequences. Consultation with an attorney is encouraged with respect to its completion or modification.



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« No »

- .2 been terminated for any reason except for an owners' convenience?

« No »

- .3 had any judgments, settlements, or awards pertaining to a construction project in which your organization was responsible for more than \$75,000?

« No »

- .4 filed any lawsuits or requested arbitration regarding a construction project?

« Yes, please refer to Attachment B.2. »

§ B.2.3 In the last five years, has your organization, its parent, or a subsidiary, affiliate, or other entity having common ownership or management; or any of the individuals listed in Exhibit A Section 1.2:
(If the answer to any of the questions below is yes, provide an explanation.)

- .1 been convicted of, or indicted for, a business-related crime?

« No »

- .2 had any business or professional license subjected to disciplinary action?

« No »

- .3 been penalized or fined by a state or federal environmental agency?

« Yes. KISC and the Kiewit Companies perform hundreds of projects every year and have received some Environmental Notice of Violations or Fines in the last five years. A full list of our Violations and Fines for the last five years can be provided upon request. »

ATTACHMENT B.1 - Pending Litigations**PROJECT | MATTER NAME | MATTER SUMMARY**

Bayonne Bridge | Bayonne Bridge - NASDI, LLC v. Skanska Koch Inc., Kiewit Infrastructure Co. (JV) d/b/a Skanska Kiewit
Lawsuit filed by demolition subcontractor (NASDI, LLC) in relation to the Bayonne "Raise the Roadway" Project against Skanska, Koch, Kiewit (SKK) stemming from a dispute over allocation of owner dispute proceeds, and NASDI's subsequent abandonment of its work.

I-95 Wilmington Viaduct Early Works | Laborers Int. Union of North America No. 199 Welfare, Pension Apprenticeship & Training Annuity & Laborers Employers Cooperative Education Trust Funds of Delaware & Laborers Int. Union Local 199 v. Kiewit Infrastructure Co.
Failure to make to pay liquidated damages on delinquent contributions

New London State Pier Facility PreCon | Blakeslee Arpaia Chapman Inc. v. Kiewit Infrastructure Co., Travelers Casualty Surety Company of America, Connecticut Port Authority
Lawsuit filed by subcontractor seeking payment on a differing site condition claim which has been submitted by Kiewit Infrastructure Co. to the Owner but not yet paid by the Owner regarding the New London State Pier Facility PreCon Project, in New London, Connecticut.

MECT - CTA O'Hare Signal | Alstom Signaling, Inc. v. Kiewit-Aldridge, AJV, Kiewit Infrastructure Co., Aldridge Electrical, Inc., Travelers Casualty and Surety Company of America Continental Casualty Company and Chicago Transit Authority | Lawsuit by subcontractor, Alstom Signaling, Inc. against Kiewit-Aldridge, AJV and Kiewit Infrastructure Co. demanding an Accounting of Public Funds, alleging breach of contract regarding work performed on the O'Hare Line - Jefferson Park to O'Hare Signal Updates and Special Trackwork Project in Chicago, Illinois.

Project Neon | Encon Arizona, LLC d/b/a TPAC v. Kiewit Infrastructure West Co. | Lawsuit by material supplier seeking payment of fees due for labor, materials and work in connection with Project Neon in Las Vegas, Nevada.

I-205 Widening | I-205 Abernethy Project - Doka USA Ltd. Dispute | Potential lawsuit against subcontractor alleging breach of contract in connection with the I-205 Abernethy project in Oregon City, OR.

Dulles | Parsons Transportation Group, Inc. of VA v. Capital Rail Constructors, Clark Construction Group, LLC and Kiewit Infrastructure South Co. | Lawsuit by designer against Capital Rail Constructors, a JV, Clark Construction Group, LLC and Kiewit Infrastructure South Co. alleging breach of contract regarding the Dulles Corridor Metrorail Project to extending the Silver Line metro system Project in Fairfax County, VA.

Comanche Springs - US 28 | Fulfillsource, LLC D/B/A Crewback v. Travelers Casualty and Surety Company and Kiewit Infrastructure South Co. | Lawsuit against Kiewit Infrastructure South Co. and surety by a second-tier subcontractor alleging breach of contract and monies due for work performed on the Comanche Springs, US 285 in Pecos, Texas.

PCCP | Stantec Consulting Services, Inc., V&A Consulting Engineers, Inc., and Patterson Pump Company v. Kiewit Louisiana Co., M.R. Pittman Group, LLC., and Traylor Bros, Inc. PCCP Constructors, a JV | Lawsuit by the project designer and various subcontractors requesting declaratory judgment regarding the PCCP Project in New Orleans, Louisiana.

PCCP | PCCP Owner Claims_COFD 01_C-0595 | PCCP Constructors, a Joint Venture, ("PCCP JV") filed suit against the United States Army Corps of Engineers (USACE) in the United States Court of Federal Claims challenging a Contracting Officer's Final Decision ("COFD") was issued on April 21, 2023, in relation to Permanent Canal Closures and Pumps ("PCCP") (Contract No. W912P8-12-C-0049) ("Contract"). In the COFD, the government alleges design deficiencies and latent defects in work performed under the Contract. In the COFD, the government alleges design deficiencies and latent defects in work performed under the Contract. In the Suit, PCCP JV disputes the Government's claims.

PCCP | PCCP Owner Claims_COFD 02_C-0597 | PCCP Constructors, a Joint Venture, ("PCCP JV") filed suit against the United States Army Corps of Engineers (USACE) in the United States Court of Federal Claims ("Suit") challenging a Contracting Officer's Final Decision ("COFD") on April 24, 2023, in relation to Permanent Canal Closures and Pumps ("PCCP") (Contract No. W912P8-12-C-0049) ("Contract"). In the COFD, the government alleges design deficiencies and latent defects in work performed under the Contract. In the Suit, PCCP JV disputes the Government's claims.

PCCP | PCCP Owner Claims_COFD 03_C-0600_Basement Cracks | A Contracting Officer's Final Decision ("COFD") on April 26, 2023, in relation to Permanent Canal Closures and Pumps ("PCCP") (Contract No. W912P8-12-C-0049) ("Contract"). The COFD concerns a government claim for design deficiencies which have resulted in basement cracks at the pumps designed and constructed under the subject contract by PCCP, a Joint Venture, ("PCCP JV", or "JV") and which are located at the 17th Street, Orleans Avenue and London Avenue Outfall Canals at or near Lake Pontchartrain, Orleans and Jefferson Parishes, Louisiana.

PCCP | PCCP Owner Claims_COFD 04_C-0599_Pump Tilt | A Contracting Officer's Final Decision ("COFD") on April 27, 2023, in relation to Permanent Canal Closures and Pumps ("PCCP") (Contract No. W912P8-12-C-0049) ("Contract"). The COFD concerns a government claim for design deficiencies and latent defects due to pump tilt and building movement at the structures designed and constructed under the subject contract by PCCP Constructors, a Joint Venture, and which are located at the 17th Street, Orleans Avenue and London Avenue Outfall Canals at or near Lake Pontchartrain, Orleans and Jefferson Parishes, Louisiana.

PCCP | PCCP Owner Claims_COFD 05_C-0598_Climber Screens | A Contracting Officer's Final Decision ("COFD") on April 27, 2023, in relation to Permanent Canal Closures and Pumps ("PCCP") (Contract No. W912P8-12-C-0049) ("Contract"). The COFD concerns a government claim for design deficiencies and latent defects pertaining to the climber screen features which have resulted in motor failure due to oil/water intrusion in the gearboxes ("gearboxes") and the dislodgement of the E-chains during operations which have caused damage at or near the structures designed and constructed under the subject Contract by PCCP Constructors, a Joint Venture ("PCCP JV", or "JV"), and which are located at the 17th Street, Orleans Avenue and London Avenue Outfall Canals at or near Lake Pontchartrain, Orleans and Jefferson Parishes, Louisiana.

Manasquan Project | Agate Construction Company, Inc. v. Weeks Marine, Inc. | As a result of delays experienced in the Manasquan project, Agate Construction Company, Inc. filed suit in New Jersey Supreme Court to recover alleged delay costs. The suit is unfounded as Weeks Marine reconciled all payments and Agate waived all claims prior to final payment.

Little Island Project | Lloyds of London and Pier 55 v. Weeks Marine | Lloyds of London (who issued builders' risk policy to Owner Pier 55) and Pier 55 have sued Weeks, Hunter Roberts, and other companies in connection with the cracking of concrete piles at the Little Island Project in Manhattan, New York. The claim is for negligent construction and breach of contract. Weeks did not manufacture the piles; this was done by co-defendant Coastal Precast Systems.

Patent Infringement | Wilco v. Weeks Marine, Inc. | Wilco filed a patent infringement lawsuit against Weeks in the Eastern District of Louisiana for infringement and misappropriation of Wilco's intellectual property, US Patent #6,918,801, regarding amphibious vehicle technology. Spud attachments were developed and the subject of a Wilco Patent which was in turn manufactured by EIK International, a Malaysian company, who then sold marsh buggies incorporating the technology to Weeks. Weeks purchased the EIK product subject to an indemnity to patent infringement. Wilco claims that it had a patent on "amphibious vehicles" employing the use of this accessory.

Tampa Pipes | PE Pipeline LLC v KIS and Murphy Pipeline Contractors LLC | PE Pipeline, LLC sued Murphy Pipeline and Kiewit, alleging Murphy wrongfully withheld payment and claiming Kiewit, as a third-party beneficiary, is responsible for the unpaid amount.

ATTACHMENT B.2 - Filed Litigations**PROJECT | MATTER NAME | MATTER SUMMARY**

Selmon Expressway | Kiewit Infrastructure South Co. v. AECOM Technical Services, Inc. | On August 31, 2021, Kiewit Infrastructure South Co. ("KISC") file suit against AECOM Technical Services, Inc. ("AECOM") to recover additional costs incurred by KISC due to significant necessary deviations from planned design elements detailed by AECOM in its pre-bid design in connection with the Selmon Expressway Western Extension in Tampa, Florida

MECT - CTA O'Hare Signal | Kiewit-Aldridge, AJV v. Alstom Signaling, Inc., Federal Insurance Company, The Continental Insurance Company and National Union Fire Insurance Company of Pittsburgh, PA, together as surety Arbitration against subcontractor, Alstom Signaling, Inc. alleging delay regarding work performed on the CTA O'Hare Signal Project, Chicago, Illinois.

Runway 4R | KIWC v. Northern Powerline Constructors, Inc. | Lawsuit by Kiewit Infrastructure West Co. against subcontractor, Northern Powerline Constructors, Inc. alleging estoppel and breach of contract regarding the Runways and Taxiways Shoulder Rehabilitation Project, in Honolulu, Hawaii.

Project Neon | Kiewit Infrastructure West Co. v. L.L.O. Inc. dba ACME Electric; TAB Contractors, Inc., Liberty Mutual Insurance Company | Lawsuit by Kiewit Infrastructure West Co. against L.L.O. dba ACME Electric; TAB Contractors, Inc.; and Liberty Mutual Insurance Company, alleging breach of contract, breach of implied covenant of good faith dealings; recovery against performance bond; declaratory relief in connection with the Project Neon Design Build Project in Las Vegas, Nevada. Anticipation of a counterclaim from ACME Electric for alleged additional work and acceleration/disruption costs.

Fullerton Creek | Kiewit v. Cornerstone Demolition | Lawsuit by Kiewit Infrastructure West Co. against subcontractor for Breach of Contract and Breach of Promissory Note.

Mud Mountain Dam Fish Passage Facility | KIWC v. Western Industries, Inc. | Lawsuit by Kiewit Infrastructure West Co. against terminated subcontractor for alleged breach of contract regarding the Mud Mountain Dam Fish Passage Facility, Buckley, Washington.

Mud Mountain Dam | Bowers Steel v. Western Industries, Inc., Kiewit, Travelers | Third party lawsuit by second-tier subcontractor to perfect Miller Act Claim for amounts unpaid by Kiewit's first-tier subcontractor.

LACTMA Blue Line Signal Sys Rehab | Mass. Electric Construction Co. v. Potomac Yard Constructors, a Joint Venture | Arbitration against Potomac Yard Constructors, a Joint Venture arising from cost and schedule impacts in connection with the Potomac Yard Metrorail Station project in Alexandria, Virginia.

Cabot Yard & MSF Improvement | Mass. Electric Construction Co. v. LM Heavy Civil Construction, LLC and The Lane Construction Corporation and Cooperativa Muratori & Cementisti - C.M.C. Di Ravenna Societa Cooperativa, collectively acting by and through the LMH-Lane Cabot Yard Joint Venture | Arbitration against LMH-Lane Cabot Yard JV pertaining to MEC's Claim for delay impacts regarding the Cabot Yard & Maintenance Facility Improvements Project in Boston, Massachusetts.

Paulsboro Marine Terminal Bulkhead | Weeks Marine, Inc. vs. Gloucester County Improvements Authority (GCIA); (Paulsboro I Litigation) | Weeks Marine filed an action on May 4, 2017, in New Jersey State Court arising from a project awarded to Weeks Marine in January 2011 for the Paulsboro Marine Terminal Bulkhead, Dredging and Upland Site Preparation Project in Paulsboro, NJ. Weeks Marine asserted a Differing Site Condition in August 2013 alleging the dredging to be significantly more difficult (harder material) than shown on the geotechnical information included with the bid package.

Paulsboro Marine Terminal Modified I Wharf Project | Weeks Marine, Inc. vs. Gloucester County Improvements Authority (GCIA); a/k/a Paulsboro II Litigation) | On November 27, 2017, Weeks Marine filed a second action in New Jersey State Court against GCIA. This litigation arises from a project awarded to Weeks Marine in August 2014 for the Paulsboro Marine Terminal Modified I Wharf Project in Paulsboro, NJ. Weeks filed its Complaint on or about 11/22/2017 which included a request for payment of seventeen (17) individual issues including Pile Cutoffs, Pile Restrikes, Pile Crack Repairs, Extended Overhead, and Acceleration. GCIA filed its counterclaim on 2/2/2018 for liquidated damages and a credit for alleged relaxation of pile driving criteria.

Venture Global Storm Surge Wall Project | Keiland Construction v. Weeks Marine | Suit in federal court in Louisiana to recover costs Keiland alleges were incurred on the Venture Global Storm Surge Wall Project. Weeks Marine terminated Keiland due to its inability to advance the Project's critical path. Court has found in favor of Weeks Marine regarding methodology of calculation for costs. Summary judgment motion filed by both Keiland (for costs and work performed and legal fees) and Weeks Marine (for overpayments made to Keiland prior to termination and to receive legal fees).

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Exhibit C

Project Specific Information

This Exhibit is part of the Contractor's Qualification Statement, submitted by « Kiewit Infrastructure South Co. » and dated the « 9th » day of « April » in the year « 2025 » (In words, indicate day, month and year.)

PROJECT:

(Name and location or address.)

« RFQ No. 2025-023 Tropic Isle Neighborhood Improvements »
« 100 NW 1st Avenue
Delray Beach, FL 33444 »

CONTRACTOR'S PROJECT OFFICE:

(Identify the office out of which the contractor proposes to perform the work for the Project.)

« 6301 NW 5th Way, Suite 3200
Ft Lauderdale, FL 33309 »

TYPE OF WORK SOUGHT

(Indicate the type of work you are seeking for this Project, such as general contracting, construction manager as constructor, design-build, HVAC subcontracting, electrical subcontracting, plumbing subcontracting, etc.)

« Construction Manager at Risk »

CONFLICT OF INTEREST

Describe any conflict of interest your organization, its parent, or a subsidiary, affiliate, or other entity having common ownership or management, or any of the individuals listed in Exhibit A Section 1.2, may have regarding this Project.

« N/A »

§ C.1 PERFORMANCE OF THE WORK

§ C.1.1 When was the Contractor's Project Office established?

« Kiewit Infrastructure South Co. has been active in South Florida since 1991. Our current office was established in 2024. »

§ C.1.2 How many full-time field and office staff are respectively employed at the Contractor's Project Office?

« Local Staff: 401 | Local Craft: 573 »

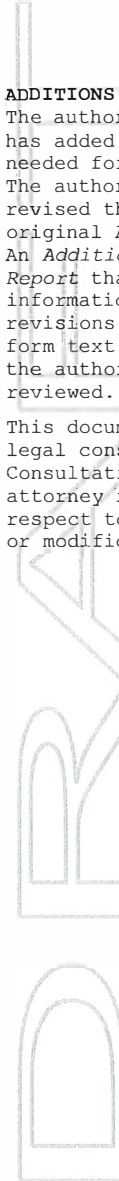
§ C.1.3 List the business license and contractor license or registration numbers for the Contractor's Project Office that pertain to the Project.

« FL Certified General Contractor: CGC 15 11417 / CGC 1522040 / CGCI 522051
FL Certified Mechanical Contractor: CMCI249632
FL Certified Plumbing Contractor: CFC 1427088

ADDITIONS AND DELETIONS:

The author of this document has added information needed for its completion. The author may also have revised the text of the original AIA standard form. An *Additions and Deletions Report* that notes added information as well as revisions to the standard form text is available from the author and should be reviewed.

This document has important legal consequences. Consultation with an attorney is encouraged with respect to its completion or modification.



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FL Certified Underground Utility & Excavation: CUC1224233 /CUC 1225 152
FL Authority to Transact Business: 837350 »

§ C.1.4 Identify key personnel from your organization who will be meaningfully involved with work on this Project and indicate (1) their position on the Project team, (2) their office location, (3) their expertise and experience, and (4) projects similar to the Project on which they have worked.

« Brad Williamson – Project Principal | Address: 5405 Cypress Center Drive, Suite 210, Tampa, FL 33609
Joe Pescatore – Project Manager | Address: 6301 NW 5th Way, Suite 3200, Ft Lauderdale, FL 33309
Fabricio Cedillo – Construction Manager | Address: 6301 NW 5th Way, Suite 3200, Ft Lauderdale, FL 33309
Haider Cortez – Project Engineer | Address: 5405 Cypress Center Drive, Suite 210, Tampa, FL 33609
Abby Conner – Lead Estimator | Address: 5405 Cypress Center Drive, Suite 210, Tampa, FL 33609
Mark Thomas – Drainage Superintendent | Address: 6301 NW 5th Way, Suite 3200, Ft Lauderdale, FL 33309
Luke Gutshall – Utility Superintendent | Address: 6301 NW 5th Way, Suite 3200, Ft Lauderdale, FL 33309
Alberto Baca – Outfall/Structures Superintendent | Address: 6301 NW 5th Way, Suite 3200, Ft Lauderdale, FL 33309
Ryan Conner – Sanitary Superintendent | 6301 NW 5th Way, Suite 3200, Ft Lauderdale, FL 33309
Please see Tab 6 for Key Personnel expertise and experience and similar projects.»

§ C.1.5 Identify portions of work that you intend to self-perform on this Project.

« MOT, roadway demolition, storm installation, sanitary installation, water main installation, roadway, seawall and outfall structures »

§ C.1.6 To the extent known, list the subcontractors you intend to use for major portions of work on the Project.

« While Kiewit can self-perform a majority of this work we recognize the importance of local sub involvement. Below is a list of potential subs we will solicit during the preconstruction process. Through the GMP process on this CMAR, it is our intent to solicit from the following subcontractors: All Dade Pavers, Atlas Concrete & Pavement Inc., One Stop Pavers, Bob's Barricades Inc., Acme Barricades, Arrive Alive Traffic Control, Weekly Asphalt Paving Inc., Roadway Construction, LLC, Gencon Partners, Inc., Ric-Man Construction Florida, Inc., David Mancini & Sons Inc., Murphy Pipeline Contractors, Shenandoah, Insituform, Blue Works Company, Atlantic Pipe Services, HK Solutions Company, Arazoza Bros. Corp, VisualScape, Inc. Dias Brothers Landscape Services, LLC, Halley Engineering Contractors, Inc., General Asphalt Co., Community Asphalt Corporation, Homestead Concrete and Drainage, Inc., V&G Construction Solutions, Complete Highway Identity, Inc., Safety Systems Barricades, Road Runner, McShea, Conti LLC, Under Power Electric, Engineered Spray Solutions LLC, Cellucrete Corp., AeroAggregates of North America, LLC, Stalite Lightweight Aggregate, Gulf Coast Underground LLC, and AUM Construction »

§ C.2 EXPERIENCE RELATED TO THE PROJECT

§ C.2.1 Complete Exhibit D to describe up to four projects performed by the Contractor's Project Office, either completed or in progress, that are relevant to this Project, such as projects in a similar geographic area or of similar project type. If you have already completed Exhibit D, but want to provide further examples of projects that are relevant to this Project, you may complete Exhibit E.

§ C.2.2 State the total dollar value of work currently under contract at the Contractor's Project Office:

« \$478.2 Million »

§ C.2.3 Of the amount stated in Section C.2.2, state the dollar value of work that remains to be completed:

« 105.7 Million »

§ C.2.4 State the average annual dollar value of construction work performed by the Contractor's Project Office during the last five years.

« \$380.06 Million »

§ C.2.5 List the total number of projects the Contractor's Project Office has completed in the last five years and state the dollar value of the largest contract the Contractor's Project Office has completed during that time.

« 18 Projects | Largest Project: \$471 Million »

§ C.3 SAFETY PROGRAM AND RECORD

§ C.3.1 Does the Contractor's Project Office have a written safety program?

« Yes »

§ C.3.2 List all safety-related citations and penalties the Contractor's Project Office has received in the last three years.

« N/A »

§ C.3.3 Attach the Contractor's Project Office's OSHA 300a Summary of Work-Related Injuries and Illnesses form for the last three years.

§ C.3.4 Attach a copy of your insurance agent's verification letter for your organization's current workers' compensation experience modification rate and rates for the last three years.

§ C.4 INSURANCE

§ C.4.1 Attach current certificates of insurance for your commercial general liability policy, umbrella insurance policy, and professional liability insurance policy, if any. Identify deductibles or self-insured retentions for your commercial general liability policy.

§ C.4.2 If requested, will your organization be able to provide property insurance for the Project written on a builder's risk "all-risks" completed value or equivalent policy form and sufficient to cover the total value of the entire Project on a replacement cost basis?

« Yes »

§ C.4.3 Does your commercial general liability policy contain any exclusions or restrictions of coverage that are prohibited in AIA Document A101-2017, Exhibit A, Insurance A.3.2.2.2? If so, identify.

« No »

§ C.5 SURETY

§ C.5.1 If requested, will your organization be able to provide a performance and payment bond for this Project?

« Yes »

§ C.5.2 Surety company name:

« Travelers Casualty and Surety Company of America »

§ C.5.3 Surety agent name and contact information:

« Traci Sutton, (402) 271-2956, traci.sutton@midwestagenciesinc.com »

§ C.5.4 Total bonding capacity:

« \$15 Billion »

§ C.5.5 Available bonding capacity as of the date of this qualification statement:

« \$8.01 Billion »

Summary of Work - Related Injuries and Illnesses

U.S. Department of Labor
Occupational Safety and Health Administration

All establishments covered by Part 1904 must complete this Summary page, even if no work-related injuries or illnesses occurred during the year. Remember to review the log to verify that the entries are complete and accurate before completing this summary.

Using the Log, count the individual entries you made for each category. Then write the totals below, making sure you've added the entries from every page of the log. If you had no cases, write "0".

Employees, former employees, and their representatives have the right to review the OSHA Form 300 in its entirety. They also have limited access to the OSHA Form 301 or its equivalent. See 29 CFR Part 1904.35, in OSHA's recordkeeping rule, for further details on the access provisions for these forms.

Number of Cases

Total number of deaths	Total number of cases with days away from work	Total number of cases with job transfer or restrictions	Total number of other recordable cases
0	0	0	0
(G)	(H)	(I)	(J)

Number of Days

Total number of days away from work	Total number of days of job transfer or restriction
0	0
(K)	(L)

Injury and Illness Types

Total number of ... (M)			
(1) Injury	0	(4) Poisoning	0
(2) Skin Disorder	0	(5) Hearing Loss	0
(3) Respiratory Condition	0	(6) All other Illness	0

This data was gathered from several project locations, and is provided only to support requisitions and bid proposal requests. This information is not to be used for any other purpose. This information was accurate on the date submitted.

Establishment information (US work only)

Your Establishment name 1044_Kiewit Infrastructure South Co.

Street 1550 Mike Fahey Street

City Omaha State NE Zip 68102

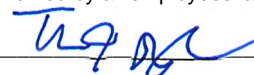
Industry description (e.g., Manufacture of motor truck trailers)
*Multiple Industries Included

North American Industrial Classification (NAICS), if known (e.g., 336212)
*Multiple Industries Included

Employment information

Annual average number of employees 796

Total hours worked by all employees last year 1,731,080

Sign here 

Knowingly falsifying this document may result in a fine.

I certify that I have examined this document and that to the best of my knowledge the entries are true, accurate, and complete.

<u>Thomas J. Boyle</u>	<u>Assistant Secretary</u>
Print First & Last Name	Print Title
<u>770-487-2300</u>	<u>10-Mar-2025</u>
Phone	Date

***As this is a summary of several projects there are multiple NAICS codes & industries included.**

Summary of Work - Related Injuries and Illnesses

U.S. Department of Labor
Occupational Safety and Health Administration

All establishments covered by Part 1904 must complete this Summary page, even if no work-related injuries or illnesses occurred during the year. Remember to review the log to verify that the entries are complete and accurate before completing this summary.

Using the Log, count the individual entries you made for each category. Then write the totals below, making sure you've added the entries from every page of the log. If you had no cases, write "0".

Employees, former employees, and their representatives have the right to review the OSHA Form 300 in its entirety. They also have limited access to the OSHA Form 301 or its equivalent. See 29 CFR Part 1904.35, in OSHA's recordkeeping rule, for further details on the access provisions for these forms.

Number of Cases

Total number of deaths	Total number of cases with days away from work	Total number of cases with job transfer or restrictions	Total number of other recordable cases
0	0	0	2
(G)	(H)	(I)	(J)

Number of Days

Total number of days away from work	Total number of days of job transfer or restriction
0	0
(K)	(L)

Injury and Illness Types

Total number of ... (M)			
(1) Injury	2	(4) Poisoning	0
(2) Skin Disorder	0	(5) Hearing Loss	0
(3) Respiratory Condition	0	(6) All other Illness	0

This data was gathered from several project locations, and is provided only to support requisitions and bid proposal requests. This information is not to be used for any other purpose. This information was accurate on the date submitted.

Establishment information

(US work only)

Your Establishment name 1044_Kiewit Infrastructure South Co.

Street 1550 Mike Fahey Street

City Omaha State NE Zip 68102

Industry description (e.g., Manufacture of motor truck trailers)
*Multiple Industries Included

North American Industrial Classification (NAICS), if known (e.g., 336212)
*Multiple Industries Included

Employment information

Annual average number of employees 1,035

Total hours worked by all employees last year 2,359,215

Sign here 

Knowingly falsifying this document may result in a fine.

I certify that I have examined this document and that to the best of my knowledge the entries are true, accurate, and complete.

Thomas J. Boyle Assistant Secretary
Print First & Last Name Assistant Secretary Print Title

770-487-2300 10-Mar-2025
Phone 10-Mar-2025 Date

***As this is a summary of several projects there are multiple NAICS codes & industries included.**

Summary of Work - Related Injuries and Illnesses

U.S. Department of Labor
Occupational Safety and Health Administration

All establishments covered by Part 1904 must complete this Summary page, even if no work-related injuries or illnesses occurred during the year. Remember to review the log to verify that the entries are complete and accurate before completing this summary.

Using the Log, count the individual entries you made for each category. Then write the totals below, making sure you've added the entries from every page of the log. If you had no cases, write "0".

Employees, former employees, and their representatives have the right to review the OSHA Form 300 in its entirety. They also have limited access to the OSHA Form 301 or its equivalent. See 29 CFR Part 1904.35, in OSHA's recordkeeping rule, for further details on the access provisions for these forms.

Number of Cases

Total number of deaths	Total number of cases with days away from work	Total number of cases with job transfer or restrictions	Total number of other recordable cases
0	2	0	2
(G)	(H)	(I)	(J)

Number of Days

Total number of days away from work	Total number of days of job transfer or restriction
67	16
(K)	(L)

Injury and Illness Types

Total number of ... (M)			
(1) Injury	4	(4) Poisoning	0
(2) Skin Disorder	0	(5) Hearing Loss	0
(3) Respiratory Condition	0	(6) All other Illness	0

This data was gathered from several project locations, and is provided only to support requisitions and bid proposal requests. This information is not to be used for any other purpose. This information was accurate on the date submitted.

Establishment information

(US work only)

Your Establishment name 1044_Kiewit Infrastructure South Co.

Street 1550 Mike Fahey Street

City Omaha State NE Zip 68102

Industry description (e.g., Manufacture of motor truck trailers)
*Multiple Industries Included

North American Industrial Classification (NAICS), if known (e.g., 336212)
*Multiple Industries Included

Employment information

Annual average number of employees 1,143

Total hours worked by all employees last year 2,624,735

Sign here 

Knowingly falsifying this document may result in a fine.

I certify that I have examined this document and that to the best of my knowledge the entries are true, accurate, and complete.

Thomas J. Boyle Assistant Secretary
Print First & Last Name Print Title

770-487-2300 10-Mar-2025
Phone Date

***As this is a summary of several projects there are multiple NAICS codes & industries included.**



CERTIFICATE OF LIABILITY INSURANCE

Evidence of Coverage

DATE (MM/DD/YYYY)

3/17/2025

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELOW. THIS CERTIFICATE OF INSURANCE DOES NOT CONSTITUTE A CONTRACT BETWEEN THE ISSUING INSURER(S), AUTHORIZED REPRESENTATIVE OR PRODUCER, AND THE CERTIFICATE HOLDER.

IMPORTANT: If the certificate holder is an ADDITIONAL INSURED, the policy(ies) must have ADDITIONAL INSURED provisions or be endorsed. If SUBROGATION IS WAIVED, subject to the terms and conditions of the policy, certain policies may require an endorsement. A statement on this certificate does not confer rights to the certificate holder in lieu of such endorsement(s).

PRODUCER Midwest Agencies, Inc. 1550 Mike Fahey Street Omaha, NE 68102	CONTACT NAME: Traci Sutton PHONE (A/C. No. Ext): 402-271-2956 E-MAIL ADDRESS: Traci.Sutton@Midwestagenciesinc.com	FAX (A/C. No): 402-271-2830
	INSURER(S) AFFORDING COVERAGE	
INSURED Kiewit Infrastructure South Co. 6301 NW 5th Way Suite 3200 Fort Lauderdale FL 33309	INSURER A: Old Republic Insurance Company	
	INSURER B: Swiss Re Corporate Solutions America Ins	
	INSURER C:	
	INSURER D:	
	INSURER E:	
	INSURER F:	

COVERAGES

CERTIFICATE NUMBER: 84378525

REVISION NUMBER:

THIS IS TO CERTIFY THAT THE POLICIES OF INSURANCE LISTED BELOW HAVE BEEN ISSUED TO THE INSURED NAMED ABOVE FOR THE POLICY PERIOD INDICATED. NOTWITHSTANDING ANY REQUIREMENT, TERM OR CONDITION OF ANY CONTRACT OR OTHER DOCUMENT WITH RESPECT TO WHICH THIS CERTIFICATE MAY BE ISSUED OR MAY PERTAIN, THE INSURANCE AFFORDED BY THE POLICIES DESCRIBED HEREIN IS SUBJECT TO ALL THE TERMS, EXCLUSIONS AND CONDITIONS OF SUCH POLICIES. LIMITS SHOWN MAY HAVE BEEN REDUCED BY PAID CLAIMS.

INSR LTR	TYPE OF INSURANCE	ADDL INSD	SUBR WVD	POLICY NUMBER	POLICY EFF (MM/DD/YYYY)	POLICY EXP (MM/DD/YYYY)	LIMITS
A	<input checked="" type="checkbox"/> COMMERCIAL GENERAL LIABILITY <input type="checkbox"/> CLAIMS-MADE <input checked="" type="checkbox"/> OCCUR <input checked="" type="checkbox"/> Contractual Liability <input checked="" type="checkbox"/> XCU Included GEN'L AGGREGATE LIMIT APPLIES PER: <input type="checkbox"/> POLICY <input checked="" type="checkbox"/> PRO-JECT <input type="checkbox"/> LOC OTHER:			MWZY 312911	3/1/2025	3/1/2026	EACH OCCURRENCE \$5,000,000 DAMAGE TO RENTED PREMISES (Ea occurrence) \$5,000,000 MED EXP (Any one person) \$10,000 PERSONAL & ADV INJURY \$5,000,000 GENERAL AGGREGATE \$10,000,000 PRODUCTS - COMP/OP AGG \$10,000,000 \$
A	AUTOMOBILE LIABILITY <input checked="" type="checkbox"/> ANY AUTO <input type="checkbox"/> OWNED AUTOS ONLY <input checked="" type="checkbox"/> SCHEDULED AUTOS <input type="checkbox"/> NON-OWNED AUTOS ONLY <input type="checkbox"/> HIRED AUTOS ONLY			MWTB 312910	3/1/2025	3/1/2026	COMBINED SINGLE LIMIT (Ea accident) \$5,000,000 BODILY INJURY (Per person) \$ BODILY INJURY (Per accident) \$ PROPERTY DAMAGE (Per accident) \$ \$
B	<input type="checkbox"/> UMBRELLA LIAB <input checked="" type="checkbox"/> OCCUR <input checked="" type="checkbox"/> EXCESS LIAB <input type="checkbox"/> CLAIMS-MADE <input type="checkbox"/> DED <input checked="" type="checkbox"/> RETENTION \$N/A			EXS 2000809	3/1/2025	3/1/2026	EACH OCCURRENCE \$20,000,000 AGGREGATE \$20,000,000 \$
A	WORKERS COMPENSATION AND EMPLOYERS' LIABILITY ANY PROPRIETOR/PARTNER/EXECUTIVE OFFICER/MEMBER EXCLUDED? (Mandatory in NH) If yes, describe under DESCRIPTION OF OPERATIONS below	Y/N N	N/A	MWC 312908 MWXS 312909 MWFEX 312928-FL MWXS 316021-CA USL&H & Jones Act StopGap	3/1/2025 3/1/2025 3/1/2025 3/1/2025	3/1/2026 3/1/2026 3/1/2026 3/1/2026	<input checked="" type="checkbox"/> PER STATUTE <input type="checkbox"/> OTH-ER E.L. EACH ACCIDENT \$5,000,000 E.L. DISEASE - EA EMPLOYEE \$5,000,000 E.L. DISEASE - POLICY LIMIT \$5,000,000

DESCRIPTION OF OPERATIONS / LOCATIONS / VEHICLES (ACORD 101, Additional Remarks Schedule, may be attached if more space is required)

Project: Tropic Isle Neighborhood Improvements

RE: Evidence of coverage

CERTIFICATE HOLDER
 City of Delray Beach
 100 Northwest 1st Avenue
 Delray Beach FL 33444
CANCELLATION

SHOULD ANY OF THE ABOVE DESCRIBED POLICIES BE CANCELLED BEFORE THE EXPIRATION DATE THEREOF, NOTICE WILL BE DELIVERED IN ACCORDANCE WITH THE POLICY PROVISIONS.

AUTHORIZED REPRESENTATIVE

Philip G. Dehn

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ACORD 25 (2016/03)

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| 22

SCHEDULE OF OTHER POLICIES

DATE ISSUED
3/17/2025

NAMED INSURED:

Kiewit Infrastructure South Co.
6301 NW 5th Way Suite 3200
Fort Lauderdale FL 33309

CERTIFICATE HOLDER:

City of Delray Beach
100 Northwest 1st Avenue
Delray Beach FL 33444

Type of Insurance	Insurer - NAIC No.	AI	SW	Policy Number	Eff / Exp	Limits
Contractor's Professional Liability 2025-2026	Old Republic Insurance Co. - 24147			MWZZ 312912	3/1/2025 3/1/2026	\$5,000,000 per claim / \$5,000,000 aggregate



OLD REPUBLIC INSURANCE COMPANY

445 South Moorland Road, Suite 300, Brookfield, Wisconsin 53005 | T: 262.797.3400

March 24, 2025

City of Delray Beach
100 Northwest 1st Avenue
Delray Beach, FL 33444

RE: Kiewit Infrastructure South Co.
Workers' Compensation Experience Modification Rating

Dear Sir or Madam:

As a Kiewit operating subsidiary, Kiewit Infrastructure South Co. has been assigned the following current and historical NCCI Interstate Workers' Compensation Experience Modification Rates:

2025	0.56	Effective Date: March 1, 2025
2024	0.55	Effective Date: March 1, 2024
2023	0.54	Effective Date: March 1, 2023
2022	0.51	Effective Date: March 1, 2022

Sincerely,

Philip G. Dehn
Authorized Representative



DRAFT

AIA® Document A305® – 2020

Exhibit D

Contractor's Past Project Experience

	1	2	3	4
PROJECT NAME	Tampa PIPES	OIA South Terminal C - Landside CMAR	Southport Turning Notch	Camino Real
PROJECT LOCATION	Tampa, FL	Green Cove Springs, FL	Ft. Lauderdale, FL	Boca Raton, FL
PROJECT TYPE	Water Supply - Transmission, Aqueducts - Pipeline Intake	Roadwork, Utilities, Drainage, Excavation, Paving, Paving Removal	Marine - Port, Harbor, Marina	Transportation - Bridges
OWNER	City of Tampa, FL	Clay County BOCC	Broward County, FL	Palm Beach County, FL
ARCHITECT	Hazen and Sawyer, P.C.	Wantman Group, Inc.	WSP	Wantman Group, Inc.
CONTRACTOR'S PROJECT EXECUTIVE	Cody Jensen	Nick Sosa	Cody Jensen	Cody Jensen
KEY PERSONNEL (include titles)	Project Director: Brad Williamson Project Manager: Joe Pescatore Lead Estimator: Abby Conner Superintendent: Luke Gutshall	Project Manager: Ben Kincaid Superintendent: Michael Cooper	Project Manager: Ben Kincaid Superintendent: Michael Cooper	Project Manager: Michael Graham Superintendent: Alberto Baca
PROJECT DETAILS	Contract Amount \$91 Million Completion Date 05/25/2025 % Self-Performed Work 40%	Contract Amount \$44.3 Million Completion Date 05/25/2025 % Self-Performed Work 68%	Contract Amount \$490 Million Completion Date 06/30/2025 % Self-Performed Work 10%	Contract Amount \$10.6 Million Completion Date 06/20/2019 % Self-Performed Work 75%
PROJECT DELIVERY METHOD	<input type="checkbox"/> Design-bid-build <input type="checkbox"/> Design-build <input type="checkbox"/> CM constructor <input type="checkbox"/> CM advisor <input checked="" type="checkbox"/> Other: Progressive Design-Build	<input type="checkbox"/> Design-bid-build <input type="checkbox"/> Design-build <input checked="" type="checkbox"/> CM constructor <input type="checkbox"/> CM advisor <input type="checkbox"/> Other:	<input type="checkbox"/> Design-bid-build <input type="checkbox"/> Design-build <input checked="" type="checkbox"/> CM constructor <input type="checkbox"/> CM advisor <input type="checkbox"/> Other:	<input checked="" type="checkbox"/> Design-bid-build <input type="checkbox"/> Design-build <input type="checkbox"/> CM constructor <input type="checkbox"/> CM advisor <input type="checkbox"/> Other:
SUSTAINABILITY CERTIFICATIONS			2022 Sustainable Technology Award - United States Green Building Council	

DRAFT AIA® Document A305® – 2020

Exhibit E

Contractor's Past Project Experience, Continued

	1	2	3	4
PROJECT NAME	SE 5 th Avenue Bridge Improvements	CFRC Capital Improvements - Multiple Locations	Bakers Haulover	Sandridge and Russel Road Reconstruction
PROJECT LOCATION	City of Pompano Beach	Clermont, FL	Miami Beach, FL	Green Cove Springs, FL
PROJECT TYPE	Transportation - Bridges	Transportation - Bridges	Transportation - Seawall	Roadwork, Utilities, Drainage, Excavation, Paving, Paving Removal
OWNER	City of Pompano Beach, FL	Florida Department of Transportation	Florida Department of Transportation	Clay County BOCC
ARCHITECT	TY Lin	AECOM	Bolton Perez and Associates	Wantman Group, Inc.
CONTRACTOR'S PROJECT EXECUTIVE	Cody Jensen	Nick Sosa	Thomas Thorn	Nick Sosa
KEY PERSONNEL (include titles)	Project Manager: Ben Kincaid Superintendent: Alberto Baca	Project Manager: David Pilon Superintendent: Andrew Wilkins	Project Manager: Jackson Bowers Superintendent: Alberto Baca	Project Manager: Ben Kincaid Superintendent: Michael Cooper
PROJECT DETAILS	Contract Amount \$2.8 Million Completion Date 02/04/2022 % Self-Performed Work 86%	Contract Amount \$30.6 Million Completion Date 12/31/2024 % Self-Performed Work 78%	Contract Amount \$8.94 Million Completion Date 08/01/2018 % Self-Performed Work 84%	Contract Amount \$44.3 Million Completion Date 05/25/2025 % Self-Performed Work 68%
PROJECT DELIVERY METHOD	<input type="checkbox"/> Design-bid-build <input type="checkbox"/> Design-build <input checked="" type="checkbox"/> CM constructor <input type="checkbox"/> CM advisor <input type="checkbox"/> Other:	<input checked="" type="checkbox"/> Design-bid-build <input type="checkbox"/> Design-build <input type="checkbox"/> CM constructor <input type="checkbox"/> CM advisor <input type="checkbox"/> Other:	<input checked="" type="checkbox"/> Design-bid-build <input type="checkbox"/> Design-build <input type="checkbox"/> CM constructor <input type="checkbox"/> CM advisor <input type="checkbox"/> Other:	<input type="checkbox"/> Design-bid-build <input type="checkbox"/> Design-build <input checked="" type="checkbox"/> CM constructor <input type="checkbox"/> CM advisor <input type="checkbox"/> Other:
SUSTAINABILITY CERTIFICATIONS				

TAB 4 4.2.4. PROPOSER'S
INFORMATION

4.2.4. PROPOSER'S INFORMATION

4.A. Legal Contracting Name

Name: Kiewit Infrastructure South Co.

State of Incorporation: Delaware

4.B. Ownership Structure Of Proposer's Company

Corporation

4.C. Proposer's W-9

Please see page 30 at the end of this section.

4.D. Proposer's Contact Information

Project Office:

6301 NW 5th Way, Suite 3200

Ft Lauderdale, FL 33309

(954) 835-2228

Please see page 8 for other office locations.

4.E. Proposer's Primary And Secondary Contact Information

PRIMARY CONTACT	SECONDARY CONTACT
Brad Williamson Office: (954) 835-2228 Cell: (239) 243-7857 Brad.Williamson@kiewit.com 6301 NW 5th Way, Suite 3200 Ft Lauderdale, FL 33309	Joe Pescatore Office: (954) 835-2228 Cell: (810) 432-1361 Joseph.Pescatore@kiewit.com 6301 NW 5th Way, Suite 3200 Ft Lauderdale, FL 33309

4.F. Proposer's Ownership Changes

Kiewit Infrastructure South Co. has had no ownership changes in the past three years and does not anticipate any within the next 6 months.

4.G. Proposer's Principal Company Officers

MILES, DAVID J.	HUCKS, JR., CHARLES E.	SWINTON, MATT L.
BENTLEY, WALTER L.	JOYCE, ANTHONY	VAN WAGNER, III EUGENE D.
GLASER, WILLIAM D.	KRUMWIEDE, CHRISTOPHER J.	APPLEGATE, BRUCE J.
CARLSGAARD, TERRY J.	OSTLUND, MICHAEL D.	JENSEN, CODY J.
CLEARY, TIMOTHY J.	PETERSEN, SHANE T.	SOSA, NICK
GOODRUM, PHILLIP B.	SODERBERG, PATRICK A.	DEMULLING, TRENT M.
NOLAN, JAMES M.	THOMAS, STEPHEN S.	BOYLE, THOMAS J.

4.H. Additional Information

Letters from a local Tampa resident and Kimley-Horn regarding Kiewit's performance on past projects.

Brad L. Baird, P.E., Deputy Administrator of Infrastructure
 107 N. Franklin Street
 Tampa, Florida 33602

Dear Mr. Baird,


I am writing to express my sincere appreciation for the outstanding service provided by the PIPES project team during the recent work on my block and at my residence 2517 West Ivy Street. After the pavers on my driveway were removed for the project, they unfortunately disappeared. However, one of the workers took the time to personally discuss the situation with me and ensured that the issue was fully resolved by replacing the missing pavers with equivalents and restoring the area as close to its original condition as possible.

I was very impressed by the team's professionalism and commitment to ensuring the work was done correctly, despite the unexpected challenge of the missing materials. This level of care and attention to detail reflects highly on the PIPES project and your leadership in overseeing these efforts.

Please extend my heartfelt thanks to the team for their dedication and efforts in going above and beyond to address the issue. It has made a positive difference in my experience as a homeowner and as a resident of Tampa.

Thank you again for your team's exceptional service.

Sincerely,



Darcy Goshorn
 2517 W Ivy St
 Tampa, FL 33607
 717-504-5355
 Darcy.Goshorn@gmail.com

Kimley»Horn

January 3, 2025

City of San Antonio Aviation Department
 9800 Airport Boulevard, San Antonio, Texas 78216

RE: RFP-Step 2: Construction Manager at Risk for Airfield Safety Enhancement and Improvements Program; RFP#: 2024-132

Mr. Jonathan Miranda,

Kimley-Horn is pleased to provide this letter of recommendation for Kiewit Infrastructure West Co. (Kiewit). As a trusted partner on numerous FAA-funded CMAR projects, including high-profile projects at Phoenix Sky Harbor International Airport, Kiewit has consistently demonstrated the expertise, collaboration, and innovative thinking necessary to deliver successful projects under challenging timelines and funding constraints.

Our partnership with Kiewit has been particularly impactful in the realm of FAA Civil CMAR projects. Over the years we have developed a strong working relationship with Kiewit that leverages our complementary skill sets—merging Kimley-Horn's design excellence with Kiewit's construction expertise. From the earliest stages of project development, Kiewit's involvement has been invaluable in identifying constructability challenges, offering practical solutions, and providing timely, detailed feedback that enhances the overall design process.

Kiewit's proactive engagement during preconstruction has been critical to ensuring that design concepts are both practical and cost-effective. Their insights into phasing, scheduling, and budget management have helped to refine project designs, streamlining construction and delivering significant cost savings. For example, their input on pavement sections, drainage systems, and utility alignments has led to engineering solutions that improve constructability while reducing costs and minimizing operational impacts on the airport. Their ability to provide rapid, detailed feedback during critical design milestones allowed for adjustments that kept project schedules on track. This responsiveness, combined with their hands-on approach to problem-solving, has been instrumental in delivering projects that meet the high standards expected by the FAA, PHX and airport stakeholders.

o ensuring that design g, scheduling, and budget ction and delivering drainage systems, and utility while reducing costs and id, detailed feedback during dules on track. This ing, has been instrumental in HX and airport stakeholders.

ork with Kiewit on multiple AA AIP funding with the projects that are delivered on time, within budget, and with minimal disruption to airport operations. Our work together on projects such as the New Crossfield Taxiway U, T3N2 Package 1 and PHX Sky Train Stage 1A have demonstrated the power of a well-integrated CMAR team to deliver innovative, high-quality results.

kimley-horn.com 1001 W Southern Ave, Suite 131, Mesa, AZ 85210 480 207 2666

4.I. Liquidated Damages And Details



April 9, 2025

The City of Delray Beach
 100 NW 1st Avenue
 Delray Beach, FL 33444

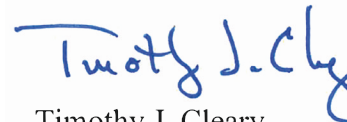
RE: RFQ No. 2025-023; Construction Manager at Risk Tropic Isle Neighborhood Improvements Project No. 19-015
 Kiewit Infrastructure South Co. Liquidated Damages Letter

Dear Sir or Madam:

In accordance with the RFQ No. 19-015; Construction Manager at Risk Tropic Isle Neighborhood Improvements project – Section 4.2.4 – Proposer's Information, paragraph I, Kiewit Infrastructure South Co. ("KISC") is to provide a list of liquidated damages and/or contract terminations.

KISC does not have a record of incurring liquidated damages in the last 10 years and has never been terminated for default.

Sincerely,
 Kiewit Infrastructure South Co.



Timothy J. Cleary
 Senior Vice President

KIEWIT INFRASTRUCTURE SOUTH CO.
 1550 Mike Fahey St., Omaha, NE 68102
 (402) 342-2052

4.J. Prior or Pending Litigation

These matters have no bearing on KISC's capability to perform the contracted services.

<u>MATTER NAME</u>	<u>PROJECT</u>	<u>MATTER SUMMARY</u>
Fulfillsource, LLC D/B/A Crewback v. Travelers Casualty and Surety Company and Kiewit Infrastructure South Co.	104903 Comanche Springs - US 28	Lawsuit against Kiewit Infrastructure South Co. and surety by a second-tier subcontractor alleging breach of contract and monies due for work performed on the Comanche Springs, US 285 in Pecos, Texas.
Parsons Transportation Group, Inc. of VA v. Capital Rail Constructors, Clark Construction Group, LLC and Kiewit Infrastructure South Co.	Dulles Corridor Metrorail Project	Lawsuit by designer against Capital Rail Constructors, a JV, Clark Construction Group, LLC and Kiewit Infrastructure South Co. alleging breach of contract regarding the Dulles Corridor Metrorail Project to extending the Silver Line metro system Project in Fairfax County, VA.
Fulfillsource, LLC d/b/a Crewback v. Travelers Casualty and Surety Company and Kiewit Infrastructure South Co.	104903 Comanche Springs - US 28	Lawsuit against Kiewit Infrastructure South Co. and surety by a second-tier subcontractor alleging breach of contract and monies due for work performed on the Comanche Springs, US 285 in Pecos, Texas.
Titan Florida, LLC v. Kiewit Infrastructure South Co., and Travelers Casualty and Surety Company of America.	103624; 103572 Selmon West Expressway	Lawsuit by concrete supplier against Kiewit Infrastructure South Co. alleging payments owed for work performed regarding the Selmon Expressway Western Extension in Tampa, FL.
Southgate Mobility Partners LLC and Southgate Constructors AJV v. Steven Johnson and Texas Sterling Construction	Midtown Express, 102412	Claim against a trucker and trucking company for damage to two bridges.
Kiewit Infrastructure South Co., v. MACK Industries, Inc. d/b/a MACK Concrete Industries, Inc., Western Surety Company, and Charlotte County, Florida	103686 Loveland Grand Master Lift Station	Lawsuit by Kiewit Infrastructure South Co. against supplier MACK Industries, Inc. d/b/a MACK Concrete Industries, Inc. regarding breach of contract, indemnification and a claim against Western Surety Company to pay surety bond regarding the Loveland Grand Master Lift Station Project, Charlotte County, Florida.
Kiewit Infrastructure South Co. v. AECOM Technical Services, Inc.	103572 Selmon Expressway	On August 31, 2021, Kiewit Infrastructure South Co. ("KISC") file suit against AECOM Technical Services, Inc. ("AECOM") to recover additional costs incurred by KISC due to significant necessary deviations from planned design elements detailed by AECOM in its pre-bid design in connection with the Selmon Expressway Western Extension in Tampa, Florida
Hayley Carson Odom Cordrays Mill Plantation, LLC v. Kiewit Infrastructure South Co, Andy O'Keefe and Kenneth Ledbetter	Cordrays Pond 23385	Lawsuit by Hayley Carson Odom alleging violations of the lease and various tort actions arising from Kiewit Infrastructure South Co.'s removal of material from her property in connection with the GDOT Project SR45 Cordrays Pond in Calhoun County, GA.
ABRAMS-KIEWIT JOINT VENTURE v. Stantec Consulting Services, Inc.	102447 Border West Expressway	Lawsuit by Abrams-Kiewit, JV against Stantec Consulting Services, Inc. alleging professional negligence and breach of contract regarding the Loop 375 Border Highway Extension Project in El Paso, Texas.
PE Pipeline LLC v. KIS and Murphy Pipeline Contractors LLC	105540 Tampa Pipes 1	Law suit filed by PE Pipeline, LLC. (a second-tier subcontractor to Kiewit Infrastructure South Co.). PE Pipeline, LLC alleges Murphy Pipeline Contractors, LLC, (a first-tier subcontractor to Kiewit Infrastructure South Co.) wrongfully failed to pay monies owed for work. In addition to suing Murphy Pipeline Contractors, LLC, PE Pipeline, LLC sued Kiewit Infrastructure South Co. alleging it is a third-party beneficiary of the subcontract between Murphy Pipeline Contractors, LLC and Kiewit Infrastructure South Co., and should pay the monies Murphy Infrastructure South Co. wrongfully withheld.

4.K. Financial Statements

Please refer to separate PDF named "Confidential Financial Statements".

Request for Taxpayer Identification Number and Certification

Go to www.irs.gov/FormW9 for instructions and the latest information.

Give form to the requester. Do not send to the IRS.

Before you begin. For guidance related to the purpose of Form W-9, see *Purpose of Form*, below.

Print or type. See Specific Instructions on page 3.	1 Name of entity/individual. An entry is required. (For a sole proprietor or disregarded entity, enter the owner's name on line 1, and enter the business/disregarded entity's name on line 2.) Kiewit Infrastructure South Co.		
	2 Business name/disregarded entity name, if different from above.		
	3a Check the appropriate box for federal tax classification of the entity/individual whose name is entered on line 1. Check only one of the following seven boxes. <input type="checkbox"/> Individual/sole proprietor <input checked="" type="checkbox"/> C corporation <input type="checkbox"/> S corporation <input type="checkbox"/> Partnership <input type="checkbox"/> Trust/estate <input type="checkbox"/> LLC. Enter the tax classification (C = C corporation, S = S corporation, P = Partnership) _____ Note: Check the "LLC" box above and, in the entry space, enter the appropriate code (C, S, or P) for the tax classification of the LLC, unless it is a disregarded entity. A disregarded entity should instead check the appropriate box for the tax classification of its owner. <input type="checkbox"/> Other (see instructions) _____		4 Exemptions (codes apply only to certain entities, not individuals; see instructions on page 3): Exempt payee code (if any) _____ Exemption from Foreign Account Tax Compliance Act (FATCA) reporting code (if any) _____ <i>(Applies to accounts maintained outside the United States.)</i>
	3b If on line 3a you checked "Partnership" or "Trust/estate," or checked "LLC" and entered "P" as its tax classification, and you are providing this form to a partnership, trust, or estate in which you have an ownership interest, check this box if you have any foreign partners, owners, or beneficiaries. See instructions <input type="checkbox"/>		
	5 Address (number, street, and apt. or suite no.). See instructions. 450 Dividend Drive		Requester's name and address (optional)
	6 City, state, and ZIP code Peachtree City, GA 30269		
	7 List account number(s) here (optional)		

Part I Taxpayer Identification Number (TIN)																																																
Enter your TIN in the appropriate box. The TIN provided must match the name given on line 1 to avoid backup withholding. For individuals, this is generally your social security number (SSN). However, for a resident alien, sole proprietor, or disregarded entity, see the instructions for Part I, later. For other entities, it is your employer identification number (EIN). If you do not have a number, see <i>How to get a TIN</i> , later.																																																
Note: If the account is in more than one name, see the instructions for line 1. See also <i>What Name and Number To Give the Requester</i> for guidelines on whose number to enter.																																																
	<table border="1" style="margin: auto;"> <tr> <th colspan="9">Social security number</th> </tr> <tr> <td style="width: 20px; height: 20px;"></td> <td style="width: 20px; height: 20px;"></td> <td style="width: 20px; height: 20px;"></td> <td style="width: 20px; height: 20px;"></td> <td style="width: 20px; height: 20px;"></td> <td style="width: 20px; height: 20px;"></td> <td style="width: 20px; height: 20px;"></td> <td style="width: 20px; height: 20px;"></td> <td style="width: 20px; height: 20px;"></td> </tr> <tr> <td colspan="3" style="text-align: center;">-</td> <td colspan="3" style="text-align: center;">-</td> <td colspan="3"></td> </tr> </table> or <table border="1" style="margin: auto;"> <tr> <th colspan="9">Employer identification number</th> </tr> <tr> <td style="width: 20px; height: 20px; text-align: center;">4</td> <td style="width: 20px; height: 20px; text-align: center;">7</td> <td style="width: 20px; height: 20px;"></td> <td style="width: 20px; height: 20px; text-align: center;">-</td> <td style="width: 20px; height: 20px; text-align: center;">0</td> <td style="width: 20px; height: 20px; text-align: center;">5</td> <td style="width: 20px; height: 20px; text-align: center;">3</td> <td style="width: 20px; height: 20px; text-align: center;">0</td> <td style="width: 20px; height: 20px; text-align: center;">3</td> <td style="width: 20px; height: 20px; text-align: center;">6</td> <td style="width: 20px; height: 20px; text-align: center;">7</td> </tr> </table>	Social security number																		-			-						Employer identification number									4	7		-	0	5	3	0	3	6	7
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4	7		-	0	5	3	0	3	6	7																																						

Part II Certification	
Under penalties of perjury, I certify that:	
1. The number shown on this form is my correct taxpayer identification number (or I am waiting for a number to be issued to me); and 2. I am not subject to backup withholding because (a) I am exempt from backup withholding, or (b) I have not been notified by the Internal Revenue Service (IRS) that I am subject to backup withholding as a result of a failure to report all interest or dividends, or (c) the IRS has notified me that I am no longer subject to backup withholding; and 3. I am a U.S. citizen or other U.S. person (defined below); and 4. The FATCA code(s) entered on this form (if any) indicating that I am exempt from FATCA reporting is correct.	
Certification instructions. You must cross out item 2 above if you have been notified by the IRS that you are currently subject to backup withholding because you have failed to report all interest and dividends on your tax return. For real estate transactions, item 2 does not apply. For mortgage interest paid, acquisition or abandonment of secured property, cancellation of debt, contributions to an individual retirement arrangement (IRA), and, generally, payments other than interest and dividends, you are not required to sign the certification, but you must provide your correct TIN. See the instructions for Part II, later.	
Sign Here	Signature of U.S. person
	Date 03/26/2025

General Instructions

Section references are to the Internal Revenue Code unless otherwise noted.

Future developments. For the latest information about developments related to Form W-9 and its instructions, such as legislation enacted after they were published, go to www.irs.gov/FormW9.

What's New

Line 3a has been modified to clarify how a disregarded entity completes this line. An LLC that is a disregarded entity should check the appropriate box for the tax classification of its owner. Otherwise, it should check the "LLC" box and enter its appropriate tax classification.

New line 3b has been added to this form. A flow-through entity is required to complete this line to indicate that it has direct or indirect foreign partners, owners, or beneficiaries when it provides the Form W-9 to another flow-through entity in which it has an ownership interest. This change is intended to provide a flow-through entity with information regarding the status of its indirect foreign partners, owners, or beneficiaries, so that it can satisfy any applicable reporting requirements. For example, a partnership that has any indirect foreign partners may be required to complete Schedules K-2 and K-3. See the Partnership Instructions for Schedules K-2 and K-3 (Form 1065).

Purpose of Form

An individual or entity (Form W-9 requester) who is required to file an information return with the IRS is giving you this form because they

TAB 5 4.2.5. EXPERIENCE,
ABILITY AND REFERENCES

4.2.5. EXPERIENCE, ABILITY AND REFERENCES

5.A. CMAR Experience

The Kiewit companies have been performing CMAR contracts over 25 years, when legislation was first passed to allow the contracting method. We have become one of the most experienced and successful CMAR contractors in the country because we understand how to become an extension of your team and serve as your advocate. This experience has allowed us to develop the tools and processes to make this procurement model successful. Below is a table of our most recent CMAR and other alternative delivery contracts.

CMAR Project Name/Location	Value
OIA South Terminal C - Landside, Orlando, FL	\$1.4 B
Russell and Sandridge Roads Reconstruction, Green Cove Springs, FL	\$44.4 M
Southport Turning Notch Expansion And Crane Rail Improvements, Ft. Lauderdale, FL	\$471.0 M
Orlando South Intermodal Terminal Facility, Orlando, FL	\$189.0 M
Lena Road Improvements, Manatee County, FL	\$4 M
60th Avenue, Manatee County, FL	\$900 K
SE 5th Avenue Bridge Improvements, Pompano Beach, FL	\$2.8 M
I-40 Gorge Bridges - Br. 57, Clyde, NC	\$18.9 M
I-40 Over Sr 1338 (Whie Oak Road) and Jonathan Creek, Clyde, NC	\$85.6 M
Alternative Delivery Project Name/Location	Value
Tampa Progressive Infrastructure Planning to Ensure Sustainability (PIPES) PDB, Tampa, FL	\$85.6 M
Selmon Expressway Western Extension DB, Tampa, FL	\$235.0 M
TBW Reservoir DB, Lithia, FL	\$106.0 M
Prospect Lake Clean Water Center DB, Ft. Lauderdale, FL	\$585.0 M

5.B. Project Experience

Please see pages 33-39.

5.C. References

City of Tampa, Florida
 Brad Baird | (813) 274-7883
 Brad.Baird@tampagov.net
 107 North Franklin Street,
 Tampa, FL 33602
 02/2020 - Present
 See page 33 for scope of work

Broward County, Florida
 Claude Gentil | (904) 529-4260
 cgentil@broward.org
 115 S Andrews Ave, Room 212,
 Ft. Lauderdale, FL 33301
 05/2017 - Present
 See page 35 for scope of work

AECOM (Consultant)
 George Gault | (407) 718-0907
 George.Gault@aecom.com |
 801 SunRail Drive
 Sanford, FL 32771
 03/2021 - 12/2024
 See page 38 for scope of work

5.D. Primary Personnel

Name	Position	Name	Position
Brad Williamson, PE, DBIA	Project Principal	Haider Cortez	Project Engineer
Joe Pescatore	Project Manager	Abby Conner	Lead Estimator
Fabricio Cedillo	Construction Manager	Mark Thomas	Drainage Superintendent
Lucas Gutshall	Utility Superintendent	Alberto Baca	Outfall/Structures Superintendent
Ryan Conner	Sanitary Superintendent		

5.E. Proposer's Accessibility

KISC's approach to accessibility is centered around a Project team that is available and committed to addressing the needs of the City and all stakeholders. We prioritize open and transparent communication, making sure that our team is consistently accessible for meetings, general communications, and supervision.

Our commitment extends beyond regular business hours, with team members available after hours and on weekends to address any urgent concerns or issues that may arise. This level of dedication guarantees that there is always a reliable point of contact for the City and residents, ensuring seamless coordination and effective project management throughout the duration of the project.

5.F. Proposer's Previous Accomplishments

Project Awards	Years
Orlando South Airport Intermodal Terminal Marquee Green Building Legacy Award - United States Green Building Council (USGBC)	2022
Selmon Expressway Western Extension ASBI Award of Excellence - American Segmental Bridge Institute (ASBI) Best In Construction - Expressway Authority Award Grand Award - Florida Transportation Builders Association (FTBA) Project Achievement in Transportation - Construction Management Association of America (CMAA) Project of the Year - American Council of Engineering Companies (ACEC) - Florida Slag Cement In Sustainable Concrete - CMAA - Florida Top 10 Bridges - Roads & Bridges Magazine	2021-2022
OIA South Terminal C - Landside CMAR Airport Business Project of the Year - Airport Business Magazine ENR SE Best Project: Airport/Transit - Engineering News-Record (ENR) - Southeast Sustainable Technology - United States Green Building Council (USGBC)	2022-2023
Big Bend Unit 1 Modernization Project ENR Southeast 2023 Regional Best Projects - Engineering News-Record (ENR)	2023
Largo Wastewater Reclamation Facility Biological Treatment Improvements Project of the Year - Design-Build Institute of America (DBIA) -Florida Region	2023
SR-5/US-1 Over Cow Key Channel Bridges Best in Construction - Community Awareness - Florida Transportation Builders Association (FTBA)	2021

TAMPA PROGRESSIVE INFRASTRUCTURE PLANNING TO ENSURE SUSTAINABILITY (PIPES) PDB

PROJECT OWNER:

City of Tampa, Florida

CONTACT:

Brad Baird

Brad.Baird@tampagov.net

LOCATION:

Tampa, FL

DATES OF PROJECT:

02/2020 - Present

PROJECT GMP:

\$91 Million

FINAL CONSTRUCTION COST:

\$85.6 Million

KISC saved the City of Tampa 5.8% in total contract value through Value Engineering and Shared Savings.

COMPONENTS OF PROJECT

- ✓ Alternative Delivery Project
- ✓ Roadway Rehabilitation
- ✓ Pavement Solutions for Muck Conditions
- ✓ Stormwater Infrastructure Improvements
- ✓ Drainage Outfall Enhancements
- ✓ Sanitary Sewer System Improvements
- ✓ Potable Water Main Improvements
- ✓ Considerations for Adjacent Residential Properties
- ✓ Coastal/Flooding Considerations
- Seawall Improvements

DESCRIPTION OF PROJECT

This ongoing Progressive Design-Build project is part of the City of Tampa's 20-year, \$2.8 billion infrastructure program, the largest in the City's history. The goal is to upgrade aging underground pipelines, transforming the over 100-year-old water and wastewater infrastructure that serves nearly 720,000 residents and businesses. The project includes over 18 miles of new potable water mains and 24.4 miles of sanitary sewer CIPP lining and new line upgrades in four Tampa neighborhoods: East Tampa, Forest Hills, MacFarlane Park, and Virginia Park. Each neighborhood has unique needs and aging infrastructure overdue for upgrades.

The KISC team developed a tailored outreach strategy engaging over 4,000 stakeholders across four neighborhoods. Efforts included 20+ community meetings, press events with Mayor Jane Castor, bilingual mailers, 24/7 project information access, and direct SMS updates to over 250 residents, ensuring inclusive and ongoing communication. Over 160 resident calls were efficiently managed by the Public Outreach team and Kiewit, alleviating the burden on City Office Lines.

The sanitary sewer mains, made of vitrified clay pipes, are 80 to 100 years old and have been fractured and damaged by tree roots. Many laterals providing service to private properties are deteriorated, undersized, or located in backyard easements with limited or no maintenance access. The infrastructure improvements for this project include:

- Water main replacement and upsizing
- New sanitary sewer mains, manholes, and service laterals
- Sewer, manhole, lateral rehabilitation, and cleanout installation
- Installation of new storm water pipe and structures
- Associated roadway and flatwork improvements

The rehabilitation program involves detailed inspections to identify deficiencies in the service lines and develop cost-efficient solutions to prevent future failures while minimizing impacts on residents, businesses, and the environment. The City's overall goal is to upgrade the aging infrastructure and avoid revisiting these neighborhoods for maintenance or emergency repairs for the next 50 years. These emergency repairs currently cost the city millions of dollars annually and cause significant unplanned disruptions to street traffic. By providing comprehensive neighborhood infrastructure upgrades, the City has greatly improved the resiliency of its water, storm, and wastewater systems throughout these neighborhoods.

OIA SOUTH TERMINAL C (STC) - LANDSIDE CMAR

PROJECT OWNER:

Greater Orlando Aviation Authority
(GOAA)

CONTACT:

Scott Shedek
scott.shedek@goaa.org

LOCATION:

Orlando, FL

DATES OF PROJECT:

01/2017 - 06/2023

PROJECT GMP:

\$1.4 Billion

FINAL CONSTRUCTION COST:

\$1.4 Billion



COMPONENTS OF PROJECT

- CMAR Project
- Roadway Rehabilitation
- Pavement Solutions for Muck Conditions
- Stormwater Infrastructure Improvements
- Drainage Outfall Enhancements
- Sanitary Sewer System Improvements
- Potable Water Main Improvements
- Considerations for Adjacent Residential Properties
- Coastal/Flooding Considerations
- Seawall Improvements

DESCRIPTION OF PROJECT

The KISC joint venture delivered CMAR services for the STC Phase 1 at Orlando International Airport, which was the first phase of a \$1.8 billion expansion program administered by GOAA. KISC's scope involved extensive drainage work, including stormwater canals and retention ponds, utility installation for the new terminal, pavement and roadway construction, maintaining traffic through the construction zone, minimizing construction impacts to airport operations, landscaping, and extensive coordination with multiple stakeholders. During the Pre-Construction phase, KISC developed four GMP packages, two shown below, which were negotiated and constructed as independent projects.

STC Site Logistics and Utilities (\$43 million) - KISC laid over 2 miles of new water and sewer pipe, installed a new roadway network to service the site, including grading, subgrade, base and asphalt pavement, roadway signage, and MOT during construction. In addition, this contract involved construction of drainage canals and retention ponds to improve stormwater management.

STC Airfield Civil Works (\$202 million) - This contract included constructing the new STC airfield paving and drainage system. KISC also performed extensive landscaping, including replacing/restoring the 160 acres that were disturbed during construction, replacing more than 40,000 LF of curb and gutter, constructing several retention ponds, installing irrigated planters, and integrated hardscaping to create a remarkable aesthetic for the airport expansion.

SOUTHPORT TURNING NOTCH EXPANSION AND CRANE RAIL IMPROVEMENTS

PROJECT OWNER:

Broward County, FL

CONTACT:

Claude Gentil
cgentil@broward.org

LOCATION:

Ft. Lauderdale, FL

DATES OF PROJECT:

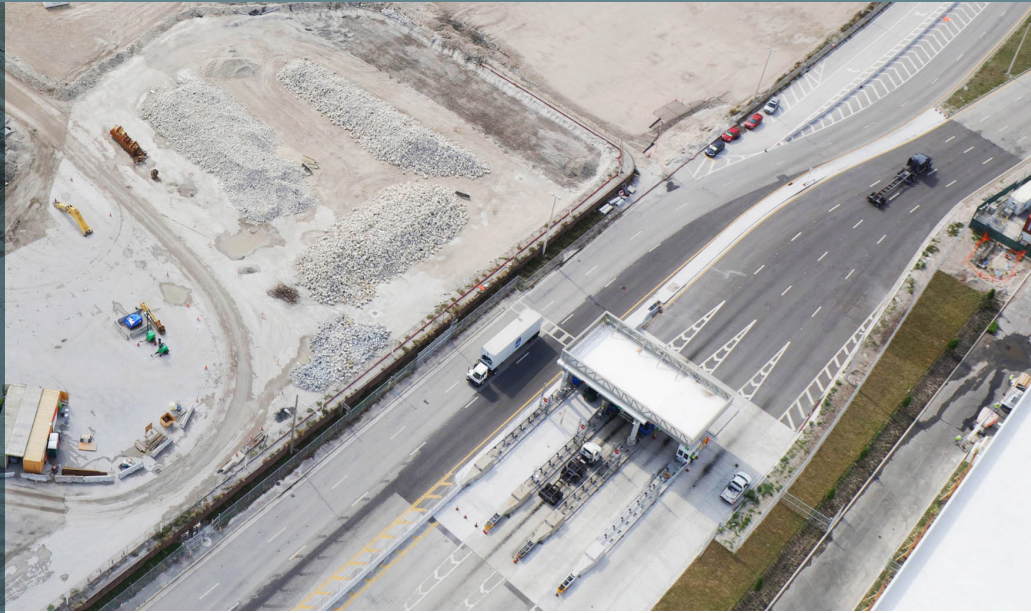
05/2017 - Present

PROJECT GMP:

\$471 Million

FINAL CONSTRUCTION COST:

\$480 Million



COMPONENTS OF PROJECT

- CMAR Project
- Roadway Rehabilitation
- Pavement Solutions for Muck Conditions
- Stormwater Infrastructure Improvements
- Drainage Outfall Enhancements
- Sanitary Sewer System Improvements
- Potable Water Main Improvements
- Considerations for Adjacent Residential Properties
- Coastal/Flooding Considerations
- Seawall Improvements

DESCRIPTION OF PROJECT

The Southport Turning Notch Expansion and Crane Rail Improvements CMAR Project is part of Port Everglades' multi-year expansion which will lengthen the existing deepwater turn-around area for cargo ships from approximately 900 feet to 2,400 feet and will allow for up to five new cargo berths. The Turning Notch portion of the project will extend the Port's Southport Turning Notch by approximately 1,500 feet to the west, with 1.5 miles of seawall/wharf construction, marine and utility infrastructure, and 18 acres of upland improvements. The project requires both excavation of 3 million cubic yards of material, processed on-site, and environmental mitigation, including the creation of an artificial reef off the coast and relocation of coral from the existing turning notch to the new reef. **During preconstruction, KISC identified poor soil conditions, including 72,000 cubic yards of contaminated muck requiring removal, to ensure a stable foundation for pavement and improvements. The team implemented a comprehensive remediation plan, excavating unsuitable material and preparing the site for long-term structural integrity.**

The Crane Rail Improvement Project includes the construction of a new electrical switchgear building, 2.2 miles of electrical duct bank, and 4,500 LF of a new gantry crane foundation system. Once completed, the crane rail project will support 5 new gantry cranes. The project is utilizing a vast equipment fleet, including drag lines to excavate the Turning Notch Expansion. The project is the largest undertaken in Broward County's history, and requires major coordination amongst the workforce, subcontractors, and port tenants, who cannot be impacted.

CAMINO REAL BRIDGE RECONSTRUCTION

PROJECT OWNER:
Palm Beach County, Florida

CONTACT:
Dan Doyle
ddoyle@pbcgov.org

LOCATION:
Boca Raton, FL

DATES OF PROJECT:
01/2018 - 06/2019

PROJECT GMP:
N/A

FINAL CONSTRUCTION COST:
\$10.6 Million



COMPONENTS OF PROJECT

- CMAR Project
- Roadway Rehabilitation
- Pavement Solutions for Muck Conditions
- Stormwater Infrastructure Improvements
- Drainage Outfall Enhancements
- Sanitary Sewer System Improvements
- Potable Water Main Improvements
- Considerations for Adjacent Residential Properties
- Coastal/Flooding Considerations
- Seawall Improvements

DESCRIPTION OF PROJECT

The Camino Real Bridge Construction project, completed by KISC in 2019, was a significant undertaking aimed at rehabilitating the 80-year-old bridge over the Intracoastal Waterway in Boca Raton, Florida. **The Tropic Isle Neighborhood Improvements Project and the Camino Real Bridge Reconstruction share several key similarities in terms of complexity, stakeholder coordination, and the need for minimal community disruption.**

This fast-track project was needed to bring the old movable bridge to current standards. Because the bridge is historic and an anchor to the Camino Real Historic District, great effort was taken in the design and construction to preserve its character. The height, tender house architecture, and railings were rehabilitated to match the 1930s era design, and the bridge was widened to allow for greater pedestrian and bicyclist safety. The project involved extensive repairs and upgrades, including replacing worn mechanical motor couplers and completing structural steel repairs. Additionally, a major Comcast line was identified as potentially lying within the footprint of the new bridge tender house, resulting in consultations with all parties to determine the best solution. Ultimately, it was decided to relocate the Comcast line. During the construction period, alternative routes were provided to minimize disruption, and the reopening of the bridge was marked by a ribbon-cutting ceremony, highlighting the importance of the bridge to the local community.

Similarly, the Tropic Isle Neighborhood Improvement Project will require coordination with utility companies and homeowners, ensuring seamless integration of improvements without disrupting essential services.

SE 5TH AVENUE BRIDGE IMPROVEMENTS

PROJECT OWNER:

City of Pompano Beach

CONTACT:

Fernand Thony
Fernand.Thony@copbfl.com

LOCATION:

Pompano Beach, FL

DATES OF PROJECT:

01/2021 - 02/2022

PROJECT GMP:

\$2.8 Million

FINAL CONSTRUCTION COST:

\$2.8 Million

COMPONENTS OF PROJECT

- CMAR Project
- Roadway Rehabilitation
- Pavement Solutions for Muck Conditions
- Stormwater Infrastructure Improvements
- Drainage Outfall Enhancements
- Sanitary Sewer System Improvements
- Potable Water Main Improvements
- Considerations for Adjacent Residential Properties
- Coastal/Flooding Considerations
- Seawall Improvements

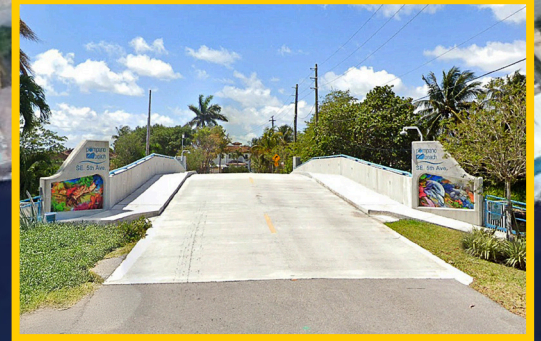
DESCRIPTION OF PROJECT

This \$2.8 million CMAR project involved the replacement of a deficient 120-ft.-long bridge, originally built in 1959, which serves as a critical connection in Pompano Beach. The SE 5th Avenue Bridge had become structurally deficient, necessitating its replacement to ensure the safety and accessibility of the local community. **This project shares similarities with the Tropic Isle Neighborhood Improvements Project, including local CMAR experience and the ability to manage complex construction in an environmentally sensitive residential area. KISC maintained driveway access for residents and protected endangered species, including manatees.**

KISC supported the City throughout preconstruction by providing value engineering and intermediate pricing throughout the design phase. KISC also self-performed the bridge demolition, precast concrete pile installation, slope protection, drainage, and bridge construction.

The project faced several challenges, including construction in a high-profile neighborhood with challenging access. However, KISC's local CMAR experience and innovative solutions ensured the project's success. Environmental considerations were also a priority, with efforts made to minimize disruption to the surrounding ecosystem and waterway.

Construction of the new bridge required the demolition and reconstruction of the adjacent seawall using precast concrete panels. The new bridge not only improves traffic flow and safety but also brings long-term economic benefits to the area.



CFRC CAPITAL IMPROVEMENTS

Clermont, FL

PROJECT OWNER:

Florida Department of Transportation

CONTACT:

George Gault
George.Gault@aecom.com

LOCATION:

Clermont, FL

DATES OF PROJECT:

03/2021 - 12/2024

PROJECT GMP:

N/A

FINAL CONSTRUCTION COST:

\$30.6 Million



COMPONENTS OF PROJECT

- CMAR Project
- ✓ Roadway Rehabilitation
- Pavement Solutions for Muck Conditions
- ✓ Stormwater Infrastructure Improvements
- ✓ Drainage Outfall Enhancements
- ✓ Sanitary Sewer System Improvements
- Potable Water Main Improvements
- Considerations for Adjacent Residential Properties
- ✓ Coastal/Flooding Considerations
- Seawall Improvements

DESCRIPTION OF PROJECT

The Central Florida Rail Corridor (CFRC) Capital Improvements project consists of infrastructure improvements along a 64.2-mile rail corridor owned by FDOT in central Florida, including new drainage and stormwater features, ditching, underdrains, culvert extensions, rail corridor widening (sub-ballast), interchanges, landscaping, and regrading. **Both the CFRC Capital Improvements and Tropic Isle Neighborhood Improvements project require working closely with multiple agencies, coordinating with existing utilities, and ensuring that all construction efforts are executed without disrupting essential infrastructure. KISC's experience in successfully executing complex stormwater infrastructure in an active rail environment will translate directly to Tropic Isle's drainage and roadway improvements in a busy residential area.**

An example of major stormwater system reconstruction is the new double box culvert at the railroad bridge over the East City Canal (MP 806.9). It serves to accommodate the construction of a second track. Prior to the project's improvements, the culverts could only accommodate a single track. Replacement of the culvert involved a temporary creek diversion into another existing culvert, removing and replacing pipes, temporary concrete works, demolition of the culvert to be replaced, construction of the new box culvert (2 EA 8'x10') with weekend pours coordinated with train schedules, backfill, cleanout, and installing handrail.

The project requires close coordination with railroads and the CFRC and involves urban construction when the rail corridor passes through roadways. KISC understands the importance of preserving mobility and minimizing construction impacts, therefore our construction approach was focused on maintaining rail traffic, coordinating with stakeholders, and construction optimizations.

BAKERS HAULOVER

Miami Beach, FL

PROJECT OWNER:

Florida Department of Transportation

CONTACT:

Pablo Orozco, PE
pablo.orozco@dot.state.fl.us

LOCATION:

Miami, FL

DATES OF PROJECT:

01/2017 - 04/2019

PROJECT GMP:

N/A

FINAL CONSTRUCTION COST:

\$9 Million

 Seawall Improvements



DESCRIPTION OF PROJECT

This FTBA Best in Construction project rehabilitated Bakers Haulover Bridge to extend the life of the structure, which connects the northern end of Biscayne Bay with the Atlantic Ocean. The scope for this unique, riveted steel plate, twin-girder bridge involved rebuilding the seawalls on both sides of the inlet (191 LF on the northside and 140 LF on the southside) underwater concrete formwork, and repairing, rehabilitating, and painting the bridge.

MIAMI WOMAN'S CLUB SEAWALL AND BAYWALK

Miami, FL

PROJECT OWNER:

City of Miami

CONTACT:

Orlando Misas
omisas@miamigov.com

LOCATION:

Miami, FL

DATES OF PROJECT:

10/2018 - 07/2019

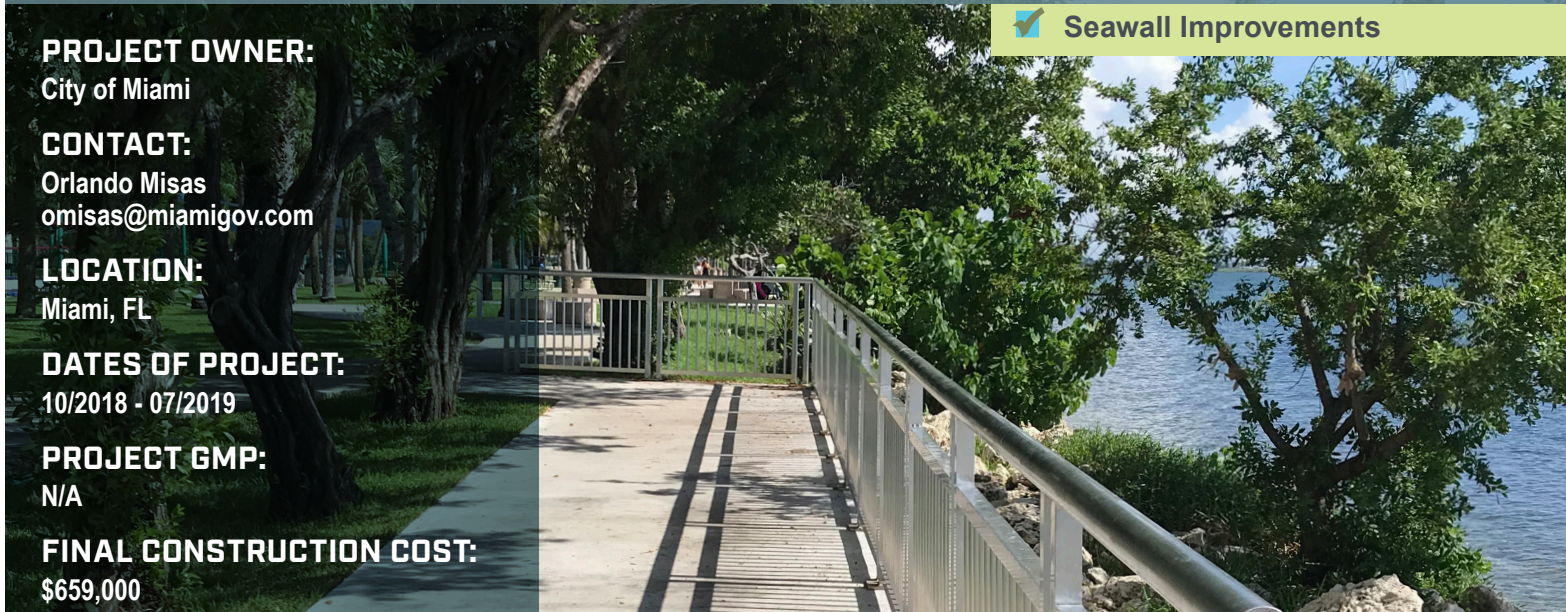
PROJECT GMP:

N/A

FINAL CONSTRUCTION COST:

\$659,000

 Seawall Improvements



DESCRIPTION OF PROJECT

The Miami Woman's Club Baywalk Project consisted of modifying an existing seawall into a new baywalk spanning between the existing marina dock tying into the Margate Pace Park existing sidewalk. Environmental measures included manatee grates, silt fence and turbidity barrier. Existing trees and asphalt was removed. Temporary shoring was installed beneath the seawall prior to score cutting and demolition of the top section of the seawall.

TAB 6 4.2.6. ORGANIZATIONAL
STRUCTURE AND CAPACITY

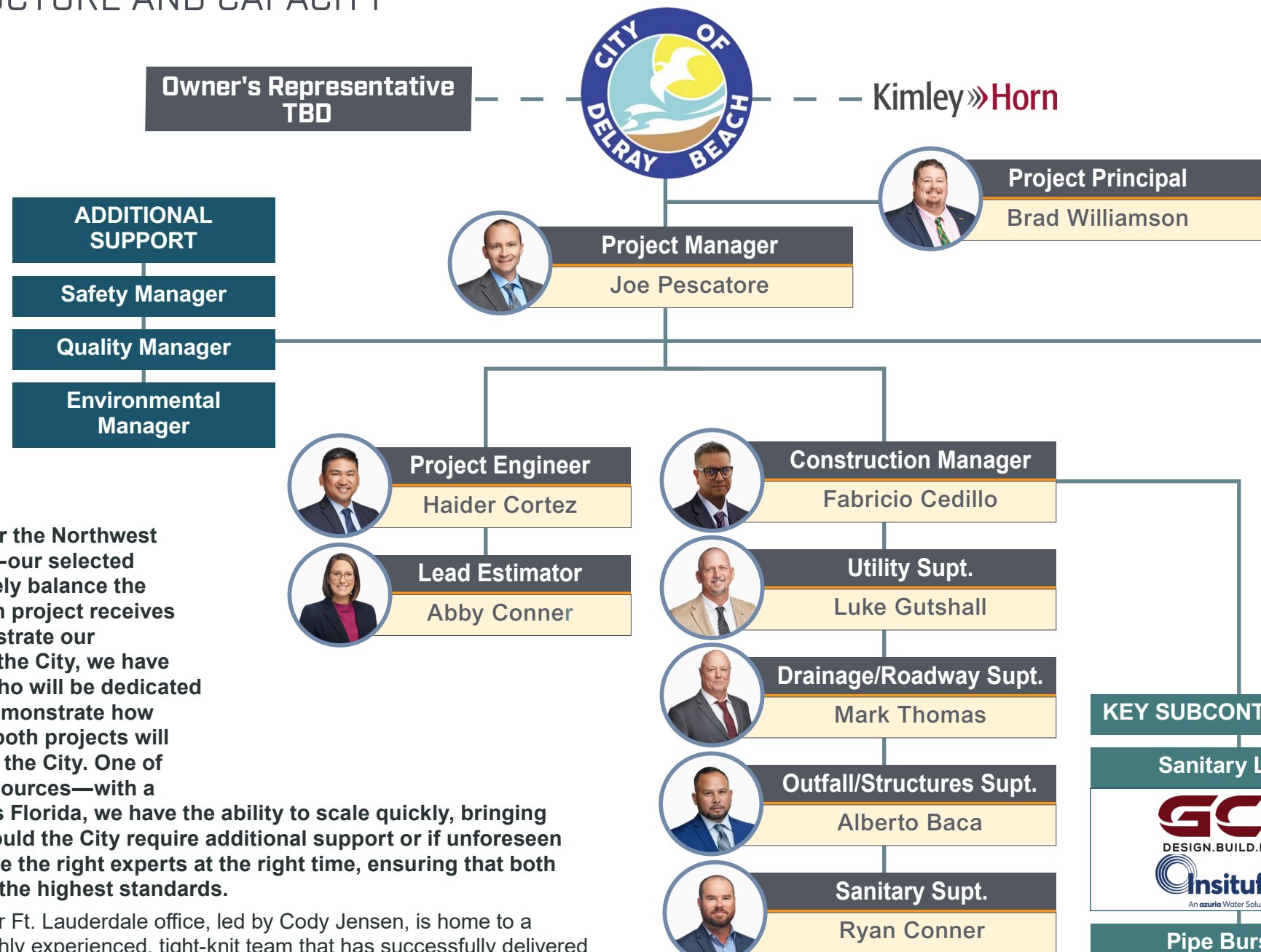
4.2.6. ORGANIZATIONAL STRUCTURE AND CAPACITY

6.B. Organizational Chart

6.C. Staffing Resources

KISC, a subsidiary of Kiewit Corporation, has operated as a contractor for 55 years, maintaining a strong presence through a decentralized network of district and area offices across the United States. This structure allows Kiewit Corporation's subsidiaries to function as competitive local contractors with the financial strength and expertise of a national firm—backed by an industry-leading bonding capacity and annual construction revenues exceeding \$17.1 billion. With a workforce of more than 31,100 skilled professionals and craft, Kiewit and its subsidiaries have delivered some of the most complex and large-scale infrastructure projects across the continent.

We recognize that several key personnel in our organizational chart were also proposed for the Northwest Neighborhood CMAR project. This is intentional—our selected leadership team has the proven ability to effectively balance the needs of large dollar projects while ensuring each project receives the dedicated focus it requires. To further demonstrate our commitment to delivering exceptional results for the City, we have strategically incorporated additional personnel who will be dedicated to this Project. We welcome the opportunity to demonstrate how utilizing the same Project Management Team for both projects will provide significant cost and schedule savings for the City. One of Kiewit's greatest strengths is the depth of our resources—with a vast network of experienced professionals across Florida, we have the ability to scale quickly, bringing in specialized expertise whenever necessary. Should the City require additional support or if unforeseen challenges arise, we have the flexibility to mobilize the right experts at the right time, ensuring that both projects remain on schedule and are executed to the highest standards.



KEY SUBCONTRACTORS

- Sanitary Lining
 - GCU DESIGN.BUILD.DELIVER.
 - Insituform An ezuria Water Solutions Company
- Pipe Bursting
 - MPC A PURIS COMPANY
- Asphalt
- Flatwork
- Landscaping

Our Ft. Lauderdale office, led by Cody Jensen, is home to a highly experienced, tight-knit team that has successfully delivered numerous projects together. For preconstruction efforts, our team will operate from this office, which is strategically located just 21 miles from City Hall and 20 miles from the Swinton Operations Center. As construction ramps up, we will establish a dedicated local project office to ensure a fully operational, on-the-ground presence adjacent to the site. We have maintained a consistent work history in Florida spanning more than 420 projects that has resulted in many repeat clients. Our team of key personnel has been strategically assembled to bring local experience, CMAR proficiency, subject matter and technical expertise, and an established team cohesion, having worked on several previous similar projects together. Our well-established relationships with local subcontractors, suppliers, and national specialty firms further reinforce our ability to execute seamlessly and deliver exceptional results for the City.

We Are Your Local Builder

- Local Florida contractor/local key staff
- Key staff experience with South Florida clients
- Strong craft workforce living near the project location
- Local market knowledge
- Largest privately owned equipment fleet in the nation
- Available resources for quick project startup

Local Office Location:
6301 NW 5th Way, Suite 3200
Ft. Lauderdale, FL 33309

FLORIDA
STAFF: **401**
CRAFT: **573**



6.A. Project Team

Brad Williamson, PE, DBIA Project Principal



Brad has 18 years of experience with Kiewit, during which he has worked on over \$2 billion in alternative delivery projects, including CMAR, Progressive Design-Build and Design-Build Projects. As a project manager, Brad has overseen \$500 million worth of work, leading his team to success. He has successfully negotiated multiple change orders and Guaranteed Maximum Prices (GMPs) with clients. Brad believes that transparency, strong relationships, and effective execution are key to building successful projects and securing repeat clients.

Tenure in the Industry:

18 Years

Tenure at Kiewit:

18 Years

Education:

B.S. Civil Engineering, VA Polytechnic Institute and State University, 2006

Why Brad?

- ✓ **Proven Expertise in Alternative Delivery Methods**
- ✓ **Deep Understanding of the Local Market**
- ✓ **Proactive Problem-Solver**
- ✓ **Empowers Team to Deliver High-Quality, Community-Driven Results**
- ✓ **Committed to Open, Honest, and Transparent Communication**

RECENT SIMILAR PROJECTS

Project Principal | Tampa PIPES | Tampa, FL | \$91 Million



Brad and his team navigated the negotiations of three Guaranteed Maximum Price (GMP) agreements with the City of Tampa on this Progressive Design-Build Project. This included multiple iterations of pricing and scope confirmation with vendors, as well as transparent and organized conversations with the Client. The city is very happy with KISC's willingness to listen, adapt, and provide solutions. Brad successfully negotiated these agreements with four City departments that were not always synchronized in their goals. Brad directly supervises all aspects of the project. During the preconstruction efforts, he led constructability reviews, reviewed scopes, as well as helped lead Abby Conner in pricing the GMPs. Close coordination with Public Outreach is critical on this project, as KISC is working in residents' front and back yards. This project includes over 83,000 LF of new waterline, stormwater, wastewater and transportation improvements throughout four Tampa neighborhoods.

Project Manager | Selmon Expressway Western Extension | Tampa, FL | \$235 Million

Brad was responsible for all project operations, including planning and scheduling, implementing the QC and Safety program, coordinating with the designers and THEA, managing budget, and overseeing daily operations. This was a highly technical project that required additional Kiewit subject matter experts. During design development, Brad successfully worked through multiple technical challenges with both THEA and FDOT. The project included a pre-cast segmental concrete box girder viaduct, over 11,000 LF of drainage, over 280 drainage structures, over 122,000 sy of stabilized roadway

and reconstruction, over 228,000 CY of embankment, along with flatwork, curb & gutter, landscaping, signalization, ITS, signage, and lighting.

Project Principal | Manatee County CMAR projects | Manatee County, FL | \$90 Million

KISC's team has prepared GMPs for two different projects with Manatee County, Florida. KISC has one executed GMP agreement, and multiple in the works with the County.

Structures Superintendent | SR-90 Tamiami Trail | Miami, FL | \$104 Million

Brad led the planning of the structures operations, scheduled and directed crews, monitored safety and quality, and assisted in tracking costs. He offered support with utility potholing, constructability reviews, value engineering, and performed test sections in the field for the design team to witness. The project scope was to reconstruct and raise 9.75 miles of the existing roadway and also build a new 1-mile-long bridge adjacent to the existing roadway.

Project Manager | I-95 Joint Repairs | Palm Beach, FL | \$1.8 Million

Brad was responsible for leading job planning and scheduling sessions, providing oversight for controlling schedule, cost, safety, quality, administration, material procurement, and equipment maintenance. This project included replacing expansion joints at eight bridges at the I-95 & PBIA interchange.



Joe Pescatore Project Manager

Joe's experience working alongside owners and designers on various design-build projects, from preconstruction to closeout has allowed him to ensure a project is completed on time and on budget while meeting all stakeholder's expectations. Joe's passion for delivering a banner project is demonstrated by his dedication to understanding the goals of the end-users, continued communication through each step of the process with the Owner, and unwavering commitment to doing the right thing.

Tenure in the Industry:

15 Years

Tenure at Kiewit:

11 Years

Education:

B.S. Civil Engineering, University of Notre Dame, 2009

Why Joe?

- ✓ **Extensive Design-Build/
Alternative Delivery
Experience**
- ✓ **Clear, effective
communicator with clients,
designers, and the public**
- ✓ **Committed to strong
project team relationships**
- ✓ **Experienced in working
within residential
communities**

RECENT SIMILAR PROJECTS

Project Manager | Tampa PIPES | Tampa, FL | \$91 Million



Joe managed the schedule and budget through daily coordination with the Project Team, the City of Tampa Contract Administration team and Water Department and Wastewater Department Engineers. Public Outreach is crucial on the project with work designed throughout four diverse neighborhoods of Tampa; working with the public relations team, Joe ensured resident questions and concerns were addressed and satisfied in a timely manner. The wastewater scope included 24.4 miles of sanitary mainline rehabilitation using cured-in-place pipe, over 1,000 service laterals were rehabilitated and repaired, over 530 manholes were rehabilitated, new manholes were installed to facilitate trenchless repairs, and point repairs were performed as identified throughout the project. The water scope included 18 miles of water main installation, the majority of which were via trenchless methods, HDD or Pipe Bursting. Stormwater scope included installation of 1,256 LF of storm pipe and 25 ea. stormwater structures.

Project Engineer | Metro Gold Line Foothill Extension Phase 2A | Pasadena, CA | \$515 Million

Joe managed subcontracts, including buy-out, monthly pay applications, and closeout, the project schedule, monthly owner billing, and the conversion of the project controls from Kiewit's legacy system to the current InEight Project Controls software. The design-build project extended the light rail line from Pasadena to Azusa, covering 11.5 miles with six new commuter rail stations. The project included complete street components such as pedestrian pathways, bike lanes, and improved roadways to enhance accessibility and safety. The project also featured significant infrastructure improvements, including 15 bridges and a 24-acre maintenance and operations facility.

Superintendent | Blue Water Energy Center | East China, MI | \$1 Billion

Joe managed field construction activities, subcontractors, and material receiving for the electrical construction of the boiler feedwater buildings, HRSGs, and Pipe Racks of the \$1B EPC gas-fired power plant construction project. This challenging project, built during the peak of Covid, extended peak manpower through the middle of the winter, resulting in significant craft turnover and procurement challenges. Joe was responsible for planning, scheduling, and estimating electrical scopes while coordinating with multiple trades for the successful completion of this project.

Lead Field Engineer | Cove Point LNG Export Terminal | Lusby, MD | \$3.8 Billion

Joe was involved early with this mega-project, as part of the construction team working alongside engineers, he helped guide the design during the constructability phase of the project, ensuring safety, quality, and productivity were built into the plans. During the preconstruction phase, Joe oversaw the planning of construction, building constructable work plans for each phase of the aboveground construction. Joe was part of the team that reviewed and evaluated new and innovative construction materials eventually successfully incorporated into the project and utilized in many future power projects. As Lead Field Engineer in the construction phase, Joe managed a team of field engineers in the implementation of the design and planning of the Power Block of the LNG Export Facility. Verifying that best safety and quality practices were followed, Joe helped ensure the construction activities were completed successfully, under budget.



Fabricio Cedillo

Construction Manager

Fabricio has 18 years of experience in the construction industry, specializing in grading and underground utility operations. He has a strong knowledge of civil work and has been involved in projects ranging from \$1 million to \$100 million. During preconstruction, he can assist with constructability reviews and cost modeling as the design is developed. Once construction begins, Fabricio will ensure the work is built both safe and with high quality. He has the CMAR experience to ensure the project is successfully completed on or ahead of schedule.

Tenure in the Industry:

18 Years

Tenure at Kiewit:

7 Years

Education:

Central Catholic High School

Why Fabricio?

- ✓ **Alternative Delivery Experience**
- ✓ **Extensive South Florida Construction Experience**
- ✓ **Constructability Review Expert**
- ✓ **Experience working in neighborhood environments**

RECENT SIMILAR PROJECTS

Superintendent | Venetian Bridges Rehabilitation | Miami, FL | \$7.1 Million

Fabricio provided oversight of project control functions including scheduling, cost control, contract administration, planning, and estimating. This project consisted of repairing concrete spalls and cracks along the beams and decks of 12 bridges along the causeway and replacing 4,000 bolts, 5,000 lbs. of structural steel, and repainting on the causeway's two bascule bridges.

Project Engineer | Port Everglades Bridge | Ft. Lauderdale, FL | \$3.8 Million

Fabricio managed the project controls, primavera schedule, contract administration, subcontracts, procurement, daily cost, and schedule. This project was to complete the bridge portion of the Port Everglades project located at the Port of Fort Lauderdale. A new bridge was completed over the FPI discharge canal in Port Everglades. The scope of work included MSE walls, the demolition and rebuild of a bulkhead cap, and 1000 tons of rip rap installation.

Project Engineer | I-595 Overlay | Ft. Lauderdale, FL | \$1.1 Million

Fabricio inspected field conditions, identified cost-saving measures, ensured compliance with the contract plans and specifications, managed quantity tracking, costs, and schedule, prepared monthly cost projections, managed document control, managed requests for informations, and provided

corrective actions as needed on site. This project involved High Friction Epoxy Overlay on asphalt and concrete surfaces on the entrance ramp to Ft. Lauderdale-Hollywood International Airport in Broward County, FL. Key project features include a floating bridge, high rise building, and a combined cycle power generation facility.

Project Engineer | SR 90 Tamiami Trail | Miami, FL | \$104 Million

Fabricio provided oversight of project control functions including scheduling, cost control, contract administration, planning, and estimating. SR 90 Tamiami Trail is a two-lane undivided rural highway that runs through the Everglades National Park connecting Miami to Tampa. The project was to reconstruct and raise 9.75 miles of the existing roadway and also build a new 1-mile-long bridge adjacent to the existing roadway. KISC provided nearly ten miles of roadway and drainage work, which required two significant redesign efforts due to utility conflicts and unanticipated settlement. KISC worked closely with USACE to ensure all of their needs were met on this important project.

Project Manager | SR 874/Don Shula Expressway Ramp Connector to SW 128th Street | Miami, FL | \$48.5 Million

Fabricio was the project manager on this job prior to joining Kiewit. He oversaw all aspects of this project including safety, quality, schedule, and cost control. The project involved constructing a new access ramp from the southern terminus of SR 874 to SW 128th Street and widening SW 128th Street from two to four lanes between SW 122nd Avenue and SW 137th Avenue. Additionally, it included roadway improvements to support increased traffic flow and enhance safety, along with traffic management measures to minimize disruptions during construction.



Haider Cortez Project Engineer

Haider is a highly skilled Project Engineer with eight years of experience in construction, specializing in cost control, design coordination, and permit acquisition. His expertise includes managing drainage and stormwater infrastructure projects across multiple delivery models, including Progressive Design-Build. Haider excels in subcontractor coordination, drawing and specification reviews, and providing critical support for estimating and engineering tasks. Having worked extensively in Florida's climate and challenging soil conditions, he understands how to develop realistic, weather-conscious schedules that keep projects on track. His ability to navigate permitting processes, optimize designs, and proactively solve field challenges makes him an invaluable asset to any project team.

Tenure in the Industry:

8 Years

Tenure at Kiewit:

3 Years

Education:

B.S. Civil Engineering, Mapua University

Engineer-in-Training, FL
#1100025715

Why Haider?

- ✓ **Alternative Delivery Experience**
- ✓ **South Florida Construction Experience**
- ✓ **Proven ability of developing project schedules, optimizing design for constructability, and implementing effective maintenance of traffic (MOT) solutions**

RECENT SIMILAR PROJECTS

Project Engineer | Tampa PIPES | Tampa, FL | \$91 Million



As the Project Engineer, Haider supported project scheduling, ensuring construction activities aligned with milestone deadlines while proactively adjusting for Florida's unpredictable weather conditions to minimize delays. He coordinated subcontractor activities, overseeing procurement, scope alignment, and performance to maintain quality and efficiency throughout the project lifecycle. Haider collaborated with cross-disciplinary teams, including designers, city officials, and field personnel, to streamline coordination and maintain project momentum. The wastewater scope included 24.4 miles of sanitary mainline rehabilitation using cured-in-place pipe, over 1,000 service laterals were rehabilitated and repaired, over 530 manholes were rehabilitated, new manholes were installed to facilitate trenchless repairs, and point repairs were performed as identified throughout the project. The water scope included 18 miles of water main installation, the majority of which were via trenchless methods, HDD or Pipe Bursting. Stormwater scope included installation of 1,256 LF of storm pipe and 25ea stormwater structures.

Prior Firm Experience - Project Engineer, DMCI

Beginning his career as a QA/QC Engineer, Haider gained hands-on experience ensuring compliance with engineering specifications, contract requirements, and industry quality standards. He conducted joint inspections with the Owner's Construction Management team, reviewed procurement documents for quality assurance, and developed quality control reports to track non-compliance issues, material deviations, and corrective actions. His work ensured high construction standards, reduced rework, and improved project efficiency, setting the foundation for his expertise in design coordination and constructability improvements.

Haider then transitioned into a preconstruction role, where he applied his field experience to cost estimation and project feasibility assessments. He performed detailed quantity take-offs for structural and architectural works, conducted site inspections to analyze project viability, and developed accurate cost models based on material, labor, and scheduling considerations. His ability to define project scope, identify cost drivers, and anticipate potential challenges provided critical insights for budget development and risk management.



Abby Conner Lead Estimator

Abby is a Lead Estimator with 18 years of experience in construction, specializing in project cost control, design management and permit acquisition. She has experience in estimating for sanitary and storm sewer projects across various delivery models, including PDB and design-build. Abby has worked on 9 alternative delivery projects and has presented over 18 GMPs to Kiewit's clients in her career. Her expertise in preconstruction estimates and collaborative delivery projects enhances thorough cost control and effective project management, delivering reliable cost projections and contributing to successful project execution.

Tenure in the Industry:

19 Years

Tenure at Kiewit:

19 Years

Education:

B.S. Civil Engineering, Purdue University, 2005

Why Abby?

- ✓ Ensures accurate budgeting with a clear financial baseline from the start
- ✓ Drives transparency through open-book estimating and collaborative cost analysis
- ✓ Optimizes project efficiency by lowering costs and improving constructability

RECENT SIMILAR PROJECTS

Lead Estimator | Tampa PIPES | Tampa, FL | \$91 Million



Abby is providing constructability and estimating reviews, cost control, and subcontractor administration. She led the estimate, and assisted with negotiations on three successful GMP proposals with the City of Tampa. Her assistance gauged the financial risk on the project. Work includes 24.4 miles of sanitary sewer rehabilitation using cured-in-place liners, trenchless sanitary sewer installation under CSX rail tracks, and 18 miles of water main installation.

Lead Estimator | OIA South Terminal C - Landside CMAR Orlando, FL | \$1.4 Billion



Abby prepared cost estimates, complete constructability reviews, and incorporated VE options throughout the development of the terminal GMPs. She also prepared scope of work packages, then solicited and administered these subcontractor packages. Work consisted of constructing Parking Garage #2, central energy plant, ground transportation facility, emergency power generator, Landside terminal, Arrival/Departure Bridge. The civil package consisted of sanitary, lift stations, storm drainage, water main, force main, fire main, landscape 60 acres, roadwork, lighting, overhead signs and the primary power ductbank for a brand new terminal.

Project Engineer | Selmon Expressway Western Extension Tampa, FL | \$235 Million

Abby managed the project controls including quantity and cost tracking, work planning, contract payment, subcontract and vendor administration, and project scheduling. Her role also included coordinating Engineering

Services During Construction (ESDC). KISC constructed a 1.9-mile elevated tollway extension connecting Lee Roy Selmon Expressway to Gandy Bridge. The new elevated roadway includes a precast segmental concrete box girder viaduct with an extradosed post-tensioned fin constructed using top-down methods.

Lead Estimator | Manatee County CMARs | Manatee County, FL | \$90 Million

KISC's team has prepared GMPs for two different projects with Manatee County, Florida. KISC has one executed GMP agreement, and multiple in the works with the County.

Lead Estimator | C-23/C-24 Stormwater Treatment Area Fort Pierce, FL | \$143 Million

Abby's primary role throughout the project was to deliver cost effectiveness reports, analyze risk factors, and implement resources to prevent potential issues. The project scope includes the construction of 1,970 acres of multi-cell STA spread among five cells within the approximately 10-mile perimeter: two cells of roughly 200 acres each, and three cells of roughly 500 acres each. Project construction consist of approximately 10 miles of levee embankment and seepage canal excavation, along with nearly 2 million cubic yards of random fill in the cell interiors. There are 14 cast-in-place concrete gated structures that will control the flow of water through the five cells.



Mark Thomas

Drainage/Roadway Superintendent

Mark has 30 years of experience in drainage, grading, roadway and utility construction, with the bulk of it occurring in Central and South Florida. Mark has run similar construction urban stormwater operations to those in these neighborhoods at the Selmon Expressway Western Extension project, which will guarantee smooth start-up and low impacts to the community.

Tenure in the Industry:

30 Years

Tenure at Kiewit:

18 Years

Education:

High School Graduate

Why Mark?

- ✓ **Alternative Delivery Experience**
- ✓ **South Florida Construction Experience**
- ✓ **Experience working in residential environments**
- ✓ **Experienced in managing drainage systems and mitigating flooding risks**

RECENT SIMILAR PROJECTS

Superintendent | C-23/C-24 Stormwater Treatment Area Fort Pierce, FL | \$143 Million

Mark has maintained the project schedule and provided oversight for project controls and safety compliance. The project scope includes the construction of 1,970 acres of multi-cell STA spread among five cells within the approximately 10-mile perimeter: two cells of roughly 200 acres each, and three cells of roughly 500 acres each. Project construction consists of approximately 10 miles of levee embankment and seepage canal excavation, along with nearly 2 million CY of random fill in the cell interiors. There are 14 cast-in-place concrete gated structures that will control the flow of water through the five cells.

Superintendent | Selmon Expressway Western Extension Tampa, FL | \$235 Million

Mark's responsibilities included the daily planning and coordination of labor, equipment, and subcontractor resources. KISC constructed a 1.9-mile elevated tollway extension connecting Lee Roy Selmon Expressway to Gandy Bridge. A key element of the roadway and drainage design was to minimize the limits of construction to preserve as much of the existing vegetation as possible. Temporary drainage was provided during construction to efficiently manage site runoff during intense summer thunderstorms.

Superintendent | Prospect Lake Clean Water Center | Fort Lauderdale, FL | \$585 Million

Mark's responsibilities included supervising foremen and hourly workers, ensuring safe and cost-effective work methods, optimizing and coordinating labor and equipment needs, creating weekly foremen and equipment forecasting schedules, attending weekly meetings, and identifying and following through on changing conditions. The Prospect Lake Clean Water Center will replace an existing 70-year-old plant and deliver the majority of all fresh water for the City of Fort Lauderdale. The first-of-its-kind facility will feature the latest technology in nanofiltration and ion exchange water treatment, producing 50 MGD. The facility is designed to add future PFAS treatment technologies and withstand Category 5 hurricane winds.

Superintendent | Railroad Avenue Extension | Hanahan, SC | \$15 Million

Mark's primary responsibilities on this SCDOT roadway and bridge project included the installation of piping systems, while also coordinating tight schedules and keeping the project on budget. Work included 25 miles of sanitary sewer rehabilitation using cured-in-place liners, trenchless sanitary sewer installation under CSX rail tracks, and 19 miles of water main installation.

Superintendent | Loveland Grand Master Lift Station | Punta Gorda, FL | \$22 Million

Mark's responsibilities included overseeing the pipe installation, managing subcontractors, performing community outreach and public relations, and communicating with the client. This project involved major pump station and gravity sewer improvements for Charlotte County. The new lift station contains 6 new submersible, non-clog 115HP pumps with VFDs and a SCADA system. The project consisted of 16,510 LF of 24-in. to 48-in. PVC piping. Most of the piping was installed utilizing open cut trenches, which exceeded 17-ft. depth, and up to 32 ft. deep. Phased MOT was required throughout the project due to the new sewer lines running within the ROW of a highway.



Luke Gutshall

Utility Superintendent

With over 20 years of experience, starting as a laborer and progressing through roles such as Pipe Layer, Foreman, and Superintendent, Luke brings a wealth of knowledge and expertise to any project. His extensive background in both planning and executing complex operations, combined with his ability to manage crews effectively, ensures high-quality and cost-effective project outcomes. Luke's attention to detail, commitment to maintaining schedules, and dedication to team safety make him an invaluable asset to any project team. He understands the critical importance of fostering strong contractor-owner relationships, which significantly contributes to overall project success.

Tenure in the Industry:

22 Years

Tenure at Kiewit:

4 Years

Education:

High School Graduate

Why Luke?

- ✓ **Excels in both planning and executing complex operations, ensuring projects are high-quality and cost-effective**
- ✓ **Meticulous attention to detail, commitment to schedules, and focus on safety**
- ✓ **Ability to build strong contractor-owner relationships that drives project success**
- ✓ **Experience working in neighborhood environments**

RECENT SIMILAR PROJECTS

Superintendent | Tampa PIPES | Tampa, FL | \$91 Million



Luke leads crews in open cut, pipe bursting methods, and HDD methods to install pipe. Work includes 24.4 miles of sanitary sewer rehabilitation using cured-in-place liners, trenchless sanitary sewer installation under CSX rail tracks, and 18 miles of water main installation.

Superintendent | Largo WWRF Biological Treatment Improvements | Clearwater, FL | \$58.6 Million

Luke was responsible for overseeing the planning, coordination, and execution of construction operations. He ensured quality and safety standards were met, managed schedules, and tracked costs to guarantee the project was completed on time and within budget. Additionally, Luke managed much of the equipment. The project scope included upgrades and improvements to secondary treatment process; primary and secondary clarifiers; blower and air header; denitrification filters; isolation and weir gates; instrumentation and controls; electrical; an additional mixer; recirculation piping; combined return activated sludge/waste-activated sludge pump station; filter feed pump station; tertiary disc filters and a methanol injection system.

Foreman | Selmon Expressway Western Extension | Tampa, FL | \$235 Million

Luke was responsible for the coordination of multiple crews and daily construction activities. He planned work schedules and ensured conflicts didn't occur between multiple operations. KISC constructed a 1.9-mile elevated tollway extension connecting Lee Roy Selmon Expressway to Gandy Bridge. The project included 14 utility providers. Kiewit developed a utility relocation matrix to inventory all known existing utilities within the project limits and to identify conflicts within the new construction, as well as the most efficient mitigation measures..

Foreman | Loveland Grand Master Lift Station | Punta Gorda, FL | \$22 Million

Luke's primary role was to execute different ordinances of construction. This project involved major pump station and gravity sewer improvements for Charlotte County. The new lift station contains 6 new submersible, non-clog 115HP pumps with VFDs and a SCADA system. The project consisted of 16,510 LF of 24-in. to 48-in. PVC piping. Most of the piping was installed utilizing open cut trenches, which exceeded 17-ft. depth, and up to 32 ft. deep. Phased MOT was required throughout the project due to the new sewer lines running within the ROW of a highway.

Foreman | Groometown Road Booster Station | Greensboro, NC | \$2.7 Million

Luke was responsible for the coordination of multiple crews and daily construction activities. The scope of work involved adding a Water Booster Station to the existing Road Water Tank. This included installing two new 2.5 MGD centrifugal axial split case pumps, new valving and piping, a chlorine room with a tablet feed chlorinator, a jib crane, a suspended ceiling, an HVAC system, control and power panels, and a 150 KW diesel stand-by generator, along with new power service. Additionally, fencing installation and sitework were completed.



Alberto Baca Outfall/Structures Superintendent

Alberto brings over two decades of hands-on experience in outfall construction, drainage systems, road rehabilitation, and culvert installation, making him a meticulous and highly effective leader in heavy civil construction. He began his career with Kiewit in 2002 as a craft worker, gaining invaluable field expertise before transitioning into a full-time role in

2007. Alberto has played a key role in complex alternative delivery projects in southern Florida, including the Bear Cut Bridge Rehabilitation in Miami, FL, where he managed structural and drainage improvements with precision. Known for his collaborative leadership style, he fosters strong working relationships with crews, clients, and stakeholders to ensure projects run efficiently, safely, and to the highest standards.

Tenure in the Industry:

24 Years

Tenure at Kiewit:

23 Years

Education:

High School Graduate

Why Alberto?

- ✓ **Proven leader in outfall and drainage construction**
- ✓ **Expert in seawall rehabilitation and marine structures**
- ✓ **Skilled in managing work in residential communities**
- ✓ **Strong track record of scheduling and cost control**

RECENT SIMILAR PROJECTS

Superintendent | NE 183 Seawall Repair | Aventura, FL | \$1.1 Million

As the Superintendent, Alberto oversaw the execution of task orders, managing both self-perform crews and subcontractors to ensure efficient and high-quality work. His responsibilities included scheduling construction activities, tracking costs, and coordinating closely with the project owner to maintain alignment on scope, progress, and expectations. He also played a key role in pricing task orders, developing schedules, and ensuring resource availability to keep the project on track. On this project, Alberto led the rehabilitation of a 1,500 LF seawall along the Intracoastal Waterway in Aventura, FL, requiring extensive spall repairs and the installation of pile jackets to combat the corrosive effects of saltwater exposure.

Superintendent | Bakers Haulover | Miami Beach, FL | \$9 Million

Alberto was responsible for managing all personnel on site, change orders, and submittals. He was also responsible for working with the owner in scheduling. This project involves rebuilding the seawalls underneath both sides of the bridge. Other operations included concrete spall repairs and repainting the bridge. The project was awarded the FTBA Best in Construction award.

Superintendent | Camino Real Bridge Reconstruction Palm Beach, FL | \$10.6 Million

Alberto was responsible for managing craft labor and assured concrete pour activities were executed in accordance with design specification, while meeting productivity objectives related to the placement of asphalt and concrete quantities. This project included structural, roadway, pedestrian, and aesthetic improvements to the historical Camino Real/Boca Club Bridge. The bridge's new foundations were placed on 18-in. auger cast pile. The project is located on the ICWW and required coordination with the U.S. Coast Guard.

Foreman | Bear Cut and West Bridges Rehabilitation | Miami Dade County, FL | \$30.9 Million

This design-build project involved removing, rehabilitating, and replacing sections of the Bear Cut and West bridges on Rickenbacker Causeway in Miami. The scope also involved widening Bear Cut Bridge to provide dedicated paths for pedestrians and bicyclists on each side, as well as roadway and drainage improvements, signing, and pavement markings, lighting, utility services and the installation of a 16-in. water main pipeline. The new superstructures at both bridges were built using precast/pre-stressed concrete.

Superintendent | SE 5th Avenue Bridge Improvements | Pompano Beach, FL | \$2.8 Million

As Superintendent, Alberto was responsible for overseeing bridge demolition, precast concrete pile installation, slope protection, drainage, and bridge construction, ensuring that all work was executed safely, efficiently, and to the highest quality standards. He managed craft labor and subcontractors, coordinated scheduling and resource allocation, and worked closely with the project team to maintain progress and resolve field challenges. This \$2.8 million CMAR project involved replacing a deficient 120-ft. bridge that serves as a critical connection in Pompano Beach.



Ryan Conner Sanitary Superintendent

With more than 20 years of industry experience, Ryan has built a career from journeyman carpenter to project manager, demonstrating strong leadership in sanitary infrastructure and water supply projects. A Florida native, he has successfully delivered nearly a dozen projects across the region, bringing deep expertise in piling construction, underground utilities, and large-scale water infrastructure. As a Superintendent, Ryan excels in crew leadership, resource management, and project scheduling, ensuring work is completed safely, efficiently, and to the highest standards. His attention to detail, proactive problem-solving, and positive leadership make him a trusted expert in delivering complex sanitary infrastructure with precision and reliability.

Tenure in the Industry:

21 Years

Tenure at Kiewit:

21 Years

Education:

A.S., Bellevue College

FL Certified General Contractor License

Why Ryan?

- ✓ **Expert in managing utilities from start to completion**
- ✓ **Ensures seamless coordination of subcontractors and suppliers**
- ✓ **Maintains quality, safety, and accurate project documentation**
- ✓ **Strong track record of scheduling and cost control**

RECENT SIMILAR PROJECTS

Superintendent | Tampa PIPES | Tampa, FL | \$91 Million

Ryan's role consisted of overseeing the installation of new water pipe, line existing sanitary utilities and installation of the storm water system. He supervised a self-perform crew handling concrete work and below the surface corrections, while managing subcontractors performing restoration activities throughout the project. The City of Tampa experienced multiple water main and sanitary main breaks annually and spent upwards of \$30 million per year in emergency repairs. Their goal is to replace 20 miles of water lines and 30 miles of sanitary mains each year. The City's vision for this project is a comprehensive approach of correcting flooding, adding sidewalks, other transportation improvements, while completing an overhaul of their underground facilities in four neighborhoods (East Tampa, Virginia Park, Macfarlane Park, Forest Hills).

Superintendent | CFRC Capital Improvements | Clermont, FL | \$30.6 Million

Ryan was responsible for overseeing and managing all aspects of utilities from start to finish. This project consists of infrastructure improvements along a 64.2-mile rail corridor owned by FDOT in central Florida, including new drainage and stormwater features, ditching, underdrains, culvert extensions, rail corridor widening (sub-ballast), interchanges, landscaping, and regrading.

Superintendent | Orlando South Terminal C - Landside CMAR Orlando, FL | \$1.4 Billion

Ryan's responsibilities includes planning and coordinating the production efforts of his crews and to motivate them to accomplish the work safely, correctly, and efficiently. He is a liaison between field engineers and craft employees to ensure the construction complied with drawings and specifications. GOAA selected KISC to perform this critical contract which

included constructing the new STC airfield drainage and paving. Previously, the site was a field adjacent to active runways and taxiways lacking a modern underground drainage system. The area is flat and does not drain into a nearby body of water. The airfield operation has been split into dozens of phases for ease of construction and in part to ensure that surface water is always handled on-site in temporary basins or managed using well point and sock drain dewatering systems relocated as operations progress.



Sandra Antonio

Public Information Officer



Media Relations Group, LLC

Sandra Antonio is a seasoned Emmy Award-winning Investigative Producer with over three decades of experience in various facets of communication, public engagement, government agencies and leading television news media across South Florida. Prior to her tenure at Media Relations Group, LLC (MRG), Sandra served as the Public

Information Officer for the Miami-Dade Department of Transportation and Public Works (DTPW). In this role, she interfaced extensively with elected officials, media organizations, department heads and key stakeholders. She was instrumental as a primary information conduit for departmental projects, fostering community outreach, and actively participating in public meetings and/or information sessions.

RECENT SIMILAR PROJECTS

Tenure in the Industry:

30 Years

Tenure at MRG:

4 Years

Education:

B.A. Sports Management/Business,
Barry University, 2009

Why Sandra?

- ✓ **Extensive experience in public information and public relations focusing on engaging the public in Transportation and Public Works projects**
- ✓ **Substantial expertise in coordinating news reporting and media efforts across various construction projects**

Public Information Officer | Miami-Dade County Department of Transportation and Public Works Vision Zero Initiative | Miami-Dade County, FL

Sandra leads the public engagement efforts for this critical, lifesaving program. Her role includes coordinating outreach events, conducting surveys to gather community input, and fostering direct engagement with residents to raise awareness and encourage participation. By working closely with local communities, stakeholders, and advocacy groups, Sandra ensures that Vision Zero's message reaches diverse audiences, driving forward the county's goal of safer, more equitable transportation for all. She also communicated regularly with residents and school staff to keep them informed about the progress of the "Safe Routes to Schools" construction projects. In addition, Sandra produces a monthly recap summary highlighting the outreach events from the past month. The Miami-Dade County Department of Transportation and Public Works has pledged to eliminate traffic fatalities and serious injuries across the county's transportation network by 2040 through its Vision Zero Program. This ambitious initiative focuses on creating safer streets, improving infrastructure, and fostering a culture of road safety.

Public Information Officer | New Dadeland South Intermodal Station Project | Miami-Dade County, FL

Sandra serves as the lead Public Information Officer (PIO) for the project, overseeing public involvement and ensuring riders stay informed about ongoing changes and construction updates at the station throughout the

project's duration. The New Dadeland South Intermodal Station will deliver significant infrastructure upgrades to the heavily used Dadeland South Metrorail station.

Public Information Officer | Little River Canal Bridge along North Miami Avenue | Miami-Dade County, FL

Sandra was the lead Public Information Officer (PIO) on this project keeping the Village of Palmetto Bay and surrounding area residents aware of ongoing changes and construction updates throughout the project's duration. The project involves constructing roundabouts, adding sidewalks, curbs and gutters, drainage systems and upgrading roadway lighting to enhance safety and efficiency.

Public Information Officer | Old Cutler Road Traffic Circles | Miami-Dade County, FL

Sandra served as the lead Public Information Officer (PIO) for the project, ensuring that residents, as well as officials from the Village of El Portal and Miami-Dade County District 3, were kept informed with regular construction updates throughout the project's duration. The new concrete bridge fully replaced the existing structure, featuring 6-ft.-wide raised sidewalks on both sides while maintaining two lanes of traffic in each direction. Additional improvements included milling and resurfacing the approach roadways, upgrading drainage and lighting systems, installing pedestrian ramps at both ends of the bridge, dredging the Little River Canal and relocating an 8-inch water main.



Vanessa Salinas

Visual Communications Outreach Specialist



Media Relations Group, LLC

Vanessa is a bilingual graphic designer with over five years of experience creating community-focused outreach materials that engage and inform local residents. She specializes in designing accessible, visually compelling content that helps bridge communication between project teams and the communities they serve. With a strong background in collaborative environments, she works closely with outreach specialists to develop flyers, presentations, infographics, and other materials that clearly convey project updates and key information. Her expertise ensures that neighborhood stakeholders receive clear, effective messaging tailored to their needs.

RECENT SIMILAR PROJECTS

Tenure in the Industry:

5 Years

Tenure at MRG:

5 Years

Education:

B.F.A, Florida International University, 2020

Why Vanessa?

- ✓ **Expert in creating clear, effective MOT graphics for communities**
- ✓ **Skilled in designing outreach materials that clearly communicate project progress**
- ✓ **Strong local experience with a deep understanding of community communication needs**

Visual Communications | SR A1A from south of East Linton Boulevard to north of SR 806/East Atlantic Avenue | Palm Beach County, FL

Vanessa provided high-level graphic services for this roadway and safety improvement project in Delray Beach including creating a project location map, informational public meeting flyer and all public meeting materials (print and digital).

Visual Communications | State Road (SR) 804/Boynton Beach Boulevard from N Congress Avenue to NW 8th Street | Palm Beach County, FL

This project is to extend the service life of the pavement by milling and resurfacing, and to enhance roadway safety along SR 804/Boynton Beach Boulevard located in Palm Beach County. Vanessa designed the name tags, comment box inserts, sign-in sheets, and comment forms for a public meeting held in October 2023. She also ran a hybrid/virtual public meeting on the backend and provided post meeting analytics, recording and editing.

Visual Communications | State Road (SR) 7/US 441 from north of Glades Road to north of Bridgebrook Drive | Palm Beach County, FL

This design project involved repaving the existing roadway and shoulders, improving traffic and pedestrian signals and replacing signing and pavement markings along a 3.4-mile corridor. Vanessa was responsible for creating all graphics associated with this project including the public meeting flyer, public meeting materials (print and digital) and the project location map.

Visual Communications | SR 842/Broward Boulevard Resurfacing, Restoration and Rehabilitation | Broward County, FL

Vanessa was responsible for the design and preparation of informational materials for the Broward Boulevard Resurfacing, Restoration and Rehabilitation public meetings. Materials included project area maps, newspaper advertisements, social meeting notices, name tags, comment box inserts, sign-in sheets, and comment forms.

Visual Communications | I-395/SR 836/I-95 Design-Build Project | Miami-Dade County, FL

This approximately \$556 million design-build project involves the reconstruction of the Interstate 395 (I-395) corridor from west of the I-95/Midtown Interchange (I-95/State Road 836/I-395) to US 41/MacArthur Causeway Bridge, in Miami-Dade County. Vanessa is responsible for weekly lane closure alerts, managing weekly project eblasts and creating easy-to-read maps for public use from very complex detour plans (TCP).



Paulette Summers

Senior Community Outreach Specialist



Media Relations Group, LLC

Paulette has 26 years of experience specializing in public involvement, facilitation services, public relations, media relations, social media and marketing countless projects, overseeing public involvement plans and community awareness campaigns for numerous agencies throughout Florida. Over the past 13 years with Media Relations Group, LLC (MRG), she has demonstrated strong public involvement, problem-solving, organizational and graphic design skills, coupled with her innate ability to build positive relationships with colleagues, clients, media, partners, community members and stakeholders throughout Broward and Palm Beach counties.

Tenure in the Industry:

26 Years

Tenure at MRG:

13 Years

Education:

B.S. International Business, Florida International University, 2003

Why Paulette?

- ✓ **Extensive public relations and marketing experience, including media outreach throughout numerous South Florida municipalities**
- ✓ **Strong public involvement, problem-solving, and organizational skills**
- ✓ **Ability to build positive strategic alliances through business partnerships and community members**

successful hybrid (virtual and in-person) public meeting held in October 2023. This project is to extend the service life of the pavement by milling and resurfacing, and to enhance roadway safety along SR 804/Boynton Beach Boulevard located in Palm Beach County.

Lead Community Outreach Specialist | State Road (SR) 804/Boynton Beach Boulevard from US 441 to Lyons Road | Palm Beach County, FL

Paulette led all public involvement efforts for this design contract, part of the 2023 Continuing Services Contract (CSC). She managed a wide range of public involvement tasks, including completing a Community Awareness Plan (CAP), preparing the project flyer, compiling Elected and Appointed Official lists, and organizing design completion notification letters and meeting materials for a public meeting.

RECENT SIMILAR PROJECTS

Lead Community Outreach Specialist | SR A1A from south of East Linton Boulevard to north of SR 806/East Atlantic Avenue | Palm Beach County, FL

As the lead Community Outreach Specialist, Paulette led all public outreach efforts for this roadway and safety improvement project in Delray Beach. Her responsibilities included planning and coordinating a public meeting held in 2023, preparing all meeting materials, a notification flyer, and a FAR ad. She also supervised junior staff in the printing and production of meeting materials.

Lead Community Outreach Specialist | State Road (SR) 7/US 441 from north of Glades Road to north of Bridgebrook Drive | Palm Beach County, FL

Paulette was responsible for leading all public involvement efforts which included completing a Community Awareness Plan (CAP), coordinating rehearsals with project staff, conducting key stakeholder research and preparing official lists, as well as a notification flyer in preparation for a public meeting. This design project involved repaving the existing roadway and shoulders, improving traffic and pedestrian signals and replacing signing and pavement markings along a 3.4-mile corridor.

Lead Community Outreach Specialist | State Road (SR) 804/Boynton Beach Boulevard from N Congress Avenue to NW 8th Street | Palm Beach County, FL

As the lead Community Outreach Specialist, Paulette was responsible for all the public outreach efforts on this design job which fell under the Continuing Services Contract (CSC). Her responsibilities included completing a Community Awareness Plan (CAP) and the planning and coordination of

6.D. Team Commitment

KISC currently has nine active projects managed through our Ft. Lauderdale office, with six scheduled for completion within the year. As these projects wrap up, key personnel and resources will be available to transition seamlessly to support the Tropic Isle Improvements project. **The majority of key personnel proposed in our organizational chart are currently supporting the Tampa PIPES Project, which is completing on time in May 2025. As such, these key personnel will be fully available and ready to transition seamlessly to the Tropic Isle Neighborhood Improvements project. These individuals—highly experienced in similar drainage, utility, and roadway work—will bring their firsthand expertise directly from a successful, large-scale effort to the City of Delray Beach. When work begins on the Northwest Neighborhood and Tropic Isle Neighborhood Improvements projects, the City will have a dedicated, fully committed team ready to deliver results from day one. We welcome the opportunity to demonstrate how utilizing the same Project Management Team for both projects will provide significant cost and schedule savings for the City.** KISC is fully committed to providing the personnel, equipment, and expertise required to deliver this Project efficiently and effectively.

Project Name	Location	Client	Start	Finish	% Complete	Value
BBCW Contract 5C	Homestead, FL	U.S. Army Corps of Engineers Jacksonville District	10/15/2021	07/01/2025	95%	\$26,989,594
Prospect Lake Clean Water Center	Fort Lauderdale, FL	Prospect Lake Water, L.P.	03/14/2023	09/10/2026	52%	\$585,398,230
S-2, S-3, S-4, S-7 and S-8 Monitoring Panels Project	South Bay, FL	South Florida Water Management District	06/01/2022	05/07/2026	70%	\$17,147,565
Biscayne Bay Coastal Wetlands Cutler Flow Way Phase 1 Pump Station S-701	Cutler Bay, FL	South Florida Water Management District	10/05/2022	11/11/2025	79%	\$36,082,178
CMAR Group #1, Project 4 Construction	Green Cove Springs, FL	Clay County BOCC	12/15/2022	05/25/2025	62%	\$44,415,525
Tampa Progressive Infrastructure Planning to Ensure Sustainability (PIPES) I Full Construction	Tampa, FL	City of Tampa, Florida	02/18/2023	05/25/2025	81%	\$85,620,566
CEPP S-620 and L-6 Improvements	Clewiston, FL	South Florida Water Management District	02/20/2023	09/06/2025	94%	\$14,054,062
Naples Pump Stations and Outfall Removal	Naples, FL	City of Naples	09/02/2024	12/04/2026	6%	\$85,474,216
South Port Turning Notch Crane Rail Improvements Construction	Ft. Lauderdale, FL	Broward County, FL	05/30/2017	06/30/2025	98%	\$480,031,588
Lena Road Improvements	Tampa, FL	Manatee County, FL	07/08/2024	07/30/2025	15%	\$3,800,000
60th Avenue	Tampa, FL	Manatee County, FL	05/17/2025	10/27/2025	38%	\$900,000

TAB 7 - 4.2.7. METHODOLOGY AND WORK PLAN

4.2.7. METHODOLOGY AND WORK PLAN

7.A. Our Team's Approach and Methodology for Engaging with the City

Kiewit Infrastructure South Co. (KISC) is committed to a collaborative and transparent CMAR process and overall Project, working closely with City representatives to ensure clear, timely, and effective communication. With experience from **over 620 CMAR projects totaling more than \$30.8 billion, along with 31 Marvin M. Black Partnering Awards from the Associated General Contractors of America**, our team has a proven track record in fostering successful partnerships.

Pre-Construction/Design Phase Collaboration

During the Pre-Construction Phase, Project Manager Joe Pescatore will serve as the single point of contact for the City. Joe will lead the KISC team to work with the City to develop and implement a Project Communications Plan that ensures effective information delivery, keeps the entire Project team informed, and resolves issues quickly. The Plan will maintain a consistent meeting schedule using project management tools for real-time, detailed information sharing. Joe will also be responsible for leading weekly progress and task force meetings with structured agendas and clearly defined action items. While face-to-face collaboration is our recommendation, we will adopt whatever method the City prefers (such as virtual meetings). Regular engagement at City offices will be maintained to ensure accessibility. KISC will leverage advanced project management tools for real-time collaboration, progress tracking, and documentation, ensuring action items are assigned, tracked, and addressed efficiently.

KISC understands that active participation during design development is critical to the CMAR role. Our team will be fully engaged in structured constructability reviews, value engineering workshops, and ongoing design discussions. Whether working on-site with Kimley-Horn or engaging remotely, we will ensure that all design feedback is timely, practical, and cost-conscious. We will establish and maintain an integrated Design, Pre-Construction, and Construction Schedule, providing detailed sequencing of key activities and responsibilities. By aligning cost, constructability, and scheduling input throughout design development, we will help maximize efficiency, mitigate risk, and reduce overall Project costs.

We employ a results-oriented, formal partnering approach (**Exhibit 100**), which has enabled KISC to execute and complete projects on time and on budget throughout our company's history. Partnering begins with a formal Partnering Kick-off Workshop, which will bring together KISC, the City, the Owner's Representative and Kimley-Horn to:

- Review work completed prior to this RFQ
- Identify potential Project challenges and collaborative solutions
- Establish clear communication lines and review protocols
- Align Project goals, schedules, and key milestones

This session will strengthen trust among all Project stakeholders, ensuring a cohesive, solution-driven approach from the outset. KISC's approach is built on accountability, transparency, and proactive problem-solving. By integrating structured communication, real-time tracking, and a collaborative project management style, we will ensure the successful execution of the Tropic Isle Neighborhood Improvements Project while maintaining a strong partnership with the City of Delray Beach.

KISC will work with the City and Kimley-Horn to develop a decision-making matrix where opportunities to repair sanitary lines and/or replace watermains via trenchless methods eliminate excavation requirements on one or both sides of the roads. By analyzing each utility line individually along the streets, the Project team will be able to identify sides of streets and partial blocks that will not require trenches, allowing the team to optimize the schedule and accelerate final paving activities on specific streets.

Exhibit 100: KISC Partnering Approach



Construction Phase Communication

As the Project transitions into Construction, KISC will implement its proven processes for efficiency, quality, and schedule adherence. Our superintendents will oversee all field activities, ensuring that subcontractors follow KISC’s high standards while managing a rolling four-week schedule to keep stakeholders informed and operations on track.

One of our most valuable tools in fostering communication is our Four Square Matrix (**Exhibit 101**), a structured approach used across all Kiewit projects. This visual tool tracks project concerns, assigns action items, and monitors progress as challenges evolve into successes. Our clients find great value in implementing this tool to discuss, assign action, and address specific project concerns as they arise. The matrix will be updated in a team setting with input from all parties at least once per month, ensuring that key issues are identified, addressed, and resolved collaboratively.

KISC will also provide full access to our Collaborative Document Management System shown in **Exhibit 102** (powered by InEight), allowing real-time visibility into project documentation, schedules, and reporting. Weekly and monthly progress reports, along with executive check-ins with the City and KISC Regional Area Manager Cody Jensen, will ensure full transparency and alignment throughout the Project.

Exhibit 101: Kiewit’s Four Square Matrix

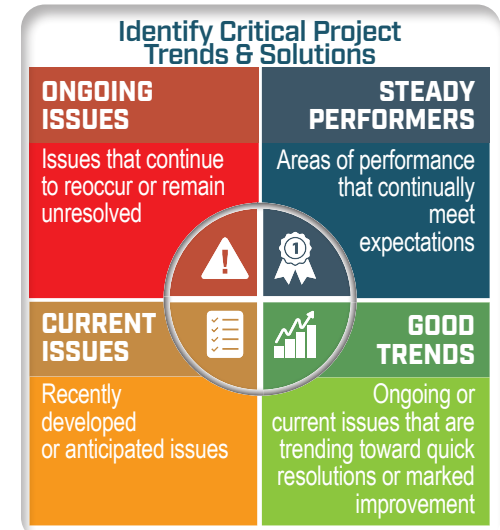
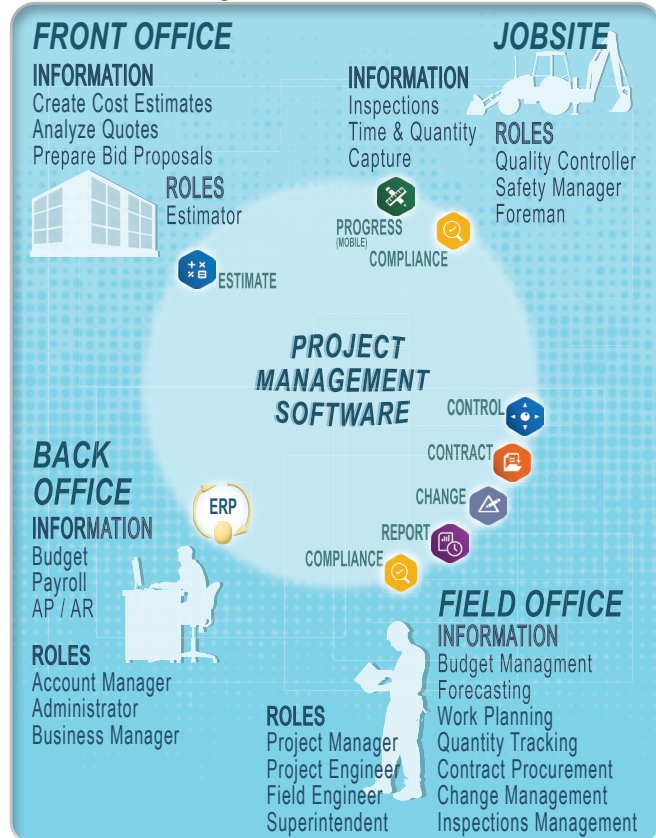


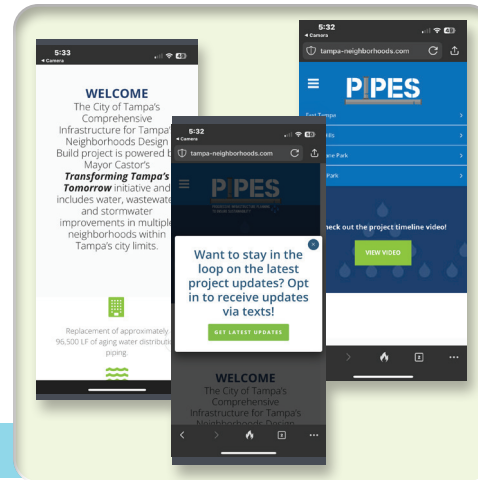
Exhibit 102: InEight Tools



- ESTIMATE:** Used for developing the estimate. This software sets the standard for accuracy, performance, and multiuser collaboration throughout the estimating process. This allows improved accuracy, integration, and complete project visibility of the estimate. **BENEFITS:** *KISC can create a top down, conceptual or detailed (bottoms-up) estimate including assemblies, crews, and production rates.*
- CONTROL:** Brings together critical budget, as-built and forecast data related to cost, manhours and productivity into one location. Control simplifies the process of managing budget versions by line item, tracking work progress and budget changes as well as calculating earned values. **BENEFITS:** *By consolidating all project metrics into one location, KISC can act swiftly to maintain cost and schedule performance.*
- COMPLIANCE:** Streamlines reporting and tracking on the project. KISC employees can submit checklists, safety reviews, incidents, environmental reviews, and task information from the field. **BENEFITS:** *Provides robust data collection for project safety, environmental, quality control, and compliance activities.*
- PROGRESS:** Progress helps foreman and superintendents record actual data such as labor and equipment hours, quantities, and daily field notes in real time. Results are synced automatically updating project status and into the other InEight systems. **BENEFITS:** *Project management can review productivity and leverage the information to better plan and manage remaining activities, as well as to provide the City with accurate progress reports.*
- CHANGE:** Serves as a repository for project issues, allowing items to be evaluated over time—either dismissed as having no impact or connected to relevant City or Contractor funding sources. This system also supports transparency in tracking possible subcontractor back charges. **BENEFITS:** *A centralized issue log where the Project team can track and manage issues.*
- REPORT:** Our reporting includes detailed project status metrics relevant to the type and stage of the project, plus documentation of any issues that may pose risks to project performance. Standard reporting intervals are weekly or monthly. **BENEFITS:** *Keeps the City and KISC’s project management team informed about the Project’s health and trends by producing over 50 high-quality reports.*
- CONTRACT:** Used to create and manage contracts throughout a project, including scope, budget, payments, issues, change orders and subcontractor communication. **BENEFITS:** *Simplifies the workflow required to create and manage contracts, saving valuable time and money during project execution.*

Our Public Engagement Plan and Program

To ensure seamless communication and proactive engagement with the residents of the Tropic Isle neighborhood, we have teamed with a key subconsultant, **Media Relations Group Inc (MRG)**, to **lead all public and community outreach efforts for this Project**. Given the impacts to residents' driveways, landscaping within the right-of-way, and the challenge of maintaining continuous access, effective communication will be critical in keeping the community informed and minimizing disruptions. Tropic Isle is a beautiful waterfront residential neighborhood with a strong sense of community, where clear, transparent weekly updates and thoughtful coordination will be essential to maintaining trust. MRG specializes in neighborhood-focused outreach strategies, ensuring that residents receive timely notifications, clear expectations, and responsive support throughout construction. The public engagement plan and program will include:



Step 1: Development of a Communications Plan

A robust communications plan will keep stakeholders, elected officials, media, and the public informed, manage expectations, and highlight Project progress. This will include:

- Regular Updates – Provide consistent updates on milestones, challenges, and successes, including daily public updates on construction plans and locations of work
- Interactive Platforms – Leverage traditional and digital communication channels, such as newsletters, weekly eblasts, social media, and a regularly updated Project website to foster engagement – with prior approval by the City designee

Step 2: Stakeholder Engagement and Collaboration

KISC recognizes the critical importance of keeping residents informed, satisfied, and ultimately enthusiastic supporters of the Project. Our team commits to ensuring residents have continual access to their homes throughout construction. We are committed to delivering on our promises, ensuring timely and thorough restoration of front yards, driveways, and other impacted areas. MRG and KISC will establish a positive presence and visibility in the neighborhood to encourage stakeholders to reach out directly to Project hotlines with questions or concerns, reducing calls and emails to City Departments and government officials. To foster collaboration and garner support, we will systematically identify and engage key stakeholders, including local government officials/agencies, community representatives, homeowner associations, schools, property and business owners, and media contacts. This process involves:



- Needs Assessment – Understanding each stakeholder group's concerns, expectations, and specific needs through thorough assessments
- Customized Engagement Plans – Tailoring strategies for each stakeholder group, utilizing face-to-face meetings, workshops, and collaborative sessions with KISC
- Continuous Feedback Loop – Establishing ongoing mechanisms for collecting and addressing stakeholder feedback, ensuring alignment with Project objectives

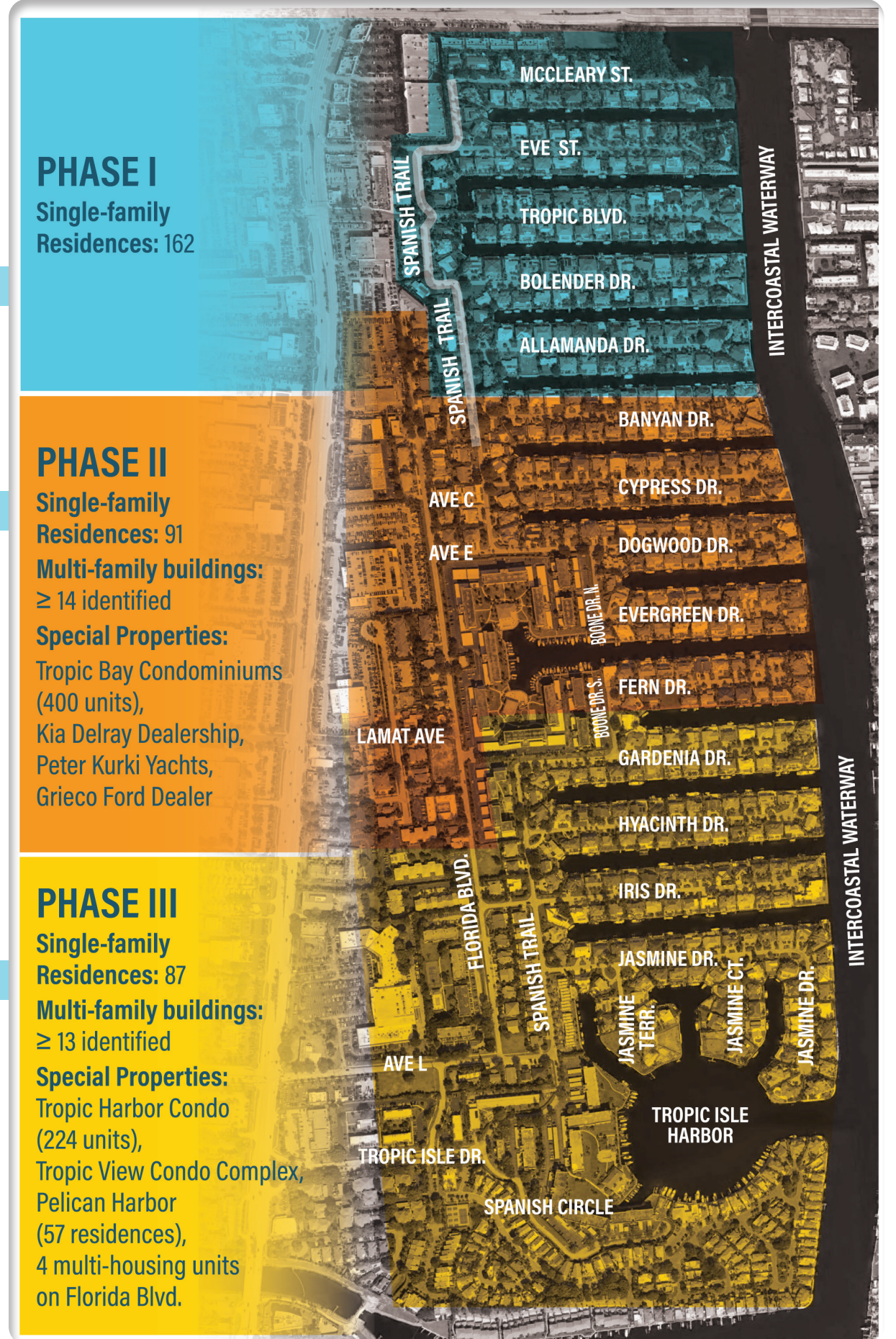
Step 3: Public Input and Feedback

For gathering critical public input, we use:

- Digital Platforms – Leveraging social media, email blasts, and websites to make it convenient for the public to provide feedback; we will take over updating and maintain the existing Project website
- Public Consultants – Hosting townhalls, open houses, community meetings, and distributing surveys to gather diverse perspectives and address concerns
- Transparent Reporting – Tracking public inquiries and resolutions while regularly sharing summaries of feedback to demonstrate responsiveness
- Public Outreach – Distributing mailers, door hangers, and information flyers while participating in community events; additionally, we will distribute our PIO's cell phone number and develop opt-in text alerts to keep the public informed 24/7 during construction



Exhibit 103 | KISC's Draft Overview of Tropic Isle Stakeholder Landscape by Project Construction Phase



7.B. CMAR Approach

KISC understands the complexities and demands of CMAR projects. Our team has extensive experience with CMAR delivery and has thoroughly reviewed the City of Delray Beach’s Tropic Isle Neighborhood Improvements Project to ensure alignment with its goals. **Exhibit 104** depicts the high-level process KISC will use to achieve your goals during both Pre-Construction and Construction Phases.

Exhibit 104 | KISC’s CMAR Process to Meet the City’s Goals



The success this CMAR process has brought to our similar projects is rooted in the rigorous Pre-Construction planning and strategic early-phase development. Establishing detailed construction schedules and phased execution plans upfront ensures a seamless transition into construction, minimizing delays and disruptions. Efforts spent during Pre-Construction allows the team to have an achievable plan to execute Construction of the Project. Equally essential is our proactive stakeholder engagement strategy, focusing on keeping residents, businesses, and all affected parties informed and involved from the outset. Through community meetings, direct outreach, and clear public education efforts, we set realistic expectations, foster trust, and build consensus, creating a foundation for collaborative Project success.

Proven Neighborhood Infrastructure Expertise for Tropic Isle

KISC has extensive experience developing GMPs for large-scale infrastructure improvements in residential communities in south Florida, making us uniquely qualified for the Tropic Isle Neighborhood Improvements Project. On the City of Tampa’s PIPES Program, our team successfully navigated complex drainage, utility, and roadway challenges in multiple neighborhoods, ensuring minimal disruption while addressing the City of Tampa’s long-term infrastructure goals.

Our team reviewed existing conditions, collaborated closely with local agencies and residents, and incorporated innovative solutions to maximize efficiency and minimize community impacts. We introduced advanced installation methods, including large bore HDD installation, CompressionFit HDPE pipe lining under active rail lines, expanded pipe bursting programs, and early works GMP development to accelerate project delivery—all of which demonstrate our ability to implement efficient, low-impact construction techniques that will be valuable in Tropic Isle’s constrained residential environment.

By leveraging this experience, we bring a deep understanding of neighborhood-focused infrastructure improvements, ensuring that the Tropic Isle Neighborhood receives a well-planned, minimally disruptive, and high-quality project that meets the City’s vision and timeline.

Safety Program and EMR

At Kiewit, safety is not just a priority—it is a core value that guides every aspect of our work. Our safety team develops a customized, site-specific safety plan for every project we build that is designed to address the unique safety risks while ensuring the well-being of our workforce and the public. **Kiewit maintains an industry-leading Experience Modification Rate (EMR) of 0.56, far below the industry average of 1.0, reflecting our proven track record of safety excellence and award-winning performance.**

Accidents occur most often when individuals lack awareness or fail to recognize unsafe behaviors. That’s why our safety approach goes beyond compliance, emphasizing education, hazard recognition, and a culture of personal responsibility, ensuring every worker understands not just the risks but the real-life consequences of unsafe actions. Our project teams—including all subcontractors—adhere to stringent safety protocols that exceed industry standards. Every team member follows site-specific procedures, participates in targeted training, and engages in ongoing on-site safety meetings to reinforce a proactive safety mindset.

On complete street construction projects, public safety is paramount—protecting pedestrians, cyclists, and motorists is just as critical as safeguarding our crews. Clear and clean maintenance of traffic, road-plated or barricaded excavations, smooth transitions on sidewalks, and an extreme housekeeping approach in our work areas are some of the extra steps that you will see from KISC; this attention to detail is uncommon from other contractors. KISC is committed to creating and maintaining a safe environment that fosters community trust and confidence by ensuring that safety is never compromised—for our workers and the public.

Environmental Management

To ensure adherence to the Environmental Management System, KISC conducts ongoing training in spill prevention and control, waste management, air quality, and process water management. Kiewit’s Environmental Department provides annual training and periodic webinars for Environmental Managers. Environmental Managers then train KISC employees on all areas of environmental compliance, including regulatory required training associated with permits and registrations.

Environmental Managers will ensure Project Environmental Coordinators are fully trained on company and City environmental standards before starting on a project. During Pre-Construction, KISC will assign a full-time Project Environmental Coordinator (PEC) for the life of the Project. The PEC will train project staff, craft, and subcontractor employees through new hire orientation and frequent environmental compliance toolbox meetings.

Our team understands the unique environmental challenges of working along the coast. KISC provides specialized training for our workers and implements targeted programs to prevent run-off and spills from reaching surrounding waterways. We conduct and document daily inspections of equipment and secondary containment systems to ensure spills are prevented before they start.

NO ACCIDENTS NO INJURIES

Think!
SAFETY
BEGINS WITH ME

The Tools We Use

Active Commitment by Management

Employee Engagement at ALL LEVELS

NO ACCIDENTS NO INJURIES NOBODY GETS HURT.

- 1> ENVIRONMENTAL RISK ASSESSMENT**
Used to determine the steps needed to ensure environmental compliance with laws and regulations, company policy and contract requirements.
- 2> PLAN THE PLAN**
Identifies who has responsibility and sets a timeline for developing the detailed plan for environmental compliance.
- 3> ENVIRONMENTAL COMPLIANCE PLAN**
Develops a list in detail of the environmental requirements, mitigation actions, and verification records for demonstrating compliance. Kiewit has a “zero tolerance” policy for noncompliance with policies and procedures.

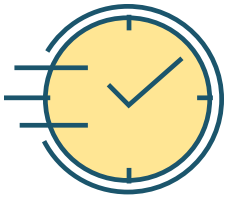
Quality Management

Our quality management approach is structured to ensure construction is performed at a high level of quality that promotes both sustainability and maintainability. To achieve this, the KISC team will develop and implement a Quality Management Plan (QMP), which will outline how we intend to meet the City's expected quality objectives. At the beginning of the Tropic Isle Neighborhood Improvements Project, we will hold a quality-focused training to ensure that the entire Project team, including subcontractors, understands the requirements of the QMP. Every operation will follow three steps – Work Plans, Pre- Activity Meetings, and Play-of-the-Day Meetings.



Work Plans: The construction team will review construction documents to develop a Work Plan, clearly defining our plan to build the work. Details from the construction documents, such as material type, testing frequency, hold points, and tolerances, will be referenced in the Work Plan to ensure full compliance with the construction documents.

Pre-Activity Meetings: Prior to the start of any operation, a Pre-Activity Meeting will be held with the City, the Owner's Representative, Kimley-Horn & Associates, Inc., inspectors, and construction supervisors for all new scope or operations throughout the life of the Project.



Play-of-the-Day Meetings: These meetings will be held to coordinate City staff, quality control staff, and the Project team to discuss construction activities to be completed each day.

The concept of doing the job “Right the First Time, Every Time” is instilled across the organization and reinforced by KISC through supervision and training. All Project personnel are trained to follow the core principles of implementing the QMP, such as:

- Establishing Quality Hold and Verification Points for each construction activity and identifying the right personnel for each step. For items that are classified as "high-risk", KISC requires the correct personnel are available and on-site to mitigate these risks. The highest risk activities will be reviewed by KISC's District Quality Manager.
- Performing tasks with a personal commitment to achieving the Project's quality requirements
- Empowering responsibility and accountability for verifying the quality of their work
- Ensuring that each crew member has the training, skills, and tools to produce quality work

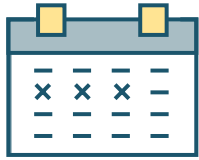
Self-Perform Plan

KISC has the ability, equipment, and experience to self-perform most major operations on the Tropic Isle Neighborhood Improvements Project. We typically self-perform operations that are schedule-critical or where we can provide cost savings. Having KISC self-perform critical operations allows a greater degree of schedule certainty, and more directly allows KISC and the City to control the critical path of the schedule. We evaluate our cost competitiveness with subcontractors in order to give clients the best value. Multiple competitive bids are received and evaluated by the client on the work we compete on to self-perform. KISC is committed to ensuring best value on every project when it comes to self-performing and subcontracting.

Subcontracting Plan

While KISC can self-perform a majority of this work, we recognize the importance of local subcontractor involvement. Below is an initial list of potential subcontractors we will solicit during the Pre-Construction process. KISC identifies subcontracting opportunities by scope, and further determines safety, quality, and compliance alignment in order to preserve the best value selection for the City.

Scope	Potential Subcontractors/Suppliers
Maintenance of Traffic	Bob's Barricades Inc., Acme Barricades, Arrive Alive Traffic Control, Safety Systems Barricades, Roadsafe Traffic Systems
Utilities/Roadway	Roadway Construction LLC, Gencon Partners, Inc., Ric-Man Construction Florida Inc., David Mancini & Sons Inc., Murphy Pipeline Contractors, Conti LLC, V&G Construction Solutions, Homestead Concrete and Drainage, Inc.
Dewatering	Mersino, Synergy, Phoenix, United Rental, Expert Dewatering & Construction, Ambient Technologies
Asphalt	General Asphalt Co., Community Asphalt Corporation, Weekly Asphalt Paving Inc., Rapid Milling & Paving, H&R Paving, Florida Blacktop Inc, Fischman Asphalt
Sanitary Lining	Gulf Coast Underground LLC, Shenandoah, Insituform Technologies, Inc., Pipeline Utilities, Inc.
Pipe Bursting	Gulf Coast Underground LLC, Murphy Pipeline Contractors
Lightweight Aggregate Supply	Aeoraggregates of North America LLC, Stalite Lightweight Aggregate, Cellucrete Corp
Landscaping/Pavers	VisualScape, Inc., Dias Brothers Landscape Services, LLC, Arazoza Bros. Corp., All Dade Pavers, Atlas Concrete & Pavement Inc., One Stop Pavers



Subcontractor Schedule: Subcontract scopes are integrated into the baseline schedule and closely monitored. Schedule requirements and milestone dates are included in the subcontract bid package and subcontract. We monitor subcontractor performance daily, developing action plans if an activity falls behind. Actions may include additional resources, extended hours, additional management involvement, supplemental crews, weekend work, or resequencing.

Subcontractor Cost Control: KISC leverages lessons learned to support subcontractors with common issues like financial challenges, scope and change order management, and safety and quality performance. Subcontractor scope and quotes will include the costs for the life of the Project, eliminating any concerns with escalation or scope gaps.



Subcontractor Quality: We proactively ensure high quality from our subcontractors. Subcontractor scopes are integral to the Project's QMP. Pre-activity meetings address quality hold points and inspection/testing procedures before operations begin. Our on-site quality control team inspects subcontractor work to ensure compliance with design and project specifications before permanent material placement. Quality expectations are communicated prior to selection and verified throughout construction.

Subcontractor Safety: Subcontractors receive the same safety training as KISC employees. A mandatory site safety and expectations orientation is provided to all employees before starting work. Project-specific hazards, including traffic and human equipment interface, are clearly communicated. Subcontractors must prepare hazard analyses for each operation, hold daily toolbox meetings, and attend monthly project-wide safety meetings.



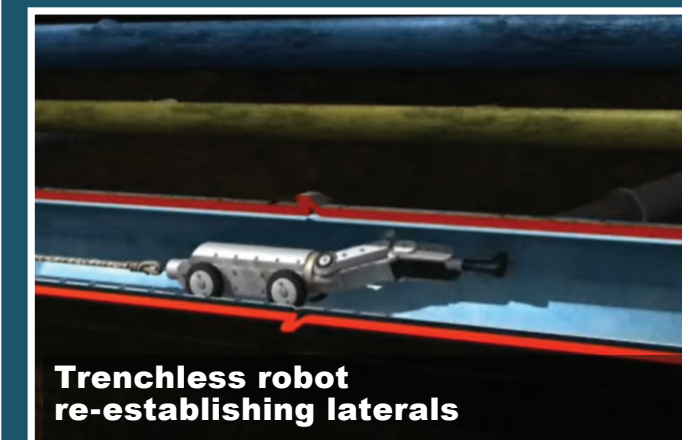
Subcontractor Compliance: Subcontractors are trained on compliance, including environmental, erosion control, and hazardous materials. They understand requirements for adequate erosion control and proper steps to mitigate potential impacts on environmentally sensitive elements. Subcontractors are held to the same high expectations as KISC employees for overall Project compliance.

Detailed Project Approach

KISC understands that the CMAR process is built on collaboration, particularly during the Pre-Construction Phase, and we will be flexible in our overall Project approach to align with the City’s vision. Drawing from our extensive experience, we confidently recommend two key strategies that will deliver clear benefits and minimize impacts to Tropic Isle. At the same time, we remain fully prepared to adapt our approach in collaboration with the City and stakeholders as the Project progresses. Within eight weeks of receiving our notice-to-proceed (NTP) for Pre-Construction (Phase 1), KISC will be able to provide an Early Works Guaranteed Maximum Price (GMP) proposal for the City’s consideration and approval. This proposal can be developed with the current design documents and supplemental GIS information that the City’s currently has. Using early works as an interim phase within the Project allows more cashflow flexibility and yearly spend available to maximize the City’s budget, and allows for adjustments if scope needs to shift to the second GMP.

Key 1: The Early Works GMP prioritizes early, high-value scopes of work that deliver visible progress with minimal residential disruption—building public trust while setting the stage for a seamless Full-Construction Phase. By focusing first on tasks that do not require long-lead permits, can be performed with minimal design, and can be completed ahead of schedule, we create an immediate impact: **cleaner, neater work zones, phased public outreach that keeps residents informed, and early completion of key elements that reinforce the City’s commitment to efficiency.** While the Early Works GMP encompasses scope throughout the entire Project, the work will be completed in the three-phased approach from north to south. The Early Works GMP would include scope items such as:

Early Work Scope	How the City will Benefit from an Early Works GMP
Public Outreach	Our team will visit each home on affected streets before work begins to personally introduce ourselves, provide Project contact information, explain the scope of work, outline the anticipated schedule, and listen to any questions or concerns. This not only builds trust and transparency but also fosters a strong relationship between the City and the community. Rather than being perceived as simply tearing up streets and disrupting daily life, we will demonstrate that we are true stewards of public service—bringing necessary improvements while respecting the people and neighborhoods affected, as well as being timely with responses to concerns.
Sanitary main and lateral CCTV & cleaning	Sets the stage for more complex rehabilitation work by providing accurate data on the system’s condition, leading to targeted repairs. These activities are minimally invasive, requiring smaller work areas, which reduces street disruptions and builds trust with homeowners through visible maintenance efforts.
Sanitary & storm point repairs	Once areas requiring repair are confirmed via CCTV, the Project team can develop an overall Early Works schedule that will allow a point repair team to stay ahead of the CIPP Lining Subcontractor to ensure the lining scope is completed as efficiently as possible.
Sanitary CIPP lining	Rehabilitates existing pipelines without the need for extensive excavation. This trenchless technology is minimally invasive, allowing for a smaller work area on the street, which is particularly beneficial to maintaining access on single entry/exit streets like those in Tropic Isle. Early completion of CIPP lining ensures that the sewer infrastructure is structurally sound, ensuring ensuing excavation and roadway work do not damage older clay mains.
Manhole Rehabilitation	Addressing manhole rehabilitation early prevents infiltration and inflow of stormwater into the sanitary sewer system, which is crucial with heavy rainfall events. Early rehabilitation is minimally invasive, often confined to the manhole area, requiring smaller work zones. This targeted approach not only extends the lifespan of the infrastructure but also sets the stage for more complex work by ensuring system integrity from the outset.
Outfall check valve installation	Prevents backflow of seawater into the stormwater system during high tides or storm surges. Performing this early, provides timely protection of the neighborhood and immediately implements additional resiliency strategies.
Third-party utility relocations	Achieves schedule efficiencies by ensuring that all utilities are appropriately positioned before major construction begins and by preventing potential conflicts and delays during later phases.
Waterline pipe bursting and/or HDD installation	Trenchless methods like pipe bursting for waterline installations ahead of full construction minimizes roadway surface disruptions and preserves existing landscapes. By strategically identifying watermain that are eligible for trenchless replacement during Early Works, the Full Construction schedule is able to be accelerated by eliminating scope on the critical path.
Landscape demolition	Major landscape removal could be performed early to confirm items are not in the way for the full construction activities. This eliminates potential conflicts and delays in preparation for Full Construction Activities.
Other enabling work for full construction	Early completion of enabling works sets the stage for more complex tasks, achieves schedule efficiencies, and demonstrates the City’s commitment to organized and efficient project management, thereby building public trust.



DETAILED PROJECT APPROACH

Key 2: Finish-As-You-Go

The second recommendation is to plan and complete the work like individual projects, with attention to detail and extreme housekeeping from start to finish. Prioritizing work in this way allows completion on a street by street basis, allows positive drainage during construction, as well as minimizes the impact to the residents. Completely finishing streets timely will provide uninterrupted routes for residents, pedestrians, bikes, and businesses, confirming smooth travel during construction. Our superintendents will personally walk the street before construction begins, meeting with each resident to explain the plan, schedule, and expected impacts. This proactive approach ensures clear communication and transparency, so there are no surprises throughout the project.

WATERLINE - WEST TO EAST

Step 1: West to East Water line

Completing the waterline from west to east will allow the new line to be properly tied into the City's system, enabling systematic clearances and ensuring that all residents maintain water service throughout construction. This also eliminates the need for a temporary bypass.



EVE STREET

STORMWATER - EAST TO WEST

Step 2: Storm Drains from Downstream Outfalls

Constructing the storm drain from the outfalls to flow upstream will provide positive temporary drainage and help minimize storm-related flooding in the neighborhood. To maintain residential access, temporary street plates or daily backfilling will be utilized for driveways. Any necessary dewatering will be managed by pumping into newly installed storm infrastructure or appropriate discharge points.



Step 3: Utilities Complete - Road Restoration

Once utility work is complete within a section, roadway restoration will begin immediately. This "finish-as-you-go" approach ensures that residents regain permanent access to their driveways as quickly as possible. The faster this work is completed, the sooner the community can return to a sense of normalcy.



Step 4: Driveway Restoration and Landscaping

Final driveway restoration will be completed, whether pavers or concrete. Landscaping and irrigation will be carefully reinstalled to restore each property's original appearance or a betterment, demonstrating a commitment to meticulous workmanship and respect for the community. As we return each element to its proper state, we strive to leave residents not just satisfied but genuinely appreciative of the care taken throughout the Project.



Proven Neighborhood Infrastructure Expertise for Tropic Isle

Implementing this phased early works process has proven highly successful, as demonstrated by our work on Tampa PIPES GMP-3 Early Works. The project included lining over 129,000 LF (24.4 miles) of sanitary mains (6-in. to 24-in.), rehabilitating over 535 sanitary manholes, and installing 65 new manholes. Operations required minimal Maintenance of Traffic setups confined to manhole locations. Based on our experience in Tampa dealing with very old infrastructure, we recommend lining all the old clay sanitary pipes prior to excavation. This reduces the risk of unintended breaks, minimizing disruptions to residents. KISC was able to evaluate CCTV Videos to identify locations of point repairs prior to CIPP Lining operations. The minimally disruptive and 1-day point repairs allowed the CIPP subcontractor to complete all work in one mobilization, eliminating additional costs and delays associated with relocating and remobilizing.

On GMP-3, KISC performed 38 point repairs based on mainline CCTV findings, ensuring successful segment lining. KISC was able to perform point repairs on 38 locations across the City, which allowed an additional 10,800 LF of existing sanitary mains to be CIPP Lined instead of replaced. This success stemmed from KISC's role as the Prime Contractor, managing specialty subcontractors. Our team controlled the entire process—from analyzing CCTV findings and performing point repairs to delivering post-lining CCTV records to the City of Tampa, including uploading records into their Granitenet database. This method is the most minimally invasive to the residents and provides an option to eliminate the need for direct bury or pipe bursting of existing sanitary mains.



Lining Sanitary Mains in City Easements



Installing New manholes at mainline terminations.

Operational Approach to Specific Scopes of the Project

Maintenance of Traffic

KISC will collaborate with the City and Kimley-Horn to design a Maintenance of Traffic (MOT) plan that minimizes public impact while ensuring safe construction operations. With the understanding that the majority of the Tropic Isle streets are dead-end streets and detours will not be an option, our team will develop a construction plan that leaves one lane open at all times to facilitate entry and exit of residents to and from their homes. Applying steps 1 and 2 (noted on the previous page) strategically addresses completing one side before the other, as the storm and water lines are on opposite sides of the streets. We will maintain rigorous housekeeping, provide clear directions, and complete the critical thoroughfares.

Advance public notices for lane closures and traffic restrictions will be communicated through various channels. Our dedicated Public Outreach team, led by MRG, will keep residents, transportation authorities, and businesses informed of upcoming lane closures. This proactive approach will maintain public confidence and support while efficiently achieving the Project's goals. In addition to our public outreach team, KISC will have crew members available to communicate and assist residents when they have questions or concerns regarding the lane closures on their streets.

Housekeeping & Laydown

KISC recognizes the importance of a clean worksite, both for efficient construction and due to the Project’s high visibility. Constant monitoring of the job site for trash and debris is essential to alleviate resident concerns and complaints about construction activities.

Extreme Housekeeping

Extreme housekeeping on a project site means meticulous organization—secure storage, strong fencing, strict entry/exit protocols, and relentless maintenance of trash, debris, and hazards to ensure safety and efficiency. Every area remains orderly, materials are strategically placed, and workspaces are kept pristine, minimizing risks and maximizing productivity.



Kiewit’s vast network of supplier relationships will allow our team to coordinate on-time delivery for the majority of the Project’s materials. Materials delivered to the Project site for installation will be kept within the lane closures, safely protected from the public in neat, square stockpiles, preventing pedestrians and children from entering these areas. Extra material and those scheduled for later installation will be kept off-site (preliminary site options provided in **Exhibit 105**) or coordinated with vendors for on-time delivery.

Roadway Work

Over 70,000 SY of roadway reconstruction will be performed across three phases once utility work is complete. KISC will develop the phasing and MOT plan with Kimley-Horn, the City, and stakeholders. Early planning efforts will be required to complete construction with single-lane closures that permit access for all residents throughout the MOT operations.

For raised roadways, 6-in. to 21-in. of Expanded Slate Aggregate will be placed beneath 8-in. of limerock base and 3-in. of asphalt. KISC understands the importance of finding a qualified supplier and installer for timely delivery of lightweight aggregates. For roads that are not required to be raised, the asphalt will be reconstructed by excavating 4.5-in. of asphalt and base, proof rolling and compaction to 98%, and then 4.5-in. of asphalt will be placed. KISC’s QA/QC team will closely monitor each phase of construction, ensuring that any signs of road instability or asphalt cracking are promptly identified. If such issues arise, we will immediately consult with the City and Kimley-Horn and, if necessary, perform an additional 4.5 inches of excavation, replacing it with new base material to maintain long-term pavement integrity.

Exhibit 105: Site laydown options map



Waterline Work

Over 22,000 LF of new 6-in.-10-in. potable watermains will be installed on this Project. KISC can self-perform or subcontract all waterline work, including dewatering. If required, dewatering will be self-performed or subcontracted to a qualified firm.

To minimize disruptions, waterline installation will be coordinated with storm drainage pipe and structure installation, roadway reconstruction, lateral installation, and point repair work. Most waterline installations will take place under single-lane closures, with uninterrupted access to residences throughout construction. Materials will be delivered as needed, preventing unnecessary stockpiling and minimizing disruptions. Extreme Housekeeping will be strictly maintained, keeping the worksite free of debris and potential hazards by organizing laydown areas, properly storing materials, and preventing obstructions that could impact residents. KISC recognizes that a clean, well-organized site not only promotes safety and efficiency but also reflects our commitment to professionalism and quality work. KISC recognizes that what the public sees of our work zone is a direct reflection of KISC, as a contractor committed to performing our work to the highest level.

Waterline work will include the demolition of existing streets, curbs, and gutters. Waterline pipe, valves, and services will be installed, chlorinated, pressure, and BacT tested. Once cleared, meter tie-ins and permanent connections will be completed. The status from installation through clearance and final tie-in will be tracked and communicated to the City for transparency.

Proven Neighborhood Infrastructure Expertise for Tropic Isle

Kiewit has successfully completed trenchless waterline installation in crowded urban locations similar to the Tropic Isle Neighborhood. Pre-chlorinated Pipe Bursting and Horizontal Directional Drill (HDD) provide alternatives to open trench construction which minimize resident impacts and reduce restoration limits. Kiewit has successfully subcontracted with Murphy Pipeline Contractors to install 18+ miles of trenchless waterline throughout the City of Tampa. Pipe Bursting and HDD eliminate up to 92% of excavation compared to traditional open cut methods of installation.

Sanitary Sewer Work

KISC's extensive experience with sanitary lining and repairs provides a strategic advantage in delivering efficient, high-quality solutions. By maintaining a self-perform crew alongside a sanitary lining subcontractor, KISC can address unexpected issues like blockages, collapses, or damaged mains in real time. This proactive approach allows for quick point repairs, ensuring continuous lining operations without interruptions so all residents remain in service.

This strategy eliminates the need for the specialty subcontractor to demobilize and remobilize, saving both time and costs while facilitating seamless completion of the entire scope of work. With over 24,000 LF of sanitary main requiring lining or replacement on this project, the ability to perform point repairs as required ensures all mains are lined as designed, with the potential to eliminate mains originally designed for remove and replacement.

The Project will involve over 400 total sanitary laterals and cleanouts across three phases. To minimize impacts on residents, KISC will work closely with the MRG team to communicate clearly and consistently with residents and stakeholders, including:

- **Right of Entry Approvals:** Securing necessary permissions for work on private property through early, effective communication.
- **Scheduling Coordination:** Planning work to minimize disruption to residents' daily routines.
- **Resident Engagement:** Keeping residents informed about the scope of work, timelines, and restoration efforts to ensure transparency and build trust.

KISC understands that residents are the ultimate stakeholders in this project. Our team is committed to conducting repairs, installations, and restoration with minimal invasiveness, ensuring a positive experience for each resident. **Our goal is to turn every resident into a "raving fan" of both the City's initiative and KISC's performance, fostering goodwill and long-term community satisfaction.**

Storm Drainage Work

Our PEC will ensure Best Management Practices (BMPs) and proper dewatering practices are followed to maintain environmental compliance. KISC will sequence the stormwater work to ensure the current storm system remains functional during the installation of the new system, mitigating the risk of flooding in the neighborhood throughout the project. During the rainy season or forecasted rain events, we can set up storm bypass piping to maintain system functionality. The installation of the new storm drain will commence from the outfall and progress upstream, allowing for positive drainage and enabling us to maintain a dewatering discharge outside the neighborhood, facilitating a quicker functioning system in line with the construction schedule. KISC will strategically excavate and lay only enough pipe each day to ensure it can be backfilled by the end of the day, providing residents with continuous access to their driveways and homes. We will use street plates or backfill stormwater trenches daily to maintain this access; KISC is committed to ensuring 100% access for all residents, including vehicle, pedestrian, and bicycle traffic. We will maintain an offsite storage yard, bringing only the necessary pipe and structures to the site each day. Our crew trucks will be equipped with trailers to facilitate this construction model. Our construction equipment will be strategically sized to handle the largest structures while fitting within the tight neighborhood streets, utilizing zero tail swing excavators and compact equipment. A CCTV subcontractor will be mobilized prior to paving to verify that joints and alignment meet the correct line and grade, ensuring a quality product built correctly the first time.

Drainage Outfall

As part of the Early Works package, KISC will procure and install the WaStop inline check valves at all outfall locations that will not be replaced. Prioritizing early installation will aid in combatting the effects of high tide and the rainy season. Careful planning and safety precautions will be required for installing these valves from the waterside of the seawalls. For locations that require a new outfall storm pipe and removal of the existing outfall, a properly coordinated and executed outfall pipe installation plan will be developed. Proper set up of trench boxes and dewatering inside the narrow easements will ensure resident's property and homes are not disturbed. This scope will be well communicated with the adjacent residents so they understand the magnitude of the impact and timeline for installation. Final restoration of the easements will be scheduled to immediately follow the backfill to ensure the impact is minimized to the public.

Final Landscape Restoration

Final landscape restoration, valley gutter installation, and improvements will serve as a significant visual and functional milestone for the Project, enhancing the aesthetic appeal of the final product. These improvements also play a critical role in effective stormwater management, ensuring the long-term resilience and functionality of the neighborhood's infrastructure. KISC will meticulously coordinate final landscape and gutter installations with the construction of underground systems including watermain, sanitary sewer, and stormwater systems. This comprehensive, integrated approach will ensure seamless alignment between surface and subsurface elements, minimizing disruptions. KISC's finish-as-we-go approach allows us to restore each street efficiently, providing both functional improvements and visual enhancements as work progresses. By completing and restoring areas in phases, this method reduces disruptions and delivers finished sections more quickly, minimizing the overall impact on residents.

To support consistent Project communication, signs will be placed near active work zones with contact information, allowing residents to inquire about restoration details and timelines. This strategy reduces inquiries to City Hall or community leaders, ensuring residents have direct access to project-specific information and support.

Seawall Improvements

During Pre-Construction, KISC looks forward to optimizing the seawall improvement design with Kimley-Horn and the City. We have utilized marine gear as well as smaller excavator-mounted hammers, and press-in methods for sheet pile installation. All options can be considered, depending on improvements required for the existing seawall. This aspect of the Project is critical for the ultimate sea level rise resiliency of the neighborhood.



7.C. CMAR Requirements in the Scope of Services

KISC has been a trusted construction partner for clients throughout Florida completing specialized water infrastructure projects, setting industry standards through innovation, expertise, and a proven track record in alternative project delivery. Kiewit has also performed CMAR contracts since 2000 and completed over 600 contracts; this experience gives us real-world data for development of solutions that optimize cost, schedule, and constructability. Clients trust KISC for our deep technical knowledge, extensive resources, and ability to drive true partnership, ensuring that alternative delivery methods are leveraged to their full potential for maximum project success.

As previously discussed, KISC's ability to self-perform the majority of the work provides direct control over the Project schedule, allowing us to actively manage procurement, resources, risks, and budget on a daily basis. This hands-on approach minimizes delays, optimizes sequencing, and enhances flexibility in responding to Project needs. By maintaining tight oversight of critical path activities, we can proactively address challenges, reduce risks, and drive efficiency, ultimately delivering a high-quality Project on time and within budget.

Phase I: Pre-Construction Services

KISC will provide all services outlined in Sections 3.3.1 (Phase I: Pre-Construction Services) and 3.3.2 (Phase II: Construction Services) as detailed in the Project scope. These services align with our extensive experience and expertise in delivering complex construction projects. Our commitment during Pre-Construction includes, but is not limited to, the following:

Phase I: Pre-Construction Services

3.3.1.1	Review, coordinate, communicate the Project construction plan proposed subcontractors, and detailed schedule with Kimley-Horn, the selected Owner's Representative, and the City.
3.3.1.2	Develop a Project estimate using InEight, our proprietary estimating tool, where cost saving, value engineering, and constructability changes can be entered in real-time and logged for coordination with Kimley-Horn and the City.
3.3.1.3	Utilize P6 to further expand and fully detail each activity in the already developed baseline schedule (see pages 72-73), for an accurate roadmap of the Project from start to finish.
3.3.1.4	Use Bluebeam Studio to solicit comments and input from our Subject Matter Experts on design. This allows both value engineering and constructability reviews to occur at the same time.
3.3.1.5	Kiewit's InEight Estimate Software utilizes a work breakdown structure to facilitate bid-leveling and accurate estimate comparison. By eliminating the potential for scope gaps and incomplete estimates, KISC and the City can have confidence that the GMP proposal will provide the best value for cost quality, and schedule.
3.3.1.6	InEight Estimate will be utilized to develop a thorough, open-book budget for the Project. This will be updated as the design and Project progresses and serves as the basis of the GMP, along with the Project schedule.
3.3.1.7	KISC will utilize BuildingConnected software to solicit appropriate and competitive bids from qualified subcontractors. In addition, KISC maintains a presence at South Florida contractor outreach events, giving the team access to hundreds of local contractors excited for the opportunity to continue and/or begin relationships with KISC and the City of Delray Beach.
3.3.1.8	Coordination with third-party utility owners, county, City, FDOT, residents, and other stakeholders is part of our everyday processes and procedures. We will make sure to develop a required permit log and will work toward acquiring those permits as expeditiously as possible.
3.3.1.9	Compiling our estimate with the detailed schedule, and our proposal including qualifications and clarifications, will serve as our GMPs proposed for the City's review and approval.
3.3.1.10	KISC can tap into resources in our company to explore alternative funding if the City would like the assistance in finding grants, per FDEP Grant 22SRP60 Agreement. KISC has already reviewed the grant agreement and addendum, and has no issues fully complying with the requirements laid out.

Value Engineering and Innovative Concepts

KISC's CMAR process leverages the collaborative nature of this delivery model by encouraging and evaluating construction means and methods that may result in cost and schedule savings, safety improvements, constructability, and minimizing neighborhood disruptions. Kiewit proposes establishing integrated multi-disciplinary working groups at the earliest stages of the Pre-Construction Phase. We will identify these groups at the previously discussed Partnering Kick-off Meeting and establish the importance of the working groups as a means of identifying and evaluating value engineering concepts. This promotes mutual understanding of issues between the KISC and Kimley-Horn teams, as well as the opportunity to workshop potential solutions collaboratively. Together with the City and Designers, we evaluate and analyze all alternatives, understanding the cost, schedule, and risk of each value engineering item for informed decisions. Value engineering items will be tracked in a shared file for the City to see updated cost reduction. Our in-house engineering and design team differentiates Kiewit, as it allows for additional expert analysis of value engineering options, should the City require further input.

Preliminary value engineering solutions and innovative concepts to reduce overall cost and future maintenance based on the advanced design:



Value Engineering Idea #1: *All sanitary mains and laterals will be rehabilitated using CIPP lining, with a small, highly efficient KISC self-perform crew handling point repairs and cleanout installations. This flexible, multi-tasking crew eliminates downtime by addressing necessary repairs before the CIPP subcontractor mobilizes, preventing costly stop-and-start delays caused by damaged existing lines.*

Value Engineering Idea #2: *Standardizing the water main material will streamline installation, improving efficiency while reducing delays associated with handling multiple material types. Additionally, bulk procurement of a single material will provide greater cost control.*

Value Engineering Idea #3: *Recycling the asphalt, removing and processing the base material to use as roadway fill/base. Recycle concrete demolished from sidewalks and also reuse as recycled concrete aggregate for roadway base.*

Procurement and Managing Project Cost Drivers

The Kiewit Supply Network (KSN) group specializes in providing comprehensive procurement services, helping us execute complex projects with expedited schedules on time at the lowest practicable cost. KSN serves as a resource for benchmarking and assessing escalation measures for the estimating team to use in the bidding process. This support function allows us to manage overall commodity risk and, in turn, provide cost-competitive bids to our clients.

By engaging our KSN team, the City can take full advantage of a proactive procurement strategy that drives efficiency and financial certainty. Through detailed cost analysis, we will pinpoint key cost drivers, quantify the financial impact of waiting to purchase versus early procurement, and provide the full Project team with data-driven projections. This insight enables us to strategically lock in material, logistics, and equipment pricing before market fluctuations drive up costs. More importantly, it allows us to establish the GMP early, securing cost certainty for the City and shielding the Project from unpredictable price escalation. In a volatile market where material shortages and inflationary pressures can derail budgets, this approach ensures that Project's dollars go further, the schedule remains intact, and the City maintains control over its financial commitments.

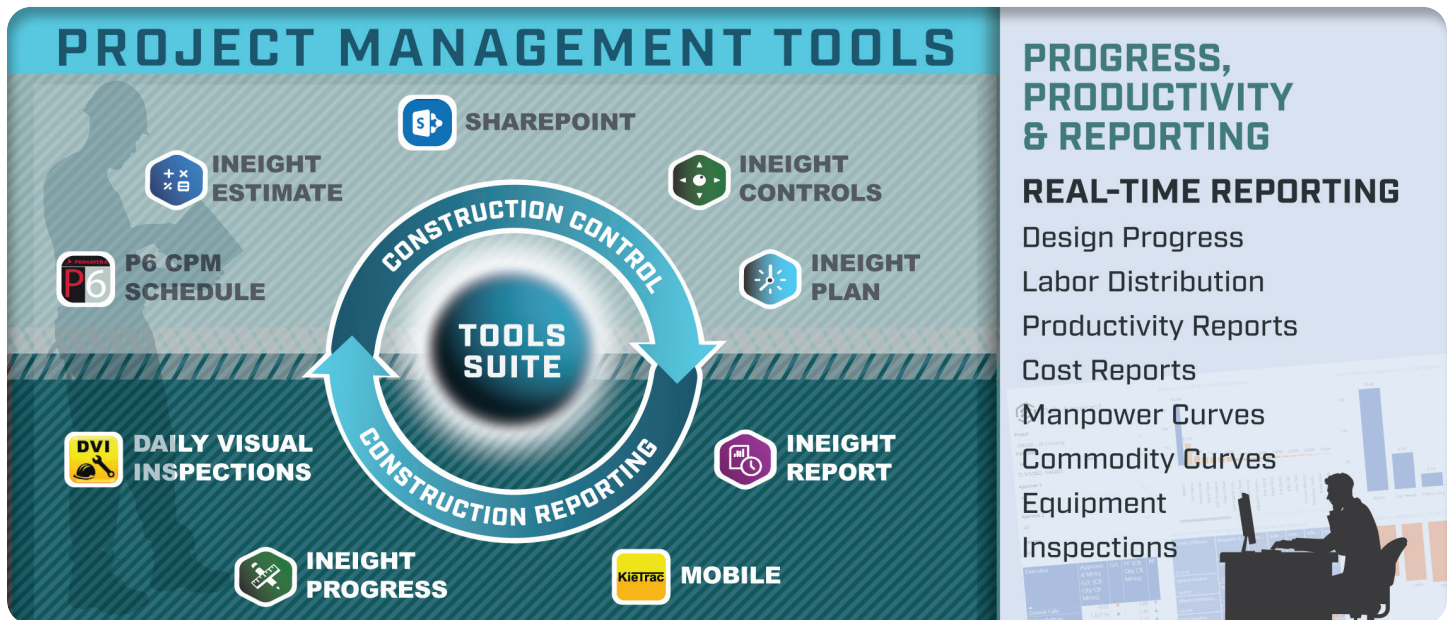
We expect the following will be major cost drivers on the Tropic Isle Neighborhood Improvements Project:

- **Watermain Materials (HDPE or DI)**
- **Watermain Fittings**
- **Asphalt**
- **Precast Concrete Pipe and Inlets**
- **Lightweight Fill and Roadway Aggregates**

GMP Development

Our cost model is built utilizing Kiewit's proprietary, industry-marketed estimating system, InEight Estimate, offering the City transparency, accuracy, and flexibility with a level of detail that enables a thorough Project estimate with no surprises. **Exhibit 106** highlights our proprietary suite of InEight project management tools.

Exhibit 106 | *InEight Project Management Tools*



Lead Estimator Abby Conner will oversee upfront estimate coordination with the City, working to align InEight Estimate with the City's document management system. This alignment will provide accurate summary-level details, including operational labor, equipment, materials, subcontractor costs, and production data, supporting estimate comparisons throughout GMP development and aiding in the establishment of the Project's schedule of values. The data can be sorted by work discipline and Project location, with multiple scenarios integrated into the Pre-Construction schedule in real time to support forecasting and planning efforts.

Immediately upon Pre-Construction award, KISC will provide an early baseline GMP estimate. The baseline estimate will be developed based on the current design and will provide many benefits to the City, including:

- Early alignment between KISC and the City estimate, which will streamline future GMP reviews.
- Identification and understanding of "where the money is" so the Project team can focus on cost reduction ideas that will have the biggest impact and aid in decision-making.
- Comparison of future value engineering items to the baseline estimate to keep a running total of Project savings.
- KISC's baseline estimate and project schedule will provide the City support related to annual grant funding approvals and budget planning

As the design progresses, we will update the baseline estimate and provide a final GMP Estimate. We will divide the GMP into multiple packages to be awarded separately and will assist in awarding the work packages.

Phase II: Construction Services

KISC will seamlessly transition from Pre-Construction to Construction by maintaining team continuity and strong communication. Brad Williamson will serve as Project Principal, with Project Manager Joe Pescatore and Fabricio Cedillo as Construction Manager, throughout both phases, ensuring a consistent leadership presence. During Pre-Construction, Fabricio will lead Superintendents Mark Thomas, Luke Gutshall, Alberto Baca, and Ryan Conner in constructability reviews, then transition with them into active construction, carrying forward Project knowledge and strategy. By building on Pre-Construction momentum, KISC will effectively communicate Project expectations to any additional construction personnel, maintaining alignment with Project goals. Our commitment to collaboration with the City and Project stakeholders will remain steadfast, working diligently to successfully deliver all scopes of service outlined in the RFP. Our commitment during construction includes, but is not limited to, the following:

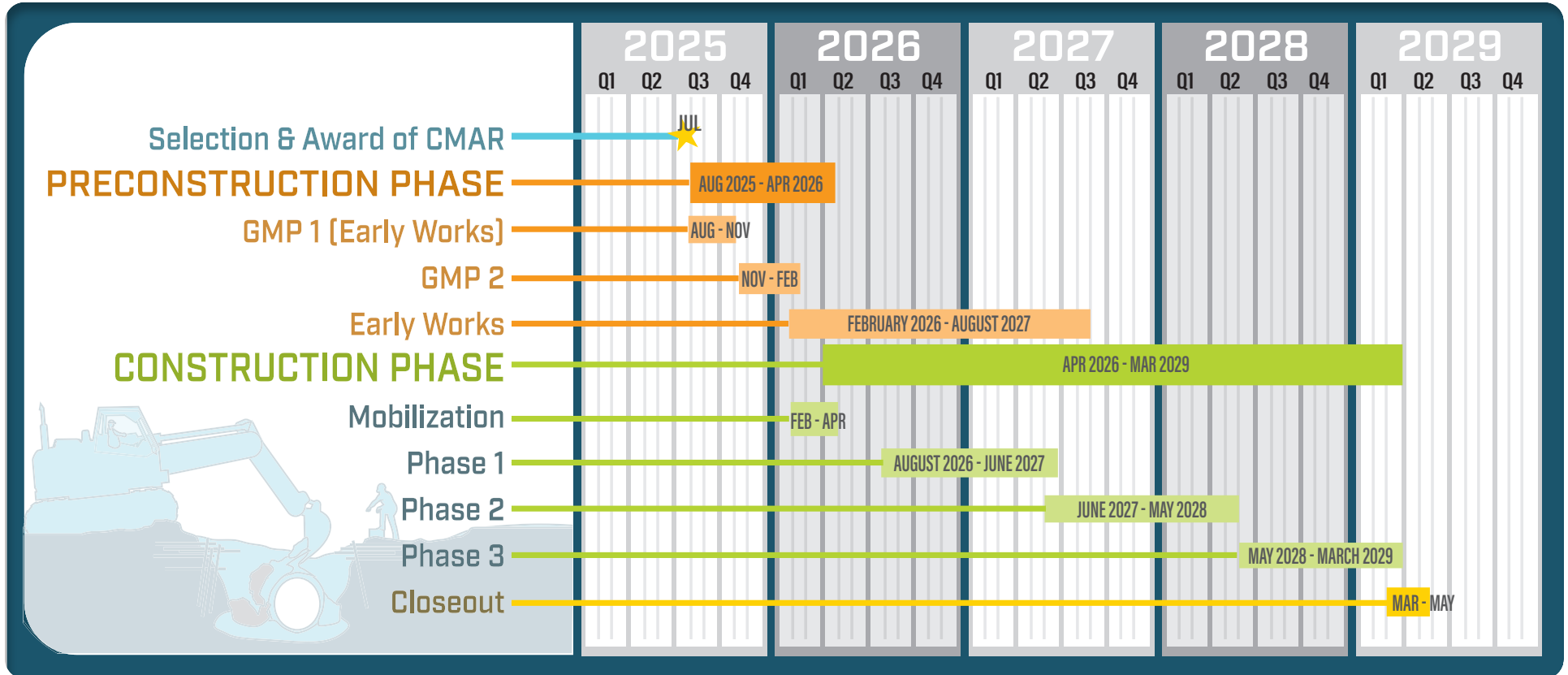
Phase II: Construction Services

- 3.3.2.1** | As your trusted and financially stable contractor, KISC will provide Performance and Payment Bonds for the full value of the GMP and will ensure strict compliance with all insurance requirements including those of our subcontractors and consultants, as part of our commitment to excellence.
- 3.3.2.2** | KISC will apply for, track and obtain, coordinate, and pay for all permits, construction inspections, and testing as required to complete the project. This will confirm seamless and efficient project execution. We incorporate robust quality processes both internally and as well as third-party testing.
- 3.3.2.3** | KISC commits to successful, timely, and cost-effective completion of the Project. We take a great amount of pride completing technically challenging projects ahead of schedule. We meticulously create, maintain, and present a comprehensive P6 construction schedule of activities showing the path to completion.
- 3.3.2.4** | We commit to consistent coordination with the City of Delray Beach Department of Public Works, Kimley-Horn, and Owner's Representative for the duration of the Project. CMAR contracts are only successful with frequent, and transparent communications.
- 3.3.2.5** | It is our practice to proactively coordinate with homeowners, governmental agencies, and other Project stakeholders, and all utility companies servicing this area. Our commitment to "over-communicate" and execute on our promises enables us to minimize - or eliminate where possible - potential disruptions.
- 3.3.2.6** | KISC will maintain a safe and reliable MOT plan, considering neighborhood activities for weekends and holidays. We pledge to maintain one active lane of traffic at all times, undertake extreme housekeeping while constructing the Project, and provide continual access to residents. Part of our process is to have staff tour the project during non work hours to ensure MOT is clean and set up per plan.
- 3.3.2.7** | Following Kiewit's ISO 9001-approved quality control program, we will self-verify construction activities to ensure conformity with the Contract Documents. In addition, we will have third party testing for appropriate hold points. We will provide certified as-builts drawings (CAD & GIS) at the completion of the Project. KISC will meet early with City staff to ensure a seamless transition of our GIS data into the City's database.
- 3.3.2.8** | We will develop and execute a commissioning plan, including the assembly of guarantees, closeout documents, training, and final acceptance, as well as delivery and training of O&M manuals to the City's satisfaction. KISC will ensure all manufacturer contacts are provided as part of the turnover process.
- 3.3.2.9** | KISC will coordinate and monitor the resolution of punch list items and warranty complaints to the satisfaction of the City during the warranty period. Just like all other projects KISC completes we will finish as we go. Performing street by street walks with City Staff, and completing outstanding items well before the completion of the project.
- 3.3.2.10** | KISC has performed numerous CMAR contracts. We pride ourselves in being available, over communication, and transparency for all of our Clients. KISC will lead and keep minutes for regular job site meetings, maintain and update schedules, oversee quality assurances, maintain and provide copies of all contract documents, and ensuring compliance with all safety programs. If needed KISC will utilize Ineight Document for all submittals, transmittals, RFI's, etc.
- 3.3.2.11** | KISC, as designated CMAR Firm, acknowledges we manage the construction project for the City as an open book project. KISC has a lot of experience administering GMP contracts, including ensuring shared savings, scope deductions, and use of contingencies is fair for all parties. Transparent Financial management is key to success of both the City and KISC.
- 3.3.2.12** | KISC has performed mutiple contracts with federal funds as part of the project. Buy American / America as well as davis bacon wages, etc is part of our normal business. KISC will comply with all State requirements noted in the City's acquired FDEP Grant.

7.D. Schedule

The summary schedule is based on our preliminary understanding of the RFP and common anticipated tasks and activities performed during CMAR Pre-Construction. Upon award, we will work with the City and Kimley-Horn to agree on critical activities and durations, ensuring an on-time construction start. See pages 73-74 for our detailed schedule to-date, based on information available at the time of this submission.

Exhibit 107 | Summary schedule



Schedule Management Approach

Kiewit's tools for active schedule management include progressively detailed schedules that help in planning and later in reporting progress on the Project. **Exhibit 108** provides an overview of the schedule management tools, meetings, and processes that KISC will utilize to maintain and control the schedule on the Tropic Isle Neighborhood Improvements Project.

Exhibit 108: *Schedule Management Strategies*



BASELINE CONSTRUCTION SCHEDULE - Start of Work Phase

Critical path method schedule with activities for the full project scope including **permitting, submittals, procurement, and construction**. Developed during preconstruction, this baseline schedule collaboratively incorporates initial plans and assumptions that are then validated against our past projects data.

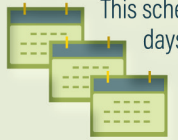
MONTHLY SCHEDULE UPDATE- Monthly

The update schedule progresses the prior month's update with work completed during the month. It evaluates float, increase or consumption, and any changes to milestones or project completion. The schedule update report is used for project progress tracking.



90-DAY SCHEDULE - Monthly

This schedule is prepared monthly for all activities with in the next 90 days, and includes all critical procurements, submittals, and reviews. It allows pre-planning so that the project team is prepared and aligned for field operations in advance and relevant material procurement activities are on track.



3-WEEK LOOK-AHEAD SCHEDULE - Weekly

This schedule is derived from the 90-Day Schedule and includes further activity details for field execution. It shows one week of as-built progress and three weeks of forecast to determine if the team is executing to schedule according to the plan. It is prepared with input from subcontractors and the procurement team and is reviewed with project management and operations team each week.



PLAY OF THE DAY - Daily



This review, undertaken daily by the Superintendent and operations supervisors, details the work required to meet the immediate objectives, and helps to coordinate operations and resources onsite to ensure efficient operations for the next day.

7.E. Location

The work will be performed within the specified Project limits. This includes all work performed by KISC and any subconsultants involved in the project. During the Pre-Construction Phase, our staff will be located at our Ft Lauderdale office, 6301 NW 5th Way, Suite 3200 Ft Lauderdale, FL 33309. During construction, we will rent a project office for our staff that is adjacent to the work.

7.F. Maintaining Availability and Access of Key Personnel

KISC is committed to collaborative engagement and accessibility throughout both the Pre-Construction and Construction Phases. Our team will be available for meetings, coordination efforts, general communication, and on-site supervision, ensuring responsiveness to the City and project stakeholders. By leveraging our resource-sharing approach of keeping the core management team managing both this project and if selected NW neighborhoods project, we can optimize schedule and cost efficiencies, providing the necessary expertise while maintaining flexibility in staffing. If additional superintendents, field engineers, subcontractors are required to be supplemented during project start up or peak construction, this can easily be accommodated with our KISC south Florida team.

For pre-scheduled meetings, KISC will coordinate attendance based on Project needs, ensuring the appropriate team members are present to contribute effectively. Whether held at City Hall (100 NW 1st Ave), Swinton Operations Center (434 South Swinton Ave), Kimley-Horn's West Palm Beach office (1920 Wekiva Way #200), or at the designated project office, our team will attend in person as needed. When remote participation is preferred or necessary, KISC's fully integrated technology capabilities—including Microsoft Teams, email, video conferencing, and telephone conferencing—will support seamless communication and collaboration.

Throughout the Project, Joe Pescatore will serve as the primary point of contact, facilitating consistent communication and coordination. The KISC team will remain accessible during both regular and off-hours, providing timely responses to inquiries and addressing any project-related concerns that arise. This flexible, responsive approach aligns with the City's expectations for availability while maintaining efficient Project execution.

This schedule has been developed based on information available at the time of this submission. KISC will create a highly detailed schedule during Phase 1 Pre-Construction, in coordination with the City and Kimley-Horn.

Delray Beach City Tropic Isle Neighborhood Improvement Estimate CPM					Estimate Schedule WBS Layout PDF												Print: 08-Apr-25 13:38										
Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2025				2026				2027				2028				2029					
						Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct
Delray Beach City Tropic Isle Neighborhood Improvement Esti					1041.00	09-Apr-25	06-Jun-29	0.00	06-Jun-29																		
Milestones and Admin					1041.00	09-Apr-25	06-Jun-29	0.00	06-Jun-29																		
ML.0	RFQ Submission	0.00	09-Apr-25		1.00	◆ RFQ Submission																					
GEN.1	Qualifications Review	60.00	10-Apr-25	08-Jun-25	1.00	◆ Qualifications Review																					
GEN.6	Pre-Construction Negotiations	45.00	09-Jun-25	23-Jul-25	1.00	◆ Pre-Construction Negotiations																					
ML.1	City Delray Beach City Commission Approval	0.00		24-Jul-25	1.00	◆ City Delray Beach City Commission Approval																					
GEN.2	City Precon Contract Preparation	30.00	25-Jul-25	23-Aug-25	1.00	◆ City Precon Contract Preparation																					
GEN.3	Project Calendar Days NTP Pre-Con to FC (LOE)	1383.00	24-Aug-25	06-Jun-29	0.00	◆ Project Calendar Days NTP Pre-Con to FC (LOE)																					
ML.2	NTP Pre-Construction	0.00	24-Aug-25		1.00	◆ NTP Pre-Construction																					
GEN.4	Project Calendar Days NTP Pre-Con to SC (LOE)	1322.00	24-Aug-25	06-Apr-29	0.00	◆ Project Calendar Days NTP Pre-Con to SC (LOE)																					
GEN.5	Pre-Construction Video	30.00	24-Aug-25	22-Sep-25	121.00	◆ Pre-Construction Video																					
ML.7	Early Works NTP	0.00	20-Feb-26		0.00	◆ Early Works NTP																					
ML.3	Full NTP Construction	0.00		16-Apr-26	66.00	◆ Full NTP Construction																					
ML.6	Early Works Substantial Completion	0.00		03-Aug-27	398.00	◆ Early Works Substantial Completion																					
ML.8	Early Works Final Completion	0.00		31-Aug-27	398.00	◆ Early Works Final Completion																					
ML.4	Substantial Completion	0.00		06-Apr-29	0.00	◆ Substantial Completion																					
ML.5	Project Complete / Final Completion	0.00		06-Jun-29	0.00	◆ Project Complete / Final Completion																					
Permit Acquisition					120.00	25-Aug-25	19-Feb-26	0.00	19-Feb-26																		
PRM-A.1	Delray Beach City MOT	30.00	25-Aug-25	06-Oct-25	90.00	◆ Delray Beach City MOT																					
PRM-A.2	Palm Beach County MOT	120.00	25-Aug-25	19-Feb-26	0.00	◆ Palm Beach County MOT																					
PRM-A.3	FDOT MOT	30.00	25-Aug-25	06-Oct-25	90.00	◆ FDOT MOT																					
GMP Preparation					160.00	25-Aug-25	16-Apr-26	45.00	16-Apr-26																		
PRP-GMP.4	90% Design Completion	60.00	25-Aug-25	18-Nov-25	40.00	◆ 90% Design Completion																					
PRP-GMP.5	GMP 1 Early Works Development	60.00	25-Aug-25	18-Nov-25	40.00	◆ GMP 1 Early Works Development																					
PRP-GMP.6	IFC Design Completion	45.00	19-Nov-25	28-Jan-26	60.00	◆ IFC Design Completion																					
PRP-GMP.8	GMP 2 Full Construction Development	60.00	19-Nov-25	19-Feb-26	45.00	◆ GMP 2 Full Construction Development																					
PRP-GMP.7	Commission Approval / Contract Prep GMP 1	20.00	19-Nov-25	18-Dec-25	40.00	◆ Commission Approval / Contract Prep GMP 1																					
PRP-GMP.9	Commission Approval / Contract Prep GMP 2	40.00	20-Feb-26	16-Apr-26	45.00	◆ Commission Approval / Contract Prep GMP 2																					
Engineering And Procurement					110.00	29-Jan-26	06-Jul-26	76.00	06-Jul-26																		
SUB.1	Construction Submittals	60.00	29-Jan-26	23-Apr-26	76.00	◆ Construction Submittals																					
PROC.2	Outfall Check Valve Procurement	90.00	20-Feb-26	26-Jun-26	26.00	◆ Outfall Check Valve Procurement																					
PROC.1	Drainage Procurement	50.00	24-Apr-26	06-Jul-26	76.00	◆ Drainage Procurement																					
Early Works					384.00	23-Feb-26	31-Aug-27	398.00	31-Aug-27																		
Early Works General					384.00	23-Feb-26	31-Aug-27	398.00	31-Aug-27																		
EW.GEN.2	Public Outreach Kickoff Early Works	10.00	23-Feb-26	06-Mar-26	0.00	◆ Public Outreach Kickoff Early Works																					
EW.GEN.1	MOT Early Works (LOE)	364.00	23-Feb-26	03-Aug-27	198.00	◆ MOT Early Works (LOE)																					
EW.GEN.0	Early Works Mobilization	5.00	02-Mar-26	06-Mar-26	0.00	◆ Early Works Mobilization																					
EW.GEN.3	Punchlist Early Works	20.00	04-Aug-27	31-Aug-27	398.00	◆ Punchlist Early Works																					
Early Works Phase 1					112.00	09-Mar-26	13-Aug-26	10.00	13-Aug-26																		
EW.PH1.1	Gravity Sewer CCTV/Cleaning Ph1	30.00	09-Mar-26	17-Apr-26	2.00	◆ Gravity Sewer CCTV/Cleaning Ph1																					
EW.PH1.4	Sanitary Bypass Ph1 (LOE)	112.00	09-Mar-26	13-Aug-26	2.00	◆ Sanitary Bypass Ph1 (LOE)																					
EW.PH1.2	Gravity Sewer CIPP Ph1	73.00	20-Apr-26	31-Jul-26	11.00	◆ Gravity Sewer CIPP Ph1																					
EW.PH1.5	Outfall Check Valve Ph1	15.00	29-Jun-26	21-Jul-26	24.00	◆ Outfall Check Valve Ph1																					
EW.PH1.3	Manhole Rehabilitation Ph1	32.00	30-Jun-26	13-Aug-26	2.00	◆ Manhole Rehabilitation Ph1																					
Early Works Phase 2					121.00	14-Aug-26	10-Feb-27	108.00	10-Feb-27																		
EW.PH2.1	Gravity Sewer CCTV/Cleaning Ph2	30.00	14-Aug-26	25-Sep-26	100.00	◆ Gravity Sewer CCTV/Cleaning Ph2																					
EW.PH2.2	Sanitary Bypass Ph2 (LOE)	121.00	14-Aug-26	10-Feb-27	100.00	◆ Sanitary Bypass Ph2 (LOE)																					
EW.PH2.5	Outfall Check Valve Ph2	15.00	02-Sep-26	25-Sep-26	187.00	◆ Outfall Check Valve Ph2																					
EW.PH2.3	Gravity Sewer CIPP Ph2	60.00	28-Sep-26	23-Dec-26	131.00	◆ Gravity Sewer CIPP Ph2																					
EW.PH2.4	Manhole Rehabilitation Ph2	31.00	28-Dec-26	10-Feb-27	100.00	◆ Manhole Rehabilitation Ph2																					
Early Works Phase 3					121.00	11-Feb-27	03-Aug-27	205.00	03-Aug-27																		
EW.PH3.1	Gravity Sewer CCTV/Cleaning Ph3	30.00	11-Feb-27	25-Mar-27	198.00	◆ Gravity Sewer CCTV/Cleaning Ph3																					
EW.PH3.2	Sanitary Bypass Ph3 (LOE)	121.00	11-Feb-27	03-Aug-27	198.00	◆ Sanitary Bypass Ph3 (LOE)																					
EW.PH3.5	Outfall Check Valve Ph3	15.00	04-Mar-27	25-Mar-27	277.00	◆ Outfall Check Valve Ph3																					
EW.PH3.3	Gravity Sewer CIPP Ph3	60.00	26-Mar-27	18-Jun-27	229.00	◆ Gravity Sewer CIPP Ph3																					
EW.PH3.4	Manhole Rehabilitation Ph3	31.00	21-Jun-27	03-Aug-27	198.00	◆ Manhole Rehabilitation Ph3																					
Mobilization/Preparatory Work					419.00	09-Mar-26	03-Nov-27	133.00	03-Nov-27																		
PRP-GEN.1	Field Investigation	40.00	09-Mar-26	05-May-26	0.00	◆ Field Investigation																					
PRP-GEN.6	Public Outreach Kickoff Full Construction	15.00	17-Apr-26	07-May-26	45.00	◆ Public Outreach Kickoff Full Construction																					
PRP-MOB.2	Full Construction Mobilization	5.00	17-Apr-26	23-Apr-26	55.00	◆ Full Construction Mobilization																					
PRP-GEN.2	Potholing	40.00	06-May-26	08-Jul-26	0.00	◆ Potholing																					
PRP-GEN.7	Public Outreach / Community Meetings Phase 1	25.00	08-May-26	12-Jun-26	45.00	◆ Public Outreach / Community Meetings Phase 1																					
PRP-GEN.3	Third Party Utility Relocation Phase 1	40.00	09-Jul-26	17-Aug-26	0.00	◆ Third Party Utility Relocation Phase 1																					
PRP-GEN.4	Third Party Utility Relocation Phase 2	40.00	18-Aug-26	26-Sep-26	282.00	◆ Third Party Utility Relocation Phase 2																					
PRP-GEN.5	Third Party Utility Relocation Phase 3	40.00	27-Sep-26	05-Nov-26	560.00	◆ Third Party Utility Relocation Phase 3																					
PRP-GEN.8	Public Outreach / Community Meetings Phase 2	25.00	12-Nov-26	18-Dec-26	134.00	◆ Public Outreach / Community Meetings Phase 2																					
PRP-GEN.9	Public Outreach / Community Meetings Phase 3	25.00	30-Sep-27	03-Nov-27	133.00	◆ Public Outreach / Community Meetings Phase 3																					
Construction					743.00	17-Apr-26	06-Apr-29	0.00	06-Apr-29																		
Summaries					1086.00	17-Apr-26	06-Apr-29	0.00	06-Apr-29																		
SUM-LOE.1	Maintain Erosion Controls (LOE)	1086.00	17-Apr-26	06-Apr-29	0.00	◆ Maintain Erosion Controls (LOE)																					
SUM-LOE.2	Third Party Utility Coordination (LOE)	120.00	09-Jul-26	05-Nov-26	560.00	◆ Third Party Utility Coordination (LOE)																					

Delray Beach City Tropic Isle Neighborhood Improvement Estimate CPM

Estimate Schedule WBS Layout PDF

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Activity ID	Activity Name	Original Duration	Start	Finish	Total Float	2025																																																2026																																																2027																																																2028																																																2029																																															
						Qtr 2				Qtr 3				Qtr 4				Qtr 1				Qtr 2				Qtr 3				Qtr 4				Qtr 1				Qtr 2				Qtr 3				Qtr 4				Qtr 1				Qtr 2				Qtr 3																																																																																																																																																																																											
						Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug																																																																																																																																																																																										
SUM-LOE.3	Maintenance of Traffic Phase 1 (LOE)	319.00	18-Aug-26	02-Jul-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
SUM-LOE.4	Maintenance of Traffic Phase 2 (LOE)	318.00	06-Jul-27	18-May-28	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
SUM-LOE.5	Maintenance of Traffic Phase 3 (LOE)	323.00	19-May-26	06-Apr-29	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
PHASE 1		205.00	18-Aug-26	02-Jul-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P1.1	Roadway DEMO P1	60.00	18-Aug-26	18-Nov-26	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P1.2	Storm Pipe & Structures Demo P1	40.00	28-Aug-26	28-Oct-26	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P1.7	Sanitary Laterals & Cleanouts P1	48.00	28-Aug-26	10-Nov-26	35.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P1.8	Potable Water Mains P1	80.00	28-Aug-26	04-Jan-27	3.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P1.3	Storm Pipe P1	80.00	10-Sep-26	15-Jan-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P1.4	Storm Structures P1	60.00	12-Oct-26	15-Jan-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P1.9	Roadway Lightweight Subgrade P1	60.00	03-Nov-26	08-Feb-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P1.10	Roadway Subgrade P1	30.00	08-Jan-27	24-Feb-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P1.11	Flatwork Valley Gutter P1	28.00	25-Feb-27	06-Apr-27	2.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P1.12	Driveways P1	40.00	25-Feb-27	23-Apr-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P1.13	Asphalt P1	25.00	26-Mar-27	30-Apr-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P1.14	Irrigation Improvements P1	45.00	22-Apr-27	30-Jun-27	2.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P1.15	Signing & Striping P1	20.00	03-May-27	01-Jun-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P1.16	Landscape Restoration P1	40.00	03-May-27	02-Jul-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
PHASE 2		205.00	06-Jul-27	18-May-28	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P2.1	Roadway DEMO P2	60.00	06-Jul-27	06-Oct-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P2.2	Storm Pipe & Structures Demo P2	40.00	16-Jul-27	16-Sep-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P2.3	Sanitary Laterals & Cleanouts P2	48.00	16-Jul-27	29-Sep-27	35.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P2.4	Potable Water Mains P2	80.00	16-Jul-27	16-Nov-27	3.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P2.5	Storm Pipe P2	80.00	29-Jul-27	01-Dec-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P2.6	Storm Structures P2	60.00	30-Aug-27	01-Dec-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P2.7	Roadway Lightweight Subgrade P2	60.00	23-Sep-27	23-Dec-27	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P2.8	Roadway Subgrade P2	30.00	23-Nov-27	12-Jan-28	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P2.11	Flatwork Valley Gutter P2	28.00	13-Jan-28	24-Feb-28	2.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P2.10	Driveways P2	40.00	13-Jan-28	13-Mar-28	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P2.9	Asphalt P2	25.00	14-Feb-28	21-Mar-28	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P2.13	Irrigation Improvements P2	45.00	10-Mar-28	15-May-28	2.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P2.12	Signing & Striping P2	20.00	22-Mar-28	19-Apr-28	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P2.14	Landscape Restoration P2	40.00	22-Mar-28	18-May-28	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
PHASE 3		205.00	19-May-28	06-Apr-29	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P3.1	Roadway DEMO P3	60.00	19-May-28	22-Aug-28	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P3.2	Storm Pipe & Structures Demo P3	40.00	31-May-28	02-Aug-28	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P3.3	Sanitary Laterals & Cleanouts P3	48.00	31-May-28	14-Aug-28	35.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P3.4	Potable Water Mains P3	80.00	31-May-28	04-Oct-28	3.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P3.5	Storm Pipe P3	80.00	13-Jun-28	16-Oct-28	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P3.6	Storm Structures P3	60.00	14-Jul-28	16-Oct-28	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P3.7	Roadway Lightweight Subgrade P3	60.00	08-Aug-28	07-Nov-28	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P3.8	Roadway Subgrade P3	30.00	10-Oct-28	27-Nov-28	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P3.11	Flatwork Valley Gutter P3	28.00	28-Nov-28	11-Jan-29	2.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P3.10	Driveways P3	40.00	28-Nov-28	30-Jan-29	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P3.9	Asphalt P3	25.00	29-Dec-28	06-Feb-29	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P3.13	Irrigation Improvements P3	45.00	29-Jan-29	04-Apr-29	2.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P3.12	Signing & Striping P3	20.00	07-Feb-29	08-Mar-29	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CON.P3.14	Landscape Restoration P3	40.00	07-Feb-29	06-Apr-29	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
Close Out		60.00	08-Apr-29	06-Jun-29	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															
CO.1	Punchlist / Demobilize	60.00	08-Apr-29	06-Jun-29	0.00	[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																																[Gantt Bar]																																															

Tab 8 - 4.2.8 Attachments

ACKNOWLEDGEMENT OF ADDENDA

Please complete Part I or Part II, as applicable

PART I:

List below the dates of issue for each addendum received in connection with this solicitation:

Addendum #1, Dated March 17, 2025

Addendum #2, Dated March 24, 2025

Addendum #3, Dated March 26, 2025

Addendum #4, Dated _____

Addendum #5, Dated _____

Addendum #6, Dated _____

Addendum #7, Dated _____

Addendum #8, Dated _____

Addendum #9, Dated _____

Addendum #10, Dated _____

PART II:

NO ADDENDUM WAS RECEIVED IN CONNECTION WITH THIS SOLICITATION

Kiewit Infrastructure South Co.

Firm Name

Timothy J. Cleary

Signature

Timothy J. Cleary | Senior Vice President

Name and Title (Print or Type)

04/09/2025

Date

PROPOSAL SUBMITTAL SIGNATURE PAGE

By signing this Proposal, the Proposer certifies that it satisfies all legal requirements as an entity to do business with the City, including all Conflict of Interest and Code of Ethics provisions.

Firm Name: Kiewit Infrastructure South Co.

Street Address: 6301 NW 5th Way, Suite 3200, Ft. Lauderdale, FL 33309

Mailing Address (if different from Street Address):
Same as above.

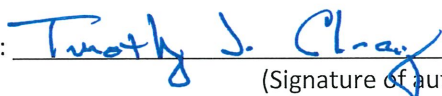
Telephone Number(s): (770) 487-2300

Fax Number(s): (770) 487-0005

Email Address: Tim.Cleary@kiewit.com

Federal Employer Identification Number: 47-0530367

Prompt Payment Terms: TBD % TBD days' net 30 days

Signature: 
(Signature of authorized agent)

Print Name: Timothy J. Cleary

Title: Senior Vice President

Date: 04/09/2025

By signing this document, the Proposer agrees to all terms and conditions of this solicitation and the resulting contract/agreement.

THE EXECUTION OF THIS FORM CONSTITUTES THE UNEQUIVOCAL OFFER OF PROPOSER TO BE BOUND BY THE TERMS OF ITS PROPOSAL, FOR NOT LESS THAN ONE HUNDRED AND TWENTY (120) DAYS, AND THE PROPOSER'S UNEQUIVOCAL OFFER TO BE BOUND BY THE TERMS AND CONDITIONS SET FORTH IN THIS SOLICITATION. FAILURE TO SIGN THIS SOLICITATION WHERE INDICATED ABOVE, BY AN AUTHORIZED REPRESENTATIVE, SHALL RENDER THE PROPOSAL NON-RESPONSIVE. THE CITY MAY, HOWEVER, IN ITS SOLE DISCRETION, ACCEPT ANY PROPOSAL THAT INCLUDES AN EXECUTED DOCUMENT WHICH UNEQUIVOCALLY BINDS THE PROPOSER TO THE TERMS OF ITS PROPOSAL.

CONFLICT OF INTEREST DISCLOSURE FORM

The award of this contract is subject to the provisions of Chapter 112, *Florida Statutes*. All Proposers must disclose within their Proposal: the name of any officer, director, or agent who is also an employee of the City of Delray Beach.

Furthermore, all Proposers must disclose the name of any City employee who owns, directly or indirectly, an interest of more than five percent (5%) in the Proposer’s firm or any of its branches.

The purpose of this disclosure form is to give the City the information needed to identify potential conflicts of interest for evaluation team members and other key personnel involved in the award of this contract.


The term “conflict of interest” refers to situations in which financial or other personal considerations may adversely affect, or have the appearance of adversely affecting, an employee’s professional judgment in exercising any City duty or responsibility in administration, management, instruction, research, or other professional activities.

Please check one of the following statements and attach additional documentation if necessary:

- To the best of our knowledge, the undersigned firm has no potential conflict of interest due to any other Cities, Counties, contracts, or property interest for this Proposal.
- The undersigned firm, by attachment to this form, submits information which may be a potential conflict of interest due to other Cities, Counties, contracts, or property interest for this Proposal.

Acknowledged by:

Kiewit Infrastructure South Co.
Firm Name


Signature

Timothy J. Cleary | Senior Vice President
Name and Title (Print or Type)

04/09/2025
Date

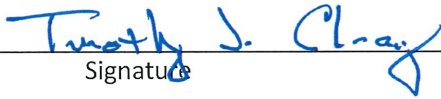
NOTIFICATION OF PUBLIC ENTITY CRIMES LAW

Pursuant to Section 287.133, *Florida Statutes*, you are hereby notified that a person or affiliate who has been placed on the convicted contractors list following a conviction for a public entity crime may not submit a Proposal on a contract to provide any goods or services to a public entity; may not submit a Proposal on a contract with a public entity for the construction or repair of a public building or public work; may not submit Proposals on leases or real property to a public entity; may not be awarded or perform work as a contractor, supplier, sub-Proposer, or consultant under a contract with any public entity; and may not transact business with any public entity in excess of the threshold amount provided in Section 287.017 [F.S.] for Category Two [\$35,000.00] for a period of thirty-six (36) months from the date of being placed on the convicted contractors list.

Acknowledged by:

Kiewit Infrastructure South Co.

Firm Name



Signature

Timothy J. Cleary | Senior Vice President

Name and Title (Print or Type)

04/09/2025

Date

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**NOTIFICATION OF PUBLIC RECORDS LAW PERTAINING TO PUBLIC CONTRACTS AND REQUESTS FOR
CONTRACTOR RECORDS PURSUANT TO CHAPTER 119, *FLORIDA STATUTES***

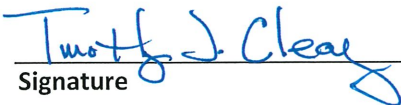
Pursuant to Chapter 119, *Florida Statutes*, Contractor shall comply with the public records law by keeping and maintaining public records required by the City of Delray Beach in order to perform the service. Upon request from the City of Delray Beach' custodian of public records, contract shall provide the City of Delray Beach with a copy of the requested records or allow the records to be inspected or copied within a reasonable time at a cost that does not exceed the cost provided in Chapter 119, *Florida Statutes* or as otherwise provided by law. Contractor shall ensure that public records that are exempt or confidential and exempt from public records disclosure requirements are not disclosed except as authorized by law for the duration of the contract term and following completion of the contract. If the Contractor does not transfer the records to the City of Delray Beach. Contractor upon completion of the contract, shall transfer, at no cost, to the City of Delray Beach all public records in possession of the Contractor or keep and maintain public records required by the City of Delray Beach in order to perform the service. If the Contractor transfers all public records to the City of Delray Beach upon completion of the contract, the Contractor shall destroy any duplicate public records that are exempt or confidential and exempt from public records disclosure requirements. If the Contractor keeps and maintains public records upon completion of the contract, the Contractor shall meet all applicable requirements for retaining public records. All records stored electronically must be provided to the City of Delray Beach, upon request from the City of Delray Beach' custodian of public records, in a format that is compatible with the information technology systems of the City of Delray Beach.

IF CONTRACTOR HAS QUESTIONS REGARDING THE APPLICATION OF CHAPTER 119, *FLORIDA STATUTES*, TO THE CONTRACTOR'S DUTY TO PROVIDE PUBLIC RECORDS RELATING TO THIS CONTRACT, CONTACT THE CUSTODIAN OF PUBLIC RECORDS AT OFFICE OF THE CITY CLERK LOCATED AT 100 NW 1ST AVENUE, DELRAY BEACH, FLORIDA 33444, PHONE NUMBER (561) 243-7000, EMAIL ADDRESS: JOHNSONK@MYDELRAYBEACH.COM.

Acknowledged:

Kiewit Infrastructure South Co.

Firm Name


Signature

Timothy J. Cleary | Senior Vice President

Name and Title (Print or Type)

04/09/2025

Date


DRUG-FREE WORKPLACE

Kiewit Infrastructure South Co. is a drug-free workplace and has
(Company Name)
a substance abuse policy in accordance with and pursuant to Section 440.102, *Florida Statutes*.

Acknowledged by:

Kiewit Infrastructure South Co.

Firm Name


Signature

Timothy J. Cleary | Senior Vice President

Name and Title (Print or Type)

04/09/2025

Date

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NON-COLLUSION AFFIDAVIT

STATE OF Georgia
COUNTY OF Fayette

Before me, the undersigned authority, personally appeared Timothy J. Cleary, who, after being by me first duly sworn, deposes and says of his/her personal knowledge that:

a. He/She is Senior Vice President of Kiewit Infrastructure South Co., the Proposer that has submitted a Proposal to perform work for the following:

RFQ No.: 19-015 Title: Tropic Isle Neighborhood Improvements

b. He/She is fully informed respecting the preparation and contents of the attached Request for Qualifications, and of all pertinent circumstances respecting such solicitation.

Such Proposal is genuine and is not a collusive or sham Proposal.

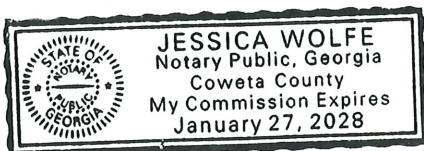
c. Neither the said Proposer nor any of its officers, partners, owners, agents, representatives, employees, or parties in interest, including this affiant, has in any way colluded, conspired, connived, or agreed, directly or indirectly, with any other Proposer, firm, or person to submit a collusive or sham Proposal in connection with the solicitation and contract for which the attached Proposal has been submitted or to refrain from proposing in connection with such solicitation and contract, or has in any manner, directly or indirectly, sought by agreement or collusion or communication or conference with any other Proposer, firm, or person to fix the price or prices in the attached Proposal or any other Proposer, or to fix any overhead, profit, or cost element of the Proposal price or the Proposal price of any other Proposer, or to secure through any collusion, conspiracy, connivance, or unlawful agreement any advantage against the City or any person interested in the proposed contract.

d. The price or prices quoted in the attached Proposal are fair and proper and are not tainted by any collusion, conspiracy, connivance, or unlawful agreement on the part of the Proposer or any of its agents, representatives, owners, employees, or parties in interest, including this affiant.

Timothy J. Cleary
Signature

Subscribed and sworn to (or affirmed) before me this 9th day of April 2025, by Timothy J. Cleary, who is personally known to me or who has produced _____ as identification.

SEAL



Notary Signature Jessica Wolfe
Notary Name: Jessica Wolfe
Notary Public (State): Georgia
My Commission No: N/A
Expires on: 1.27.2028

TRUTH – IN – NEGOTIATION CERTIFICATE

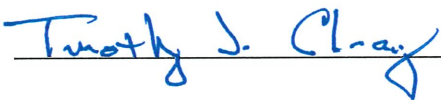
The undersigned warrants (i) that it has not employed or retained any company or person, other than bona fide employees working solely for the undersigned, to solicit or secure the Agreement and (ii) that it has not paid or agreed to pay any person, company, corporation, individual, or firm other than its bona fide employees working solely for the undersigned or agreed to pay any fee, commission, percentage, gift, or any other consideration contingent upon or resulting from the award or making of the Agreement.

The undersigned certifies that the wage rates and other factual unit costs used to determine the compensation provided for in the Agreement are accurate, complete, and current as of the date of the Agreement.

Name: Timothy J. Cleary

Title: Senior Vice President

Date: 04/09/2025

Signature: 

(The remainder of this page is intentionally left blank)

SCRUTINIZED COMPANY CERTIFICATION

This certification is required pursuant to Florida State Statute Section 287.135.

As of July 1, 2011, a company that, at the time of bidding or submitting a proposal for a new contract or renewal of an existing contract, is on the Scrutinized Companies with Activities in Sudan List or the Scrutinized Companies with Activities in the Iran Petroleum Energy Sector List is ineligible for, and may not bid on, submit a proposal for, or enter into or renew a contract with an agency or local governmental entity for goods or services of \$1 million or more.

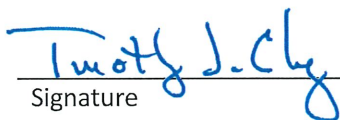
Companies must complete and return this form with its response.

Company: Kiewit Infrastructure South Co. FID or EIN No.: 47-0530367

Address: 6301 NW 5th Way, Suite 3200

City: Ft. Lauderdale State: FL Zip: 33309

I, Timothy J. Cleary, as a representative of Kiewit Infrastructure South Co.
certify and affirm that this company is not on the Scrutinized Companies with Activities in Sudan List or
the Scrutinized Companies with Activities in the Iran Petroleum Energy Sector List.



Signature

Senior Vice President

Title

Timothy J. Cleary

Printed Name

04/09/2025

Date

AFFIDAVIT REGARDING THE USE OF COERCION FOR LABOR AND SERVICES

Vendor Name: Kiewit Infrastructure South Co.

Vendor FEIN: 47-0530367

Vendor's Authorized Representative Name and Title:

Timothy J. Cleary | Senior Vice President

Address: 6301 NW 5th Way, Suite 3200

City: Ft. Lauderdale State: FL Zip: 33309

Phone Number: (770) 487-2300 Email Address: Tim.Cleary@kiewit.com

Florida Statute §787.06(13) requires all nongovernmental entities executing, renewing, or extending a contract with a governmental entity to provide an affidavit signed by an officer or representative of the nongovernmental entity under penalty of perjury that the nongovernmental entity does not use coercion for labor or services as defined in that statute. The City of Delray Beach, Florida is a governmental entity for the purposes of this statute.

As the officer or representative of the company, I certify that the company identified above does not:

- Use or threaten to use physical force against any person;
- Restrain, isolate, or confine or threaten to restrain, isolate, or confine any person without lawful authority and against his or her will;
- Use lending or other credit methods to establish a debt by any person when labor or services are pledged as a security for the debt, if the value of the labor or services as reasonably assessed is not applied towards the liquidation of the debt, the length and nature of the labor or services are not respectively limited and defined;
- Destroy, conceal, remove, confiscate, withhold, or possess any actual or purported passport, visa, or other immigration document, or any other actual or purported government identification, of any person;
- Cause or threaten to cause financial harm to any person;
- Entice or lure any person by fraud or deceit;
- Provide controlled substances as outlined in Schedule I or Schedule II of Florida State Statute §893.03 to any person for the purpose of exploitation of that person.

Under penalties of perjury, I declare that I have read the foregoing document and the at the facts stated in it are true.

Signature:  (Authorized Signature)

Print Name and Title: Timothy J. Cleary | Senior Vice President

Date: 04/09/2025

SECTION 10: SOLICITATION SUMMARY

PURCHASING AND CONTRACT ADMINISTRATION DIVISION

**The City of Delray Beach
100 NW 1st Avenue
Delray Beach, FL 33444**

SOLICITATION SUMMARY

IMPORTANT NOTICE

The information you provide on this page will be read aloud at the PUBLIC OPENING for this solicitation. It is VERY IMPORTANT that the summary information you provide below is exactly the same information contained in your Proposal. If subsequent to the opening of Proposals, the City determines that the information contained in the electronic version of your Proposal is different from the information on this Solicitation Summary form, the City reserves the right to deem your Proposal NON-RESPONSIVE and remove your Proposal from further evaluation and consideration for contract award.

PROPOSAL INFORMATION

Proposal Number: RFQ 2025-023

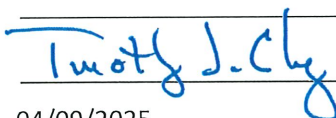
Title: Tropic Isle Neighborhood Improvements (Project No. 19-015) Construction Manager at Risk

Due Date and Time: **March 26, 2025 @ 2:00 PM (LOCAL TIME)**

Name of Proposer: Kiewit Infrastructure South Co.

Address: 6301 NW 5th Way, Suite 3200 Ft. Lauderdale, FL 33309

Contact Person: Timothy J. Cleary | Senior Vice President

Authorized Signature: 

Date: 04/09/2025

By signing and submitting this Solicitation Summary form, the Proposer affirms that the information provided above is an exact and correct summary of the information contained in the electronic version of the Proposer's Proposal to the City of Delray Beach.