

**1201 George Bush Blvd.**  
KEITH PROJECT NUMBER: 12771.00

# TRAFFIC IMPACT STATEMENT

DECEMBER 2022



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1201 GEORGE BUSH BLVD.  
DELRAY BEACH, FL 33483

# TRAFFIC IMPACT STATEMENT

*Prepared For:*

Stamm Development Group  
1835 Market Street, Suite 625  
Philadelphia, PA 19103

*Prepared By:*



301 East Atlantic Boulevard  
Pompano Beach, Florida 33060

## Engineer's Certification

I, Chris Rogers, PE, PE number 67359, certify that I currently hold an active Professional Engineer's License in the State of Florida, and I am competent through education or experience to provide engineering services in the civil and traffic engineering disciplines contained in this report. I further certify that this report was prepared by me or under my responsible charge as defined in Chapter 61G15-18.001 F.A.C. and that all statements, conclusions and recommendations made herein are true and correct to the best of my knowledge and ability.

Project Description: Traffic Impact Statement – 1201 George Bush Blvd. Project

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Chris Rogers, P.E.  
Florida Registration P.E. No. 67359  
KEITH

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# 1 PROJECT OVERVIEW

## 1.1 Introduction

KEITH was retained by the *Stamm Development Group* to complete a Traffic Impact Statement for the proposed (3) Three Story Multi-Family Residences at 1201 George Bush Blvd. in Delray Beach, Florida. The Traffic Impact Statement was prepared following the City of Delray Beach Land Development Regulations and the Palm Beach County's Unified Land Development Code (ULDC) Article 12 Traffic Performance Standards (TPS) *Art. 12.D.1.C, No Study Needed*, which are included in **Appendix A**. **Figure 1.1** shows the project location, and a conceptual site plan is included in **Appendix B**.



**Figure 1.1: Project Location**

## 1.2 Existing Conditions

The existing site contains of an office building, paved parking areas and landscaped areas. The Parcel Control Numbers (PCN) for the site is 12-43-46-09-05-000-0060. A copy of the existing property information for the parcel is included in **Appendix C**.

## 1.3 Existing Roadway Characteristics

**George Bush Blvd.:** This roadway facility is a two-lane, undivided road and functionally classified as urban collector. It runs in the east/west direction and is currently maintained by Delray Beach City. The current posted speed limit is 30 miles per hour.

**Andrews Avenue:** This roadway facility is a two-lane, undivided local road. It runs in the north/south direction and is currently maintained by Delray Beach City. The current posted speed limit is 25 miles per hour.

**Pelican Ln:** This roadway facility is a two-lane, undivided local road. It runs in the east/west direction and is currently maintained by Delray Beach City. The current posted speed limit is 25 miles per hour.

**SRA1A:** This roadway facility is a two-lane, undivided road and functionally classified as urban collector. It runs in the north/south direction and is currently maintained by FDOT. The current posted speed limit is 35 miles per hour.

**Appendix D** includes a copy of the County's Roadway Data.

## 2 PROPOSED DEVELOPMENT

### 2.1 Proposed Land Use

The proposed development comprises (3) Three Story Multi-Family Residences. A copy of the conceptual site plan is included in **Appendix B**.

### 2.2 Trip Generation

The Palm Beach County (PBC) Trip Generation Rates (Updated July 25, 2022) was utilized to estimate the proposed development's daily and peak hour trips. Based on the proposed land use, the Small Office Building land use code (LUC) 712 and Multifamily Housing (Low-Rise) (LUC) 220 was applied in the trip generation calculations. The following equations were used for Daily, A.M. peak, and P.M. peak hour periods:

- **PBC Trip Generation Rates – LUC 712 Small Office Building:**

Daily:  $T=14.39(X)$  (Directional Distribution: 50% In / 50% Out)

AM Peak Hour:  $T=1.67(X)$  (Directional Distribution: 82% In / 18% Out)

PM Peak Hour:  $T=2.16(X)$  (Directional Distribution: 34% In / 66% Out)

- **PBC Trip Generation Rates – LUC 220 Multifamily Housing (Low-Rise):**

Daily:  $T=6.74(X)$  (Directional Distribution: 50% In / 50% Out)

AM Peak Hour:  $T=0.40(X)$  (Directional Distribution: 24% In / 76% Out)

PM Peak Hour:  $T=0.51(X)$  (Directional Distribution: 63% In / 37% Out)

The trip generation results are summarized in **Table 2.1** for the Daily, A.M. peak hour, and P.M. peak hour, and a copy of the PBC Trip Generation Rates is included in **Appendix E**. The proposed development is expected to generate no additional trips. According to the PBC's adopted trip generation rates threshold for residential projects, a Traffic Impact Study will not be required if the development is expected to generate less than or equal to 20 Gross Peak Hour Trips. Therefore, only a Traffic Statement is required for this proposed development.

**Table 2.1: Trip Generation**

Daily Trip Generation									
Land Use	ITE Code	Intensity		Trip Generation Rate	Directional Distribution		Total Calculated Trips		
		Quantity	Units		% Entering	% Exiting	Entry	Exit	Total
<b>Existing Development</b> Small Office Building	712	3984	Square Feet	T = 14.39(X)	50%	50%	29	29	58
<b>Existing Daily Trips</b>							<b>29</b>	<b>29</b>	<b>58</b>
<b>Proposed Development</b> Multifamily Housing (Low-Rise)	220	3	Dwelling Units	T = 6.74 (X)	50%	50%	11	11	22
<b>Proposed Daily Trips</b>							<b>11</b>	<b>11</b>	<b>22</b>
<b>Estimated Net New Daily Trips</b>							<b>-18</b>	<b>-18</b>	<b>-36</b>

Source: Palm Beach County Trip Generation Rates (Updated July 25, 2022).

A.M. Peak Hour Trip Generation									
Land Use	ITE Code	Intensity		Trip Generation Rate	Directional Distribution		Total Calculated Trips		
		Quantity	Units		% Entering	% Exiting	Entry	Exit	Total
<b>Existing Development</b> Small Office Building	712	3984	Square Feet	T = 1.67 (X)	82%	18%	7	1	8
<b>Existing Daily Trips</b>							<b>7</b>	<b>1</b>	<b>8</b>
<b>Proposed Development</b> Multifamily Housing (Low-Rise)	220	3	Dwelling Units	T = 0.4 (X)	24%	76%	0	2	2
<b>Proposed Daily Trips</b>							<b>0</b>	<b>2</b>	<b>2</b>
<b>Estimated Net New Daily Trips</b>							<b>-7</b>	<b>1</b>	<b>-6</b>

Source: Palm Beach County Trip Generation Rates (Updated July 25, 2022).

P.M. Peak Hour Trip Generation									
Land Use	ITE Code	Intensity		Trip Generation Rate	Directional Distribution		Total Calculated Trips		
		Quantity	Units		% Entering	% Exiting	Entry	Exit	Total
<b>Existing Development</b> Small Office Building	712	3984	Square Feet	T = 2.16 (X)	34%	66%	3	7	10
<b>Existing Daily Trips</b>							<b>3</b>	<b>7</b>	<b>10</b>
<b>Proposed Development</b> Multifamily Housing (Low-Rise)	220	3	Dwelling Units	T = 0.51 (X)	63%	37%	1	1	2
<b>Proposed Daily Trips</b>							<b>1</b>	<b>1</b>	<b>2</b>
<b>Estimated Net New Daily Trips</b>							<b>-2</b>	<b>-6</b>	<b>-8</b>

Source: Palm Beach County Trip Generation Rates (Updated July 25, 2022).



### **3 CONCLUSION**

A Traffic Impact Statement has been prepared to evaluate and estimate any traffic related impacts of the proposed development following the City of Delray Beach Land Development Regulations and the Palm Beach County's Unified Land Development Code (ULDC) Article 12 Traffic Performance Standards (TPS). Based on the trip generation analysis, the proposed development is expected to generate no additional trips during the AM and PM peak hour periods, which is less than 20 Gross Peak Hour Trips based on the PBC's Trip Generation Rates; therefore, a Traffic Impact Study is not required. Additionally, the proposed development is not expected to adversely impact any surrounding roadways within the project area.

# APPENDIX A

## Palm Beach County's Unified Land Development Code (ULDC)

judgment shall be used to take into account special circumstances such as the opening of a parallel road or a high traffic generation that may distort the growth trend. For Projects with a lengthy buildout time (five years or more) an area-wide growth rate using a number of locations in the tables may be appropriate. No growth rate less than zero percent may be used without approval of the County Engineer when the growth rate is a negative. Zero percent shall be used unless approved by the County Engineer. [Ord. 2006-043] [Ord. 2007-013]

**c. TPS Database**

Using the TPS Database, all traffic from the unbuilt portion of Projects which have received a concurrency reservation prior to the County Engineer's approval of the proposed Project's traffic study which will add significant trips to any Link within the proposed Project's Radius of Development Influence during the Buildout Period of proposed Project shall be specifically accounted for in projecting Traffic for Test 1. For Major Intersections, the TPS Database shall specifically account for all Project Traffic volumes if at least one approach to the intersection has a Project Traffic volume greater than or equal to one percent of the adopted LOS D. No double counting of trips shall occur. For Test 2, only the traffic generated from the unbuilt portions of the Projects as set forth above which are projected to be built during the Five-Year Analysis Period shall be considered. [Ord. 2005-002] [Ord. 2006-043] [Ord. 2009-040]

**5. Assured Construction**

Assured Construction shall be considered completed as scheduled at the time of submittal of the Traffic Impact Study for the purpose of preparation of the study. Whether it is in fact Assured Construction and the timing of the Assured Construction shall be subject to the confirmation of the County Engineer. The Traffic Impact Study shall specifically identify the need for phasing based on Assured Construction. [Ord. 2007-013]

**Section 2 Conditions**

The Concurrency Reservation or Site Specific Development Order shall contain such conditions as are necessary to ensure compliance with this Article. The Local Governments, including the legislative and administrative boards, the DRO, and officials, issuing Concurrency Reservations or Site Specific Development Orders are authorized to, and shall, impose such conditions. The Local Governments including the legislative and administrative boards, the DRO, and officials shall require where necessary to ensure compliance with this Section that an Agreement be executed prior to the issuance of the Site Specific Development Order. Performance Security shall be required to ensure compliance with the conditions or performance under the Agreement or Condition of Approval. The Agreement or Conditions of Approval shall be binding on the owner, its successors, assigns, and heirs; and it, or notice thereof, shall be recorded in the Official Records of the Clerk of the Circuit Court in and for PBC, Florida.

**CHAPTER D PROCEDURE**

**Section 1 Required Submission of Impact Study**

**A. Application Procedure**

Prior to acceptance of any application for a Site Specific Development Order in the Unincorporated Area, or issuance of a Site Specific Development Order in the Incorporated Area, a non-refundable application fee established by the BCC from time to time to defray the actual cost for processing the application, shall be submitted along with the Traffic Impact Study or documentation sufficient to establish that the application is not subject to the standards of this Article.

In order to receive a time extension pursuant to [Art. 2.E, Monitoring of Development Orders \(DOs\) and Conditions of Approval](#), the Applicant shall be required to submit either: [Ord. 2007-013]

1. A new Traffic Impact Study that meets the standards of this Article in effect at the time the extension is requested; or [Ord. 2007-013]
2. Documentation sufficient to establish that the Project with the additional time provided by the extension meets the standards of this Article in effect at the time the extension is requested. [Ord. 2007-013]

**B. Review by County Engineer**

The County Engineer or Municipal Engineer, as applicable, shall review the information submitted pursuant to this Article and determine whether the proposed Project complies with this Article. In the Unincorporated Area the County Engineer shall coordinate with the Planning Division whether the Site Specific Development Order meets the other Concurrency Requirements of the Plan. The procedures set forth in the Adequate Public Facilities Chapter, shall control; except as to any appeals from this Article, in which

case [Art. 12.F, Appeals](#), of this Article shall control. Nothing herein or in the Adequate Public Facilities Chapter shall preclude direct informal communication between the County Engineer and the Applicant or his agents. In the Unincorporated Area, a statement that an application for a Site Specific Development Order is being considered shall be sent to any Municipality within the proposed Project's Radius of Development Influence 30 days prior to the issuance of the Site Specific Development Order for all proposed Projects generating more than 100 Gross Peak Hour Trips. The statement shall be sent by U.S. Mail, or hand delivered.

**C. No Study Needed**

**1. Residential**

New residential Projects generating fewer than or equal to 20 Gross Peak Hour Trips based on PBC's adopted trip generation rates shall not be required to submit a Traffic Impact Study. The Net Trips shall be distributed over the Major Thoroughfare system by the County Engineer in accordance with generally accepted traffic engineering principles.

**2. Non-Residential**

Non-residential Projects generating less than or equal to 20 Gross Peak Hour Trips based on PBC's adopted trip generation's rates shall not be required to submit a Traffic Impact Study. The Net Trips shall be distributed over the Major Thoroughfare system by the County Engineer or in accordance with generally accepted traffic engineering principles.

**3. Amendments**

Projects generating less than or equal to 20 Gross Peak Hour Trips based on PBC's adopted trip generations rates shall not be required to submit a Traffic Impact Study for an amendment, provided the total Project, including the amendment, does not exceed 20 Gross Peak Hour Trips. The Net Trips shall be distributed over the Major Thoroughfare system by the County Engineer in accordance with generally accepted traffic engineering principles.

**Section 2 Review of Traffic Impact Study**

**A. County Engineer Review**

On all proposed Projects having more than 100 Gross Peak Hour Trips, the County Engineer shall have sole authority for reviewing Traffic Impact Studies for purposes of determining compliance with this Article.

**B. Municipal Review**

On all other proposed Projects the Municipality shall perform such review unless the Municipality provides in writing, delivered to the County, that the Municipality elects to require review by the County Engineer. If the Municipality elects to perform the review, it shall be done by a Municipal Engineer. The review shall be in accordance with the requirements of this Article. In the case of Municipal review, 30 days prior to approval of the application for the Site Specific Development Order, the Traffic Impact Study, along with the determination of the reviewing traffic engineer, shall be sent to the County Engineer, c/o Traffic Division, 2300 North Jog Road, West Palm Beach, Florida, 33411. A statement that the Municipality is considering an application for a Site Specific Development Order shall also be sent to any Municipality within the Project's Radius of Development Influence involved 30 days prior to issuance of the Site Specific Development Order for all proposed Projects generating more than one 100 Gross Peak Hour Trips. All documents under this Article shall be sent by U.S. Mail, or hand delivered.

**C. Prohibitions**

1. In the case of all Site Specific Development Orders issued by the DRO, no application shall be certified for inclusion on the DRO agenda if issuance of the Site Specific Development Order would be prohibited by this Article.
2. In the case of all other Site Specific Development Orders in the Unincorporated Area, no application shall be accepted if issuance of the Site Specific Development Order would be prohibited by this Article.
3. In all cases in the Unincorporated Area if the Site Specific Development Order does not meet the other Concurrency Requirements of the Plan, no application shall be certified for inclusion on an agenda of a reviewing body or accepted, as the case may be, except as otherwise provided by [Art. 2.F, Concurrency \(Adequate Public Facility Standards\)](#).
4. In the case of all Site Specific Development Order in the Incorporated Area, no Site Specific Development Order shall be issued if such issuance would be prohibited by this Article. In no case shall the Site Specific Development Order be issued prior to 30 days following delivery of the notice in accordance with [Art. 12.D.2.B, Municipal Review](#).

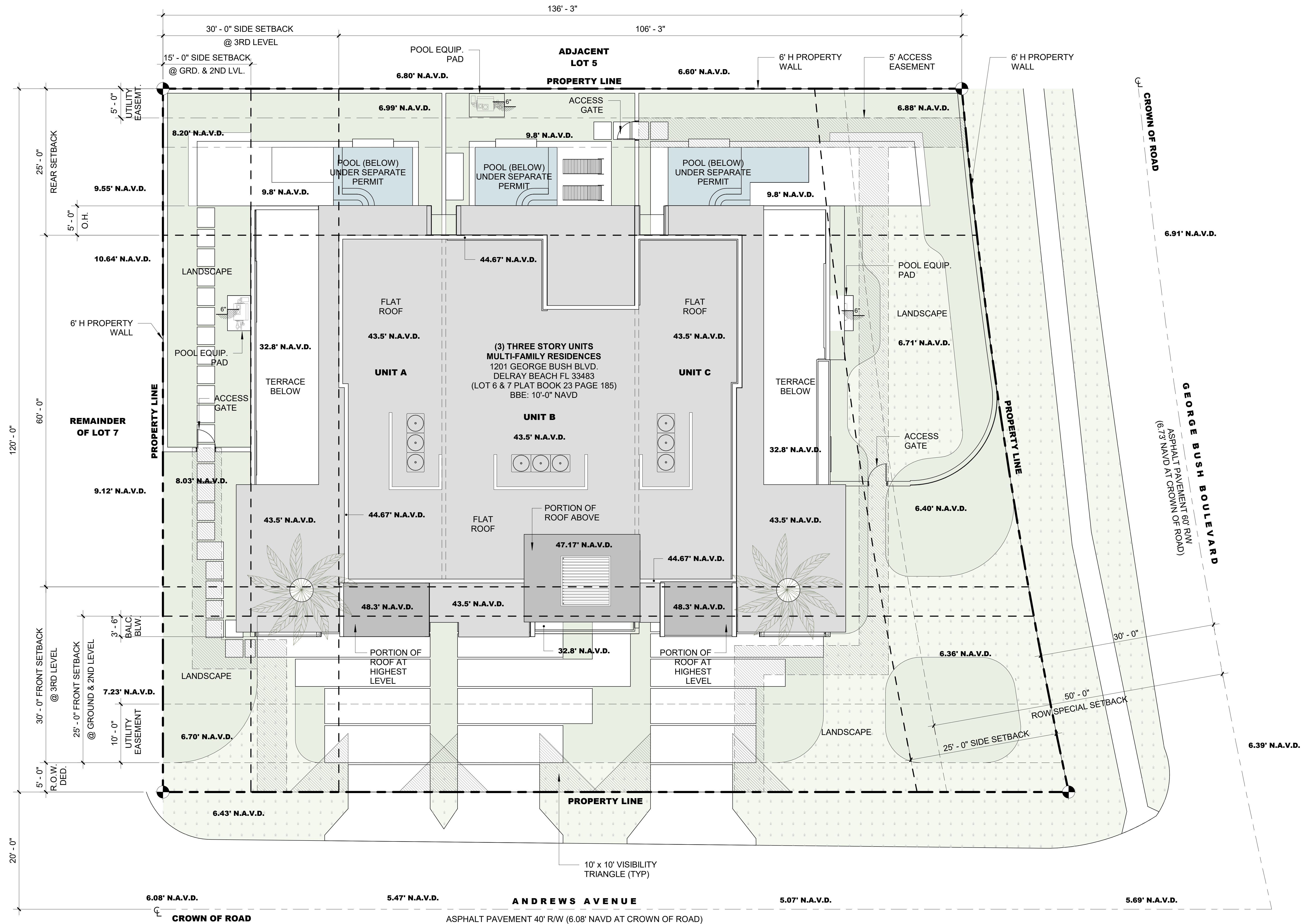
**D. Appeals**

Determinations of the County Engineer or Municipal Engineer must be in writing and any denial shall state the reasons thereof. Determinations of denial may be appealed pursuant to [Art. 12.F, Appeals](#).

# APPENDIX B

## Conceptual Site Plan





# APPENDIX C

## Existing Property Information

**Location Address** 1201 GEORGE BUSH BLVD  
**Municipality** DELRAY BEACH  
**Parcel Control Number** 12-43-46-09-05-000-0060  
**Subdivision** BLUE SE  
**Official Records Book** 32460 **Page**47  
**Sale Date** APR-2021  
**Legal Description** BLUE SEAS LT 6 & S 54.5 FT OF LT 7

**Owners**  
 1201 GEORGE BUSH BLVD LLC

**Mailing address**  
 1835 MARKET ST STE 625  
 PHILADELPHIA PA 19103 2915

Sales Date	Price	OR Book/Page	Sale Type	Owner
APR-2021	\$2,100,000	32460 / 00047	WARRANTY DEED	1201 GEORGE BUSH BLVD LLC
FEB-2013	\$1,250,000	25835 / 01277	WARRANTY DEED	1201 GEORGE BUSH LLC
JAN-1977	\$100	02767 / 01662		
JAN-1975	\$62,000	02433 / 00032	QUIT CLAIM	
JAN-1974	\$100	02276 / 01724		

1 2

No Exemption Information Available.

**Number of Units** 0 **\*Total Square Feet** 3984 **Acres** 0.3975  
**Use Code** 1700 - OFFICE BLDG-NON MEDICAL 1 TO 3 STORIES **Zoning** RO - RO - RESIDENTIAL/OFFICE ( 12-DELRAY BEACH )

Tax Year	2022	2021	2020
<b>Improvement Value</b>	\$493,508	\$421,914	\$428,064
<b>Land Value</b>	\$1,212,050	\$983,146	\$983,146
<b>Total Market Value</b>	\$1,705,558	\$1,405,060	\$1,411,210

All values are as of January 1st each year

Tax Year	2022	2021	2020
<b>Assessed Value</b>	\$1,705,558	\$1,405,060	\$1,411,210
<b>Exemption Amount</b>	\$0	\$0	\$0
<b>Taxable Value</b>	\$1,705,558	\$1,405,060	\$1,411,210









Tax Year	2022	2021	2020
<b>Ad Valorem</b>	\$33,204	\$28,338	\$28,714
<b>Non Ad Valorem</b>	\$1,063	\$1,021	\$1,000
<b>Total tax</b>	\$34,267	\$29,359	\$29,714

# APPENDIX D

## Palm Beach County's Roadway Data



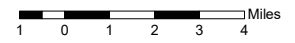
**MAP TE 3.1  
FUNCTIONAL  
CLASSIFICATION OF ROADS**

-  Urban Principal Arterial (U-PA)
-  Rural Principal Arterial (R-PA)
-  Urban Minor Arterial (U-MA)
-  Rural Minor Arterial (R-MA)
-  Urban Collector (U-COLL)
-  Rural Major Collector (R-MAJ)
-  Rural Minor Collector (R-MIN)
-  Undefined

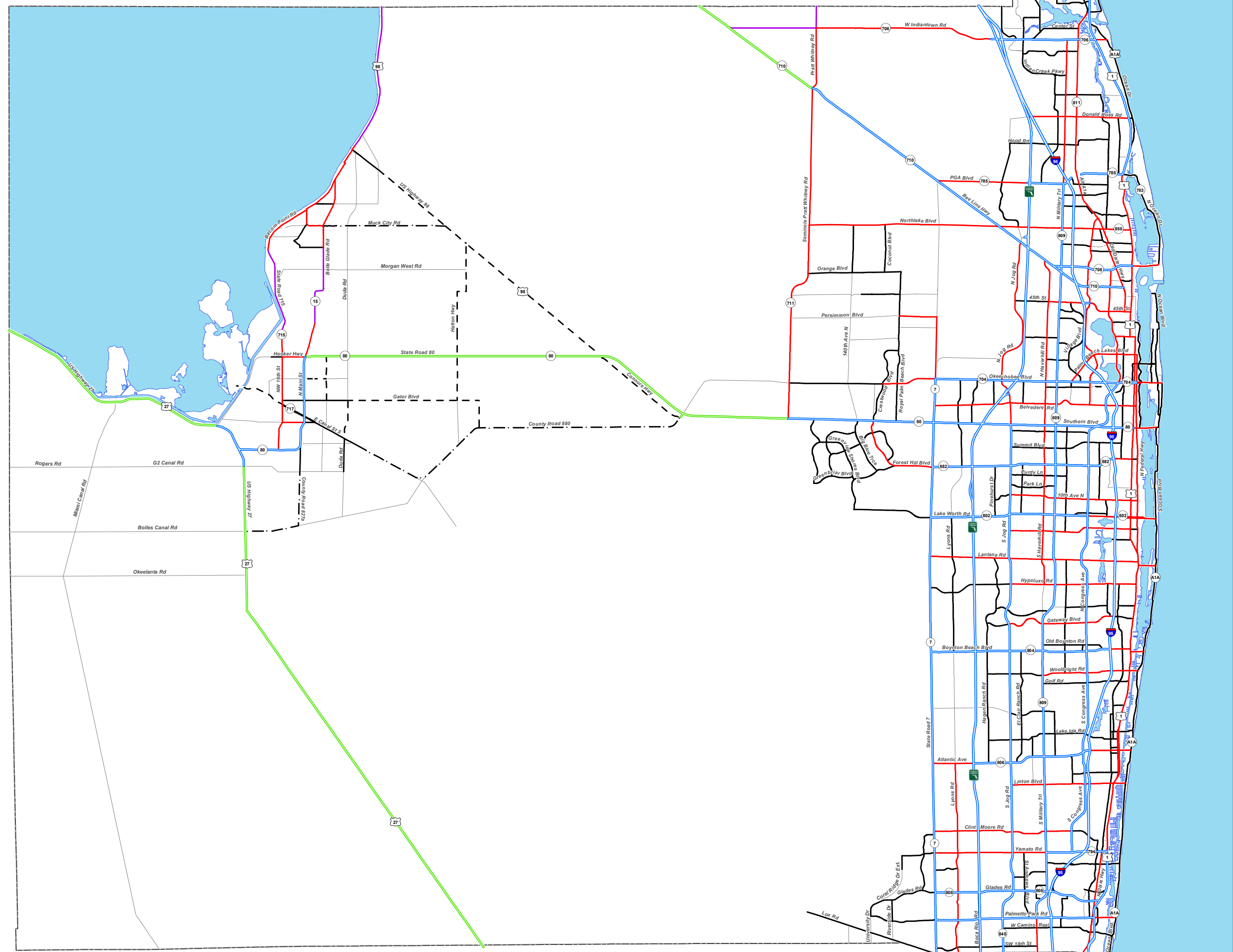
SOURCES:  
 Palm Beach County Planning Division  
 Engineering & Public Works Dept. Geoprocessing Section  
 Palm Beach County Metropolitan Planning Organization  
 Federal Highway Administration "Palm Beach County 2000 - 2010  
 Federal Functional Classification and Urban Area Boundaries" Map  
 Last Amended In Round 08-1 by Ord. 2008-029



**PALM BEACH COUNTY  
COMPREHENSIVE PLAN  
MAP SERIES**



Effective Date: 11/07/2008  
 Filename: N:\Map Series\MXDs\Adopted  
 Contacts: PBC Planning Department





# APPENDIX E

## Palm Beach County's Trip Generation Rates

## Palm Beach County Trip Generation Rates

(Must be used with traffic studies submitted to the County on or after 9/1/2022. However, immediate use is highly recommended)

Gr	Landuse	ITE Code	Unit	Daily Rate/Equation	Pass-By %	AM Peak Hour		PM Peak Hour	
						In/Out	Rate/Equation	In/Out	Rate/Equation
Industrial	General Light Industrial	110	1000 S.F.	4.87	10%	88/12	0.74	14/86	0.65
	Manufacturing	140	1000 S.F.	4.75	10%	76/24	0.68	31/69	0.74
	Warehouse	150	1000 S.F.	1.71	10%	77/23	0.17	28/72	0.18
	Mini-Warehouse/SS	151	1000 S.F.	1.45	10%	59/41	0.09	47/53	0.15
	HCF Center Warehouse - Non Sort	155	1000 S.F.	1.81	10%	81/19	0.15	39/61	0.16
Residential	Single Family Detached	210	Dwelling Unit	10	0%	26/74	0.7	63/37	0.94
	Multifamily Low-Rise Housing upto 3 story (Apartment/Condo/TH)	220	Dwelling Unit	6.74	0%	24/76	0.4	63/37	0.51
	Multifamily Mid-Rise Housing 4-10 story (Apartment/Condo/TH)	221	Dwelling Unit	4.54	0%	23/77	0.37	61/39	0.39
	55+ SF Detached	251	Dwelling Unit	4.31	0%	33/67	0.24	61/39	0.30
	55+ SF Attached	252	Dwelling Unit	3.24	0%	34/66	0.2	56/44	0.25
	Congregate Care Facility	253	Dwelling Unit	2.21	0%	58/42	0.08	49/51	0.18
	Assisted Living Facility	254	Beds	2.6	0%	60/40	0.18	39/61	0.24
Ldg	Hotel	310	Rooms	7.99	10%	56/44	0.46	51/49	0.59
Rec	Golf Course	430	Holes	30.38	5%	79/21	1.76	53/47	2.91
	Health/Fitness Club	492	1000 S.F.	32.93	5%	51/49	1.31	57/43	3.45
Institutional	Elementary School	520	Students	2.27	0%	54/46	0.74	46/54	0.16
	Middle/Junior School	522	Students	2.1	0%	54/46	0.67	48/52	0.15
	High School	525	Students	1.94	0%	68/32	0.52	48/52	0.14
	Private School (K-8)	530	Students	3.17 <sup>a</sup>	0%	56/44	1.01	46/54	0.26
	Private School (K-12)	532	Students	2.48	0%	63/37	0.79	43/57	0.17
	Church/Synagogue <sup>b</sup>	560	1000 S.F.	7.6	5%	62/38	0.32	44/56	0.49
	Day Care	565	Students	4.09	50%	53/47	0.78	47/53	0.79
	Library	590	1000 S.F.	72.05	10%	71/29	1	48/52	8.16
Med	Hospital	610	1000 S.F.	10.77	10%	67/33	0.82	35/65	0.86
	Nursing Home	620	Beds	3.06	10%	72/28	0.14	33/67	0.14
Office	General Office (10k-250k SF GFA) <sup>h</sup>	710	1000 S.F.	10.84	10%	88/12	1.52	17/83	1.44
	General Office (>250k SF GFA) <sup>h</sup>	710	1000 S.F.	$\text{Ln}(T) = 0.87 \text{Ln}(X) + 3.05$	10%	88/12	$\text{Ln}(T) = 0.86 \text{Ln}(X) + 1.16$	17/83	1.44
	Small Office Building (<=10k SF GFA)	712	1000 S.F.	14.39	10%	82/18	1.67	34/66	2.16
	Medical Office (Stand-Alone)	720	1000 S.F.	$T=42.97(X)-108.01$	10%	79/21	3.10	30/70	3.93
	Medical Office (Near Hospital)	720	1000 S.F.	31.86	10%	81/19	2.68	25/75	2.84
	Government Office	730	1000 S.F.	22.59	10%	75/25	3.34	25/75	1.71