



SITE PLAN REVIEW AND APPEARANCE BOARD STAFF REPORT

DEVELOPMENT SERVICES DEPARTMENT

100 NW 1ST AVENUE, DELRAY BEACH, FLORIDA 33444

PLANNING & ZONING DIVISION: (561) 243-7040 • BUILDING DIVISION: (561) 243-7200

SITE PLAN REVIEW AND APPEARANCE BOARD

Meeting: January 9, 2019 File No.: 2018-013-SPM-SPR-CL3 Application Type: Delray Oasis Business Park

General Data:

Applicant: David Marulli, DNA Properties

Agent: Roger Cope, Cope Architects, Inc.

Owner: Delray Oasis Business Park LLC

Location: 1201, 1203, 1205, 1235 and 1237 N. Federal Hwy (Unified Lots)

PCN: 12-43-46-09-08-001-0060, 12-43-46-09-08-001-0010, 12-43-46-09-12-000-0051

Property Size: 0.75 Acres

FLUM: General Commercial (GC)

Zoning: General Commercial (GC)

Adjacent Zoning:

- North: GC
- East: GC/ Multi-Family Resident Low Density (RL)
- South: GC / RL
- East: GC

Existing Land Use: Mixed Residential and Commercial Uses

Proposed Land Use: Commercial Uses



Location Map:
Delray Oasis
Business Park



Item before the Board:

The action before the Board is the approval of a Class III Site Plan Modification application associated with building use conversions, expansion of parking lot, site improvements, and architectural elevation changes for the Delray Oasis Business Park project pursuant to LDR Section 2.4.5 (G)(1)(c), including the following:

- ☐ Site Plan
- ☐ Landscape Plan
- ☐ Architectural Elevations

Recommendations by Separate Motions:

Site Plan:

Move approval of the Class III Site Plan Modification application (2018-013) for the **Delray Oasis Business Park**, by adopting the findings of fact and law contained in the staff report and finding that the request is consistent with the Comprehensive Plan and meets criteria set forth in Section 2.4.5(G)(5) and Chapter 3 of the Land Development Regulations.

**Note: If the site plan is denied, no further action shall be taken on the landscape plan or the architectural elevation plan due to potential inconsistencies.*

Landscape Plan:

Move approval of the Landscape Plan (2018-013) for the **Delray Oasis Business Park**, by adopting the findings of fact and law contained in the staff report and finding that the request and approval thereof is consistent with the Comprehensive Plan and meets criteria set forth in Sections 4.6.16 of the Land Development Regulations.

Architectural Elevations:

Move approval of the Architectural Elevations (2018-013) for the **Delray Oasis Business Park**, by adopting the findings of fact and law contained in the staff report and finding that the request meets criteria set forth in Section 4.6.18(E) of the Land Development Regulations.

Project Planner:

Kent Walia, AICP, Senior Planner;
waliaak@mydelraybeach.com,
561-243-7365

Review Dates:

SPRAB Board:
January 9, 2019

Attachments

1. Site Plans
2. Elevations
3. Landscape Plans



Notes:

1. Sewage calculations must be provided for engineering review prior to site plan certification.
2. Revise the survey to include the finish floor elevations of the adjacent buildings prior to site plan certification.
3. Revise the open space percentage indicated on the site plan to be consistent with the pervious percentage indicated on the landscape plan prior to site plan certification.
4. Revise the plans to provide a minimum of 5 ft. landscape barrier between the edge of the eastern parking row and east property line prior to site plan certification, in accordance with 4.6.16 (H)(3)(f).
5. Comply with Florida Building Code requirements for ADA vertical accessibility prior to issuance of a building permit for the second story building (1205 building).
6. Revise the photometric plan to comply with the LDR 4.6.8 (A) Table 2, prior to site plan certification.

Project Request:

The Class III Site Plan Modification request for Delray Oasis Business Center is associated with conversion of building uses, expansion of a parking lot, façade modifications and associated site improvements. The proposed use conversions are as follow:

Building	Existing Uses	Proposed Uses
1203 Building	Storage	Personal Service
1205 Building (2 nd Story)	Residential Unit	Personal Service
1235 Building	Commercial/ Residential Unit	Veterinary Clinic
1237 Building	Storage	Business Office

Currently, the development has 25 parking spaces on site. To accommodate the proposed use conversions, a total of 30 parking spaces is required and 30 spaces are proposed to be provided on site. The three parking spaces existing at the northeast side of the property (that back up into the alley) will be expanded into a parking lot with eight spaces. A bicycle rack adjacent to the 1235 building is proposed. Moreover, improvements to the existing landscaping are proposed to enhance the existing open space and to comply with code requirements. Architectural elevation changes are proposed to the 1237 building.

Background:

The subject site totaling 0.75 acres is located on unified lots at addresses 1201, 1203, 1205, 1235, and 1237 N. Federal Hwy, with Palm Beach County parcel numbers: 12-43-46-09-08-001-0060, 12-43-46-09-08-001-0010, and 12-43-46-09-12-000-0051 respectively. The development is situated on Lots 1,2,3,4,5,6 and 7 on Block 1 in the Kenmont subdivision, and Lots 6 and 7 Section 9 of the Harry Seemiller subdivision at the northeast corner of N. Federal Hwy and S. Lake Avenue, in the North Palm Trail neighborhood.

The 1235 building was constructed in 1940, the 1205 building was constructed in 1930, and the 1201 building was constructed in 1955. The property was annexed into the City of Delray Beach via Ordinance 145-88 in 1988. The property has a General Commercial (GC) zoning designation and a GC Future Land Use Map (FLUM) designation.

At its meeting on July 28, 1993, SPRAB approved a minor site plan modification (1993-097) to convert the upstairs office at the 1205 building to a 1,156 sf two-bedroom apartment, to construct a 160 sf., addition to the first-story portion of the building along with landscape and parking lot improvements to the site.

In 1995, the City approved the change of use to building 1235 from commercial to residential / commercial mixed-use.

At its meeting of December 1, 1999, the SPRAB approved a Class II Site Plan Modification (2000-028) associated with site improvements to the property located at 1235 N. Federal Hwy. According to the City's records, the front portion of the home, consisting of 1,218 sf., was converted to retail space and the remaining rear portion of the building, consisting of 425 sf., was utilized as a one-bedroom dwelling unit. The 400 sq. ft. garage was utilized for storage.

In 2013 a permit application (13-147993) was granted to the property located at 1201 N. Federal Hwy for the installation of a pre-fabricated storage unit. No records were found indicating a site plan application approval for this addition.



On May 24, 2017, the SPRAB approved a Class I Site Plan Modification (2017-018) for façade improvements associated with buildings 1201, 1203, 1205 and 1235.

On January 22, 2018, the Planning and Zoning Board recommended approval of a Conditional Use request (2017-256) to the City Commission to establish a veterinary clinic at the 1235 N. Federal Highway building. Consequently, on April 3, 2018, the City Commission approved the Conditional Use with conditions. The conditions were that the applicant shall submit an application for the approval of required improvements resulting from the new veterinary use, such as adding adequate parking spaces, introducing a bike rack, installation of additional hedges between the subject commercial property and the abutting residentially zoned district and provisions to accommodate refuse disposal.

Site Plan Analysis:

Compliance with the Land Development Regulations:

Items identified in the Land Development Regulations shall specifically be addressed by the body taking final action on the site and development application/request.

Establishment of the Proposed Use/ Certificate of Occupancy:

Per LDR Section 2.4.6(C), a certificate of occupancy is required prior to the establishment of use on any site or occupancy of a structure. Prior to issuance of an occupancy permit, all conditions associated with this development approval shall be met.

Base District Requirements:

- **LDR Section 4.3.4 (K) Development Standards Matrix:** As indicated below, the proposed 30% open space percentage exceeds LDRs requirements even with the proposed addition of the new parking lot area:

<u>Zoning</u> General Commercial (GC)	Min. Required	Proposed
Front (S. Federal Hwy)	5'*	4" *
Side Interior	10'	21'- 6"
Side Street (Bond Way)	10'	10" *
Rear	10'	2'*
Open Space:	25%	30. 7% (10,106) **

* Existing legal non-conformity subject to LDR Article 1.3, Nonconforming Uses, Lots and Structures.

**Revise the open space percentage indicated on the site plan to be consistent with the previous percentage indicated on the landscape plan prior to site plan certification.

Special District Boundary Treatment:

- Pursuant to LDR Section 4.6.4 (A)(2)(a), *where the rear or side of commercially zoned property directly abuts residentially zoned property without any division or separation between them, such as a street, alley, railroad, waterway, park, or other public open space, the commercially zoned property shall provide a ten-foot building setback from the property line located adjacent to the residentially zoned property. In addition, either a solid finished masonry wall six feet in height, or a continuous hedge at least four and one-half feet in height at the time of installation, shall be located inside and adjacent to the portion of the boundary line of the commercially zoned property which directly abuts the residentially zoned property. If a wall is used, it shall have only those openings as required by other city codes to meet hurricane or severe storm construction standards. No walkways or other pedestrian ways shall be located through the wall or hedge.*
 - ✓ With respect to the residential zoning abutting the existing commercial site, the existing building to be utilized as a veterinary clinic is located approximately 90 feet to the west of the residential zoning property and the building proposed to be utilized as a business office is located approximately 60 feet. Currently, a solid privacy fence is existing on the adjacent residentially zoned property to the east. To comply with this requirement, a continuous hedge measuring four and one-half feet in height will be installed on the east side of the property to provide additional buffering from the adjacent residential property. Thus, this requirement is met.

Refuse Facilities:

- Pursuant to LDR Section 4.6.6(C)(1), **Dumpsters, Recycling Containers, and similar service areas must be enclosed on three sides with vision obscuring gates on the fourth side, unless such areas are not visible from any adjacent public**



right-of-way.

- ✓ The existing refuse area, located on the east side of the property as shown on page SP1, and is enclosed on three sides by an existing masonry wall, has a wood gate on the fourth side and is screened with landscaping on the two visible sides. Thus, this requirement is met.

Lighting:

- Pursuant to LDR Section 4.6.8(B)(3), on-site lighting must be provided and be consistent with the minimum and maximum foot candle illumination level requirements. The chart below indicates the minimum and maximum acceptable standards for building entrances, parking lots and landscape lighting:

Photometric Plan	Requirements		Proposed		In Compliance
	Minimum (fc)	Maximum (fc)	Minimum (fc)	Maximum (fc)	
Parking Lot (Commercial)	1.0	12.0	0.8	2.4	No*
Building Entrance	1.0	10.0	10.0	9.7	Yes
Accent Pathway	0.5	5.0	0.7	1.9	Yes

- ✗ Listed as an outstanding requirement under notes section, the photometric plan shall be revised prior to site plan certification to meet the illumination levels for commercial parking lots as indicated in LDR Section 4.6.8.

Number of Parking Spaces Required:

- Pursuant to LDR Section 4.6.9, (C)(1)(c), **Bicycling Parking Facilities**, *shall be provided in shopping centers at the rate of five spaces per 100,000 square feet of gross floor area.*
 - ✓ The applicant has provided approximately 6,752 sf of gross floor area, as such, per the site plan page SP1, a bicycling parking rack that will accommodate five bicycling parking spaces is proposed adjacent to the 1235 building, thus this requirement is met.
- Pursuant to LDR Section 4.6.9 (C)(1)(g), **Compact Car Parking**, *up to 30 percent of the required parking for any use may be designated for compact cars.*
 - ✓ The proposal is required to provide 30 parking spaces and 30 spaces are proposed on site. As indicated on the site plan, 8 parking spaces will be designated for compact cars which totals 27% of the total required parking. Thus, the proposal complies with this limitation.

Requirement for Commercial Uses:

- Pursuant to LDR Section 4.6.9(C)(3)(a), **General Commercial** *uses shall provide 4.5 spaces per 1,000 square feet of gross floor area, which includes retail floor area, associated warehouse and storage floor area, and employee and management facilities. Pursuant to LDR Section 4.6.9(C)(4)(a), **Business and Professional Offices** shall provide 4 spaces per 1,000 sf of net area. As the use does not share any common spaces with other offices, the total gross floor area is subject to the parking requirements.*
 - ✓ According to site plan page SP1, the site has a total of 6,752 sf. of leasable space. It is indicated on the plans that 6,360 sf. of gross floor area will be designated for uses classified as General Commercial, and a total of 392 sf. will be designated for a Business and Professional Office. As the business office space does not share any common areas with other offices, the total gross floor area is subject to the parking requirements. The site is required to provide 30 spaces, as identified in the table below, and 30 spaces are provided throughout the development. Thus, the proposal meets this requirement.
- Pursuant to LDR Section 4.6.9(D)(4)(c), **Dead-End Parking Bays**, *dead-end parking bays are discouraged, but when site conditions dictate that there be dead-end parking bays, they shall be designed so that there is a 24 ft. wide by 6 ft. deep maneuvering area at the end of the bay. The maneuvering area shall not encroach upon the required landscape areas.*
 - ✓ The subject site is already developed and is landlocked on three sides. A dead-end parking bay is proposed at the north end of the new parking lot to accommodate additional parking spaces. The proposed maneuvering area located at the end of the bay measures 24 ft. wide by 8 ft. deep; thus, this requirement is met.



Site Visibility Requirement:

- Pursuant to LDR Section 4.6.14(B)(1), **Visibility Triangle**, *when an access way intersects a street or alley, 10 ft. sight visibility triangles areas shall be provided as measured from the area on both sides of a driveway formed by the intersection of a driveway and the parcel boundary with a length of 10 ft. along the driveway and a length of 10 ft. along the property line and the third side being a line connecting the ends of the other two lines.*
 - ✓ On page SP1, 10 ft. sight visibility triangles are provided at the new parking entrance adjacent to alleyway. Thus, this LDR requirement is met.

Landscape Analysis:

Pursuant to LDR section 4.6.16(C)(1)(a), prior to the issuance of a building permit for a structure or a paving permit, compliance with the requirements of Section 4.6.16 shall be assured through the review and approval of a landscape plan submitted pursuant to Section 2.4.3(C). A landscape plan has been submitted, evaluated and recommended for approval by the City Senior Landscape Planner. The following analysis highlights the compliance determination for various landscaping aspects of the proposal:

Minimum Landscape requirements

LDR Section	Required	Provided (see LP-4)	In Compliance
Area of shrubs	3,032 SF	5,706	Yes
Interior landscape	1,223 SF	1,223 SF	Yes
Number of Native trees	16 trees	16 trees	Yes
Perimeter trees	11 trees	11 trees	Yes
Existing trees	-	38	Yes
Total trees required	65	65	Yes

- Pursuant to LDR Section 4.6.16 (H)(3)(f), **Landscape barrier**, A minimum 5' wide landscape buffer is required where a proposed parking area abuts an adjacent property line. The landscape strip shall be sodded or have ground cover and be free of any vehicular encroachment, including car overhang.
 - ✗ The plans show varying landscape strip widths less than the required minimum 5' feet. Therefore, as shown, the proposed buffer does not comply with this LDR section. Therefore, this requirement shall be addressed prior site plan certification, and is listed in staff comments.
- Pursuant to LDR Section 4.6.16 (H)(3)(i), **Landscape islands**, *which contain a minimum of 135 square feet of planting area, with a minimum dimension of nine feet, exclusive of the required curb, shall be placed at intervals of no less than one landscaped island for every 13 standard parking spaces. One shade tree shall be planted in every island with a minimum of 75 square feet of shrubs and groundcovers.*
 - ✓ As shown on the proposed site plan and landscape plans, the landscape islands and planting areas associated with the new parking lot measure a minimum of 9 ft. wide and comply with the minimum planting area required; thus, this requirement is met.

Minimum Landscape Requirements for Existing Developments:

Per LDR 4.6.16 (H)(7)(a)(c)

- (a) Provide for perimeter landscaping adjacent to public rights-of-way to screen vehicular parking.
 - ✓ Landscape plan L-3 proposes a 4.5' high continuous row of shrubs, and a row of trees to create a 3'-8" landscape buffer at the new parking area adjacent to the low-density residential property.
- (c) Provide landscape screening at all dumpsters, refuse areas and ground mounted air-conditioning units.
 - ✓ Landscape plan L-3 and L-4 proposes a row of Maki Podocarpus hedges around the dumpster enclosure and ground mounted ac units; thus, this requirement has been met.

A variety of plants and groundcover materials will be installed throughout the site to enhance the development. Based on the above analysis, the Landscape Plan will comply with LDR Section 4.6.16.

Architectural Elevation Analysis

LDR Section 4.6.18(E) – Criteria for Board Action: *The following criteria shall be considered, by the Site Plan Review and Appearance Board (SPRAB), in the review of plans for building permits. If the following criteria are not met, the application shall*



be disapproved:

1. The plan or the proposed structure is in conformity with good taste; good design, and in general contributes to the image of the City as a place of beauty, spaciousness, harmony, taste, fitness, broad vistas, and high quality.
2. The proposed structure, or project, is in its exterior design and appearance of quality such as not to cause the nature of the local environment or evolving environment to materially depreciate in appearance and value.
3. The proposed structure, or project, is in harmony with the proposed developments in the general area, with the Comprehensive Plan, and with the supplemental criteria which may be set forth for the Board from time to time.

Architectural elevation changes are proposed to the 1237 building, which is also requesting approval for a use conversion from garage to a business office. The existing 392 sf. structure will be refurbished without altering the existing structure footprint. The entrance door will be relocated to the south elevation and a barrel tile mansard roof canopy will be introduced above it. New windows will be installed in the existing window openings. The proposed mahogany window shutters are proportionally harmonious with the existing window openings. The metal roof will be replaced with barrel tile mansard roof. The building façade will receive a stucco finish and will be painted white (Sherwin Williams Eider White). The proposed architectural elevation changes to the 1237 building match the recently approved architectural elevation changes associated with the other buildings on site.

No exterior modifications are proposed to the other structures on site. The proposed architectural elevation changes will be compatible and harmonious with the surrounding properties. The proposed modifications will be consistent and in harmony with the recently approved architectural elevation changes for the other buildings located at this development and will represent a positive improvement. Based upon the above, positive findings can be made with regard to the criteria listed in LDR Section 4.6.18(E).

Required Findings:

Pursuant to Section 3.1.1 **Required Findings**, prior to the approval of development applications, certain findings must be made in a form which is part of the official record. This may be achieved through information on the application, written materials submitted by the applicant, the staff report, or minutes. Findings shall be made by the body which has the authority to approve or deny the development application. These findings relate to the following areas:

Section 3.1.1 (A) - Future Land Use Map:

- The resulting use of land or structures must be allowed in the zoning district within which the land is situated and said zoning must be consistent with the applicable land use designation as shown on the Future Land Use Map.
 - ✓ The subject property is designated General Commercial (GC) on the Future Land Use Map (FLUM) and is designated General Commercial (GC) on the Zoning Map. The GC zoning is consistent with the GC land use designation. The proposed Class III Site Plan Modification request is associated with the change of use of several existing buildings, a parking lot expansion, landscape improvements, and related site improvements to a unified development. Per LDR Section 4.4.9 (B)(2), (4), the principal use of a Business, Professional, and Service facilities are permitted uses in the GC zoning district. Additionally, per LDR Section 4.4.9 (D), Veterinary Clinics are allowed as a Conditional Use (CU) in the GC Zoning District. The Veterinary Clinic received Conditional Use approval (Application 2017-256) from the City Commission on April 3, 2018. Based upon the above, a positive finding can be made with respect to consistency with the Future Land Use Map designation.

Section 3.1.1 (B) – Concurrency:

- Facilities which are provided by, or through, the City shall be provided to new development concurrent with issuance of a Certificate of Occupancy. These facilities shall be provided pursuant to levels of service established within the Comprehensive Plan.
 - ✓ As described in Appendix “A”, a positive finding of concurrency can be made as it relates to water and sewer, streets and traffic, drainage, and solid waste.



Section 3.1.1 (C) – Consistency:

- Compliance with performance standards set forth in Chapter 3 and required findings in Section 2.4.5(F)(5) for the request shall be the basis upon which a finding of overall consistency is to be made. Other objectives and policies found in the adopted Comprehensive Plan may be used in making a finding of overall consistency.
 - ✓ As described in Appendix “B”, a positive finding of Consistency can be made as it relates to Standards for Site Plan Actions.

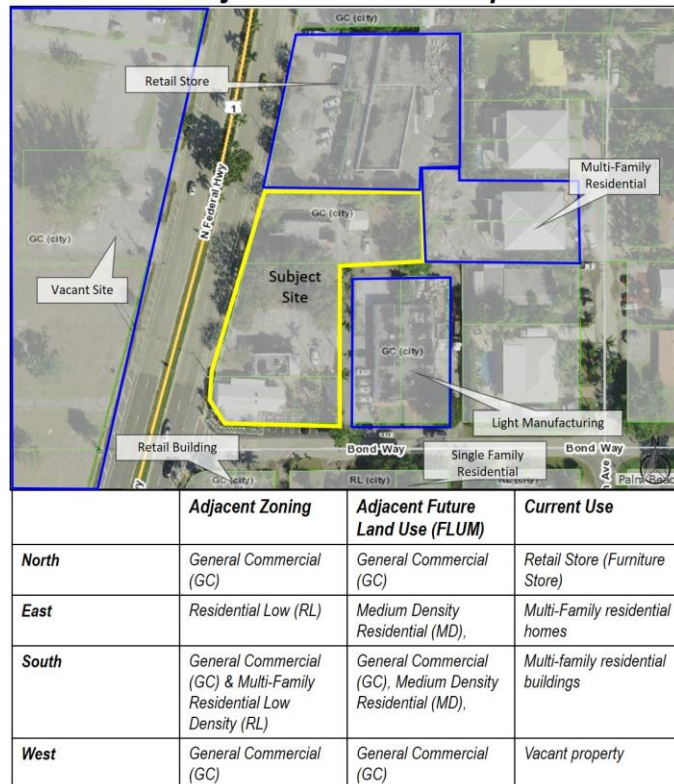
Section 3.1.1 (D) – Compliance with the LDRs:

- Items identified in the Land Development Regulations shall specifically be addressed by the body taking final action on the site and development application/request.
 - ✓ As described under the Site Plan Analysis section of this report, a positive finding of compliance with the LDRs can be made.

Adjacent Land Uses:

- Pursuant to LDR Section 2.4.5(F)(5), **Adjacent land use designations**, “the approving body must make a finding that development of the property pursuant to the site plan will be compatible and harmonious with adjacent and nearby properties and the City as a whole, so as not to cause substantial depreciation of property values”: The following table indicates the zoning and land use of the properties surrounding the subject property:

Adjacent Land Use Map



- ✓ The subject site is located at 1201, 1203, 1205, 1235 and 1237 N. Federal Hwy (Unified Lots) in the GC zoning district. The subject property is located adjacent to GC zoned properties to the North, South, West and East and RL zoned property partially adjacent to the northeastern corner of the site where the proposed parking lot area is located. The proposed conversions do not pose an adverse effect on to the surrounding properties as they exist and are allowed as principal uses in the GC zoning district. Additionally, a proposed landscape buffer is being provided on the northeastern side of the property where the proposed parking lot lies adjacent to RL zoned property to create an additional buffering between commercial and residential lots consistent with Section 4.6.4 (A)(1)(iii). Based upon the above, a positive finding can be made with regard to LDR Section 2.4.5(F)(5) that the site plan will be compatible and harmonious with adjacent and nearby properties.



Comprehensive Plan Policies:

A review of the objectives and policies of the adopted Comprehensive Plan was conducted, and the following applicable objectives or policies were noted.

Future Land Use Element (FLUE)

- *Objective A-1: Property shall be developed or redeveloped, in a manner so that the future use, intensity and density are appropriate in terms of soil, topographic, and other applicable physical considerations; encourage affordable goods and services; are complementary to and compatible with adjacent land uses; and fulfill remaining land use needs.*
 - ✓ The property is proposed to be developed consistent with the surroundings. No incompatibility issues with respect to soil, topographic, intensity of use and other applicable physical consideration is anticipated.

Review by Others:

Palm Beach County Traffic Division

- Pursuant to 2.4.6 (I)(3)(b), the **Palm Beach County Traffic Division** reviewed the traffic study and deemed it compliant with accordance with the County's regulations as described in Appendix "A."

Utility Providers:

- Pursuant to 2.4.2 (C)(2)(a), utility providers were notified of the site plan submission and we have not received any objectionable comments to date.

Courtesy Notices:

- Pursuant to 2.4.2 (C)(3), courtesy notices have been provided to the following homeowner's associations, which have requested notice of developments in their areas:
 - North Palm Trail
 - La Hacienda

Letters of objection or support, if any, will be presented at the Site Plan Review and Appearance Board (SPRAB) meeting.

Assessment and Conclusion:

The Class III Site Plan Modification request for Delray Oasis Business Center is associated with conversion of building uses, introduction of a new parking lot, façade modifications and associated site improvements. The proposed change of uses to business office and general commercial uses are permitted in the GC zoning district as principal uses. The proposed and existing uses are compatible with the adjacent uses, and the proposed changes will not represent a negative impact to the abutting residential and commercial properties. A total of 30 parking spaces are required to be provided on site to accommodate the existing and proposed uses, and a total of 30 spaces will be provided upon construction of the proposed parking lot expansion. The proposed landscape plan was evaluated and recommended for approval by the City Senior Landscape Planner based on compliance with code requirements and positive improvements to the existing open space. The proposed architectural elevation changes to the 1237 building will be compatible and harmonious with the surrounding properties. Thus, the proposal complies with the policies of the Comprehensive Plan and the Land Development Regulations.

Alternative Actions:

- A. Continue with direction.
- B. Move approval of the request for a Class III Site Plan Modification (2018-013) for the Delray Oasis Business Park, by adopting the findings of fact and law contained in the staff report and finding that the request is consistent with the Comprehensive Plan and meets criteria set forth in LDR Section 2.4.5(G)(5), Chapter 3, Section 4.6.18(E), and Section 4.6.16.
- C. Move denial of the request for a Class III Site Plan Modification (2018-014) for Delray Oasis Business Park, by adopting the findings of fact and law contained in the staff report and finding that the request is inconsistent with the Comprehensive Plan and does not meet criteria set forth in LDR Section 2.4.5(G)(5), Chapter 3, Section 4.6.18(E), and Section 4.6.16.

**Appendix “A” – Concurrency Findings**

Pursuant to LDR Section 3.1.1(B), Concurrency, as defined pursuant to Objective B-2 of the Land Use Element of the Comprehensive Plan, must be met and a determination made that the public facility needs of the requested land use and/or development application will not exceed the ability of the City to fund and provide, or to require the provision of, needed capital improvements for the following areas:

Water and Sewer:

As mentioned, the proposed request is for a use conversion of several existing buildings, a parking lot expansion and applicable site improvements. Since the request does not involve the construction of additional buildings, the existing water and sewer on site which service the buildings adequately supply the development. Pursuant to the City's Comprehensive Plan, treatment capacity is available at the City's Water Treatment Plan and the South Central County Waste Water Treatment Plan for the City at build-out. Upon staff's review of the plans, positive findings can be made with respect to this level of service standard.

- ✓ Water service is provided via an existing 8" water main
- ✓ Sewer service is being provided via an existing 8" sanitary sewer

Streets and Traffic:

- The Traffic Analysis provided by applicant created by KBP Consulting, Inc., indicates that the number of daily trips associated with the proposed development scenario (i.e. commercial retail, veterinary office, and general office) is projected to be reduced by 62 trips as compared to the site fully occupied by all commercial retail tenants. During the AM peak hours, six (6) net new vehicle trips are projected, and during the PM peak hours the number of vehicle trips is expected to be reduced by four (4). Upon review of the traffic study, staff has determined that the traffic generation for the proposed use changes will have a negligible impact on vehicle trips on adjacent roadways during daily, AM peak hour, and PM peak hour time periods.
 - ✓ The traffic statement was reviewed by the Palm Beach County Traffic Division and met with approval on January 15, 2018, pursuant to the Traffic Performance Standards in Article 12 of the Palm Beach County Unified Land Development Code (ULDC), therefore, a positive finding can be made.

Parks and Recreation Facilities: Park dedication requirements do not apply for non-residential uses. Thus, the proposed development will not have any impact with respect to this standard.

Solid Waste:**Proposed Demand:**

Yoga/Commercial Use: $2,450\text{-sf} \times 5.4 \text{ lbs} = 13,320 / 2,000 = 6.61 \text{ tons per year}$
Massage/Commercial Use: $283\text{-sf} \times 5.4 \text{ lbs} = 1,528 / 2,000 = .76 \text{ tons per year}$
Hair Salon/Commercial Use: $1,111\text{-sf} \times 5.4 \text{ lbs} = 6,000 / 2,000 = 3 \text{ tons per year}$
Vacant/Commercial Use: $873\text{-sf} \times 5.4 \text{ lbs} = 4,714 / 2,000 = 2.35 \text{ tons per year}$
Veterinary Clinic/Commercial: $1,653\text{-sf} \times 5.4 \text{ lbs} = 8,926 / 2,000 = 4.46 \text{ tons per year}$
Professional Office: $392\text{-sf} \times 5.4 \text{ lbs} = 2,116 / 2,000 = 1.06 \text{ tons per year}$

Tonnage per year = 18.24 tons

- ✓ The conversion of building 1237 of the Delray Oasis Business Park will add approximately 18.24 tons of waste per year to the current demand. The Solid Waste Authority has indicated that its facilities have sufficient capacity to handle all development proposals till the year 2048; thus, meeting concurrency.

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**APPENDIX "B" - STANDARDS FOR SITE PLAN ACTIONS Sec. 3.2.3 (A) through (J)**

- A. Building design, landscaping and lighting (glare) shall be such that they do not create unwarranted distractions or blockage of visibility as it pertains to traffic circulation.
- ☐ Not applicable
☒ Meets intent of standard
☐ Does not meet intent
- B. Separation of different forms of transportation shall be encouraged. This includes pedestrians, bicyclists and vehicles in a manner consistent with policies found under Objectives D-1 and D-2 of the Transportation Element.
- ☐ Not applicable
☒ Meets intent of standard
☐ Does not meet intent
- C. Open space enhancements as described in Policies found under Objective B-1 of the Open Space and Recreation Element are appropriately addressed.
- ☒ Not applicable
☐ Meets intent of standard
☐ Does not meet intent
- D. The City shall evaluate the effect that any street widening or traffic circulation modification may have upon an existing neighborhood. If it is determined that the widening or modification will be detrimental and result in a degradation of the neighborhood, the project shall not be permitted.
- ☒ Not applicable
☐ Meets intent of standard
☐ Does not meet intent
- E. Development of vacant land which is zoned for residential purposes shall be planned in a manner which is consistent with adjacent development regardless of zoning designations.
- ☒ Not applicable
☐ Meets intent of standard
☐ Does not meet intent
- F. Property shall be developed or redeveloped in a manner so that the future use and intensity are appropriate in terms of soil, topographic, and other applicable physical considerations; complementary to adjacent land uses; and fulfills remaining land use needs.
- ☐ Not applicable
☒ Meets intent of standard
☐ Does not meet intent
- G. Redevelopment and the development of new land shall result in the provision of a variety of housing types which shall continue to accommodate the diverse makeup of the City's demographic profile and meet the housing needs identified in the Housing Element. This shall be accomplished through the implementation of policies under Objective B-2 of the Housing Element.
- ☒ Not applicable
☐ Meets intent of standard
☐ Does not meet intent
- H. The City shall consider the effect that the proposal will have on the stability of nearby neighborhoods. Factors such as noise, odors, dust, traffic volumes and circulation patterns shall be reviewed in terms of their potential to negatively impact the safety, habitability and stability of residential areas. If it is determined that a proposed development will result in a degradation of any neighborhood, the project shall be modified accordingly or denied.
- ☐ Not applicable
☒ Meets intent of standard



☐ Does not meet intent

- I. Development shall not be approved if traffic associated with such development would create a new high accident location, or exacerbate an existing situation causing it to become a high accident location, without such development taking actions to remedy the accident situation.

☐ Not applicable

☒ Meets intent of standard

☐ Does not meet intent

- J. Tot lots and recreational areas, serving children from toddler to teens, shall be a feature of all new housing developments as part of the design to accommodate households having a range of ages. This requirement may be waived or modified for residential developments located in the downtown area, and for infill projects having fewer than 25 units.

☒ Not applicable

☐ Meets intent of standard

☐ Does not meet intent

AERIAL VIEW



PROPERTY INFORMATION

LOCATION ADDRESS:
1201 N FEDERAL HWY,
DELRAY BEACH, FL 33483

PROPERTY OWNER:
DELRAY OASIS BUSINESS PARK LLC.

PARCEL CONTROL NUMBER:
12-43-46-09-08-001-0060

ZONING DISTRICT:
GC - GENERAL COMMERCIAL

DESCRIPTION OF WORK

This is a Class III Modification Site Plan, we are providing additional parking and new landscaping

DRAWING LIST

- SP1 Site Plan, Location Plan, Notes & Aerial View
- SP2 Site Plan Demolition
- SP3 Existing Floor Plans
- SP4 Life safety Plan
- Photometric Building 1201, 1203 Photometric Plan

EXISTING & PROPOSED USES

1201 Building: No change in use. The existing 2450 sq.ft. Building is approved for commercial uses.

1203 Building: Change of use of a 285 sq.ft. storage unit to a commercial use.

1205 Building first story: No change in use. The existing 1,111sq.ft. One-story portion of the building is approved for commercial space.

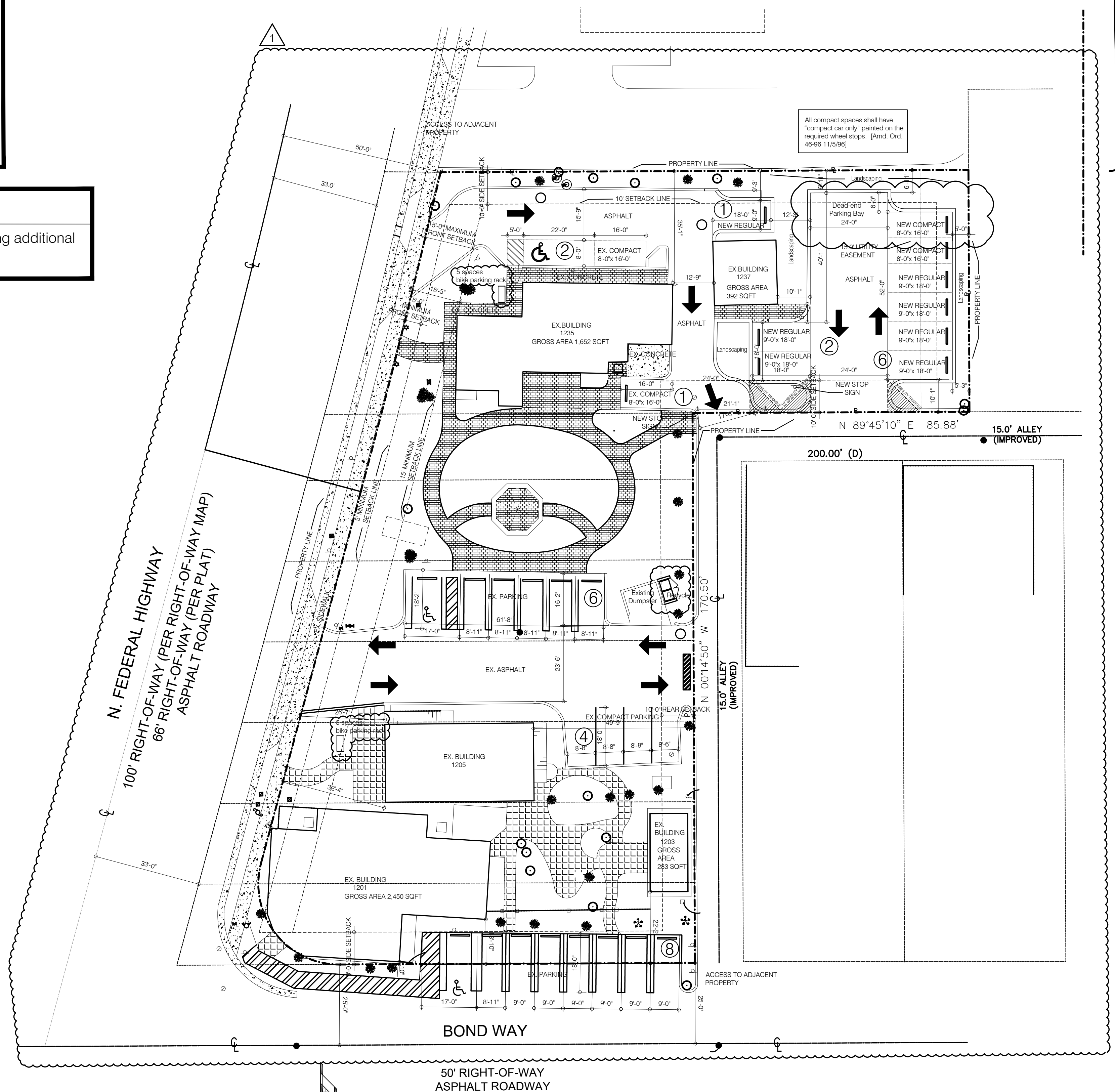
1205 Building second story: Change of use of a 872 sq.ft. two-bedroom residential unit to a commercial use.

1235 Building: No change of use in the front portion of the building; it is approved for commercial uses. Change of use of a 425 sq.ft residential unit (rear portion of the building) to a veterinary clinic.

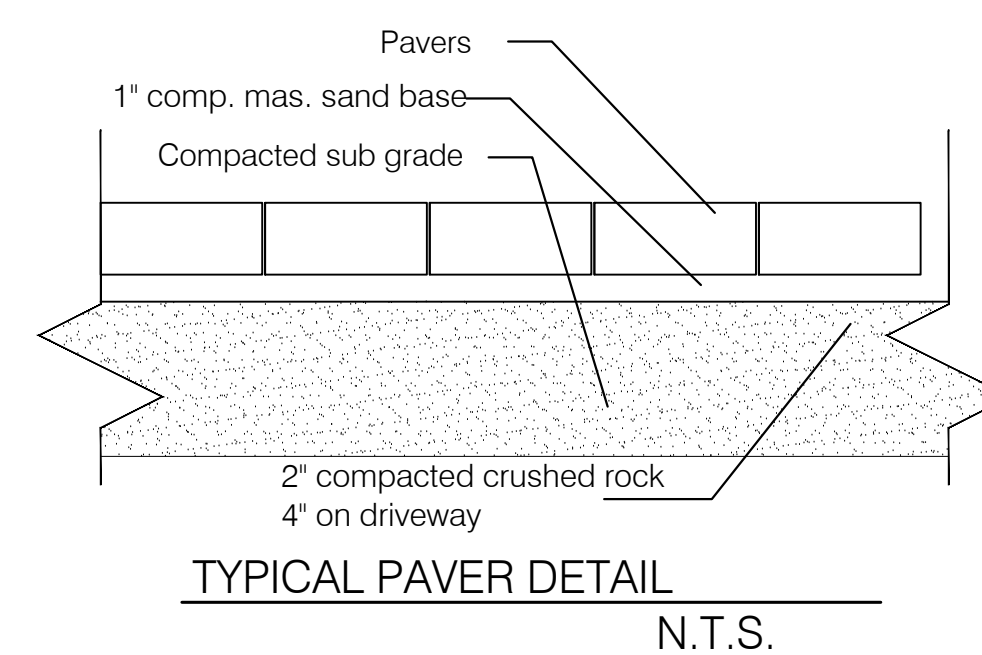
1237 Building: Change of use of a 392 sq.ft. Accessory garage to a business office.

PARKING REQUIREMENT

Building	Use/ Occupancy	Parking requirement	Gross Area SQFT	Parking spaces required
Building 1201	Yoga/Commercial Use	4.5 spaces per 1000 SF	2,450	11
Building 1203	Massage/Commercial Use	4.5 spaces per 1000 SF	283	1.2
Building 1205 First Floor	Hair salon/Commercial Use	4.5 spaces per 1000 SF	1,111	4.9
Building 1205 Second Floor	Vacant/ Commercial Use	4.5 spaces per 1000 SF	873	3.9
Building 1235	Veterinary Clinic/ Commercial Use	4.5 spaces per 1000 SF	1,653	7.4
Building 1237	Professional office	4.0 spaces per 1000 SF	392	1.6
Total Required Parking Spaces				30 = 30
Parking	Handicap (2 between 26 & 50)	Regular	Compactv(30%)	Total
Existing	3	12	6	21
Proposed	0	7	2	9
Total	3	19	8	30



Proposed Site Plan 1 = 20" Sc.



LEVEL 2 ALTERATION
REMODELING FOR:
DELRAY OASIS BUSSINES PARK
1201 N. FEDERAL HWAY
DELRAY BEACH , FL.

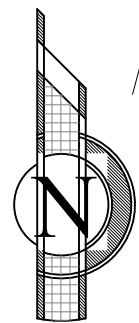
JOHN SHERMAN REED
ARCHITECT
FL LICENSE# AR95171

JSR DESIGN GROUP INC.
1 WEST CAMINO REAL SUITE 107E
BOCA RATON, FL 33432
PH. 561-362-7203
A-26002033

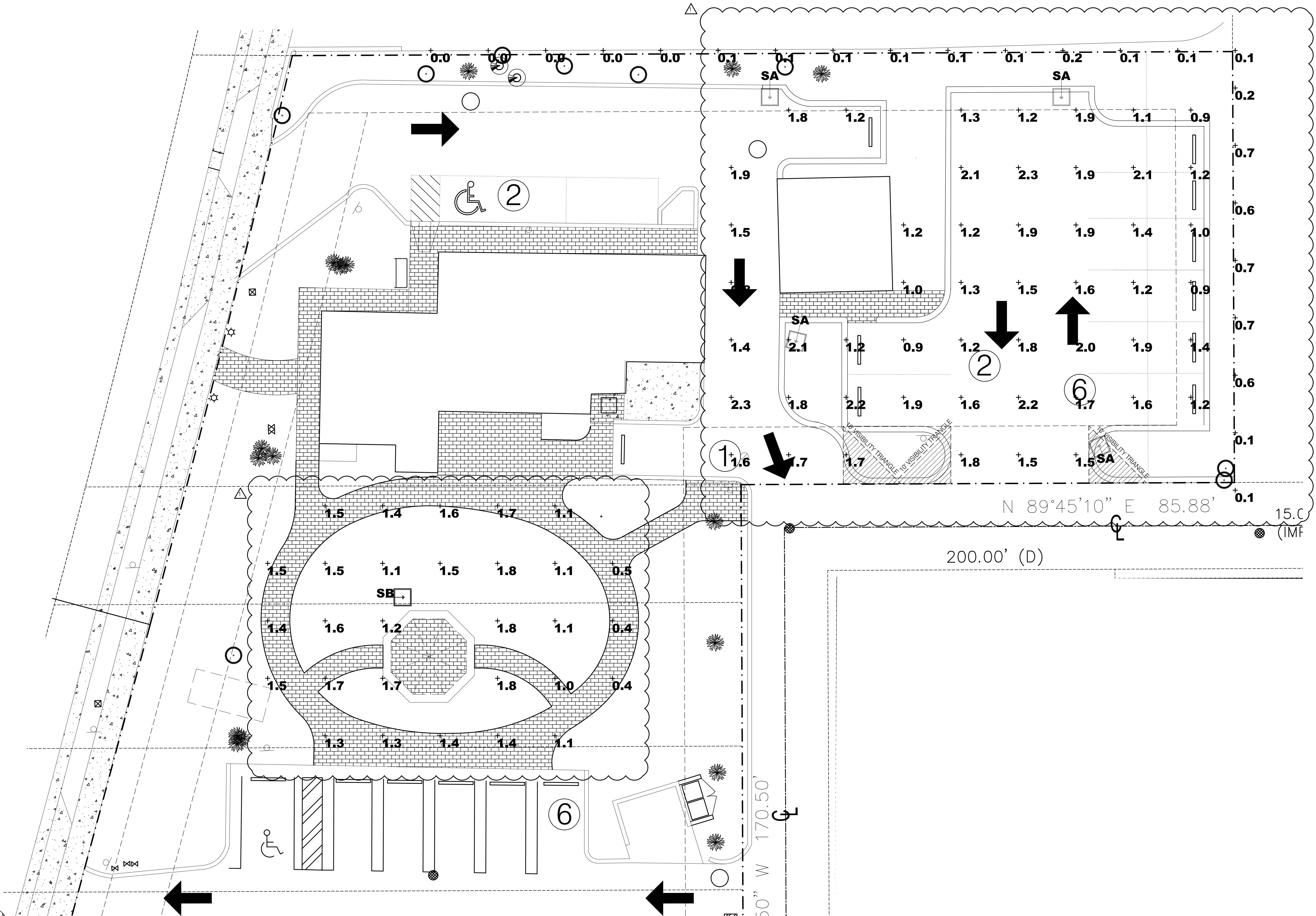
City Comments
7-17-18

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16516
SP1



Proposed Photometrics 1/8" Sc.



Luminaire Schedule								
Symbol	Qty	Label	Arrangement	Total Lamp Lumens	LLF	Description		Lum. Watts
	4	SA	SINGLE	N.A.	0.900	BEACON VP-S-22NB-70-4K-T4-BLC CONCRETE POLE MTD - 18' A.G.		66
	1	SB	SINGLE	N.A.	0.900	BEACON VP-S-22NB-70-4K-T5QM CONCRETE POLE MTD - 18' A.G.		66

Calculation Summary								
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min	
NEW DRIVES & PARKING	Illuminance	Fc	1.56	2.3	0.8	1.95	2.88	
OVAL WALKWAY	Illuminance	Fc	1.32	1.8	0.4	3.30	4.50	
PROPERTY LINE - RIGHT	Illuminance	Fc	0.46	0.7	0.1	4.60	7.00	
PROPERTY LINE - UPPER	Illuminance	Fc	0.07	0.2	0.0	N.A.	N.A.	



LEVEL 2 ALTERATION
REMODELING FOR:
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AZ26002033

City Comments
7-17-18

8/7/2018 2:57:32 PM. VSD04

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16516

PHOTOMETRIC I

1201, 1203, 1205, 1235
& 1237 N. Federal Hwy.
DELRAY BEACH, FL 33483

326 NE 1st Avenue Delray Beach, Florida 33444-3804
561 276 7750
FAX 561 276 7046

Revision:	OCTOBER 2, 2018
	OCTOBER 25, 2018

North: _____

Scale: 1" = 10'-0"

Job Number: _____

Project Name: _____

Drawing Name: _____

Checked By: _____

Date: DECEMBER 20, 2017

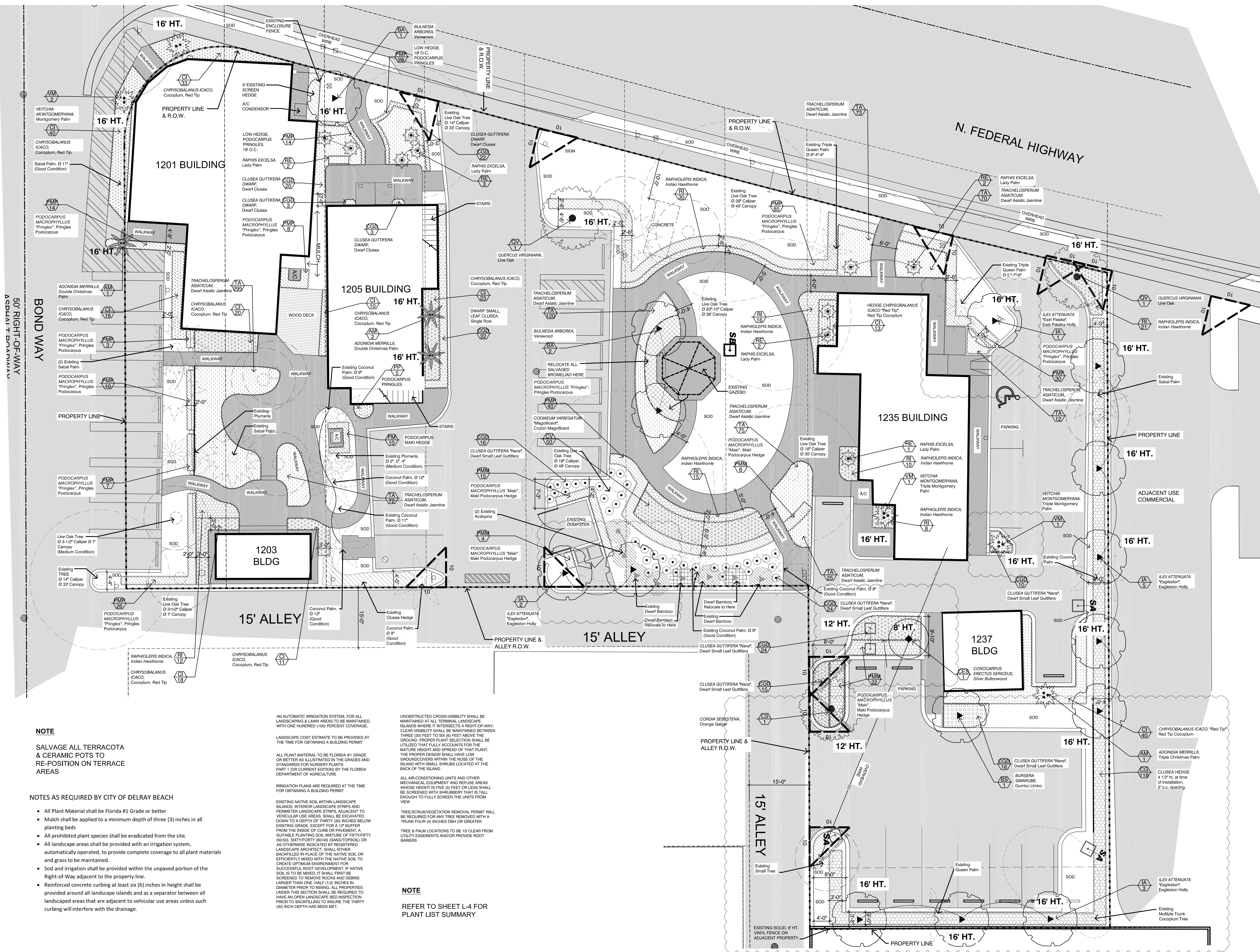
Sheet Title

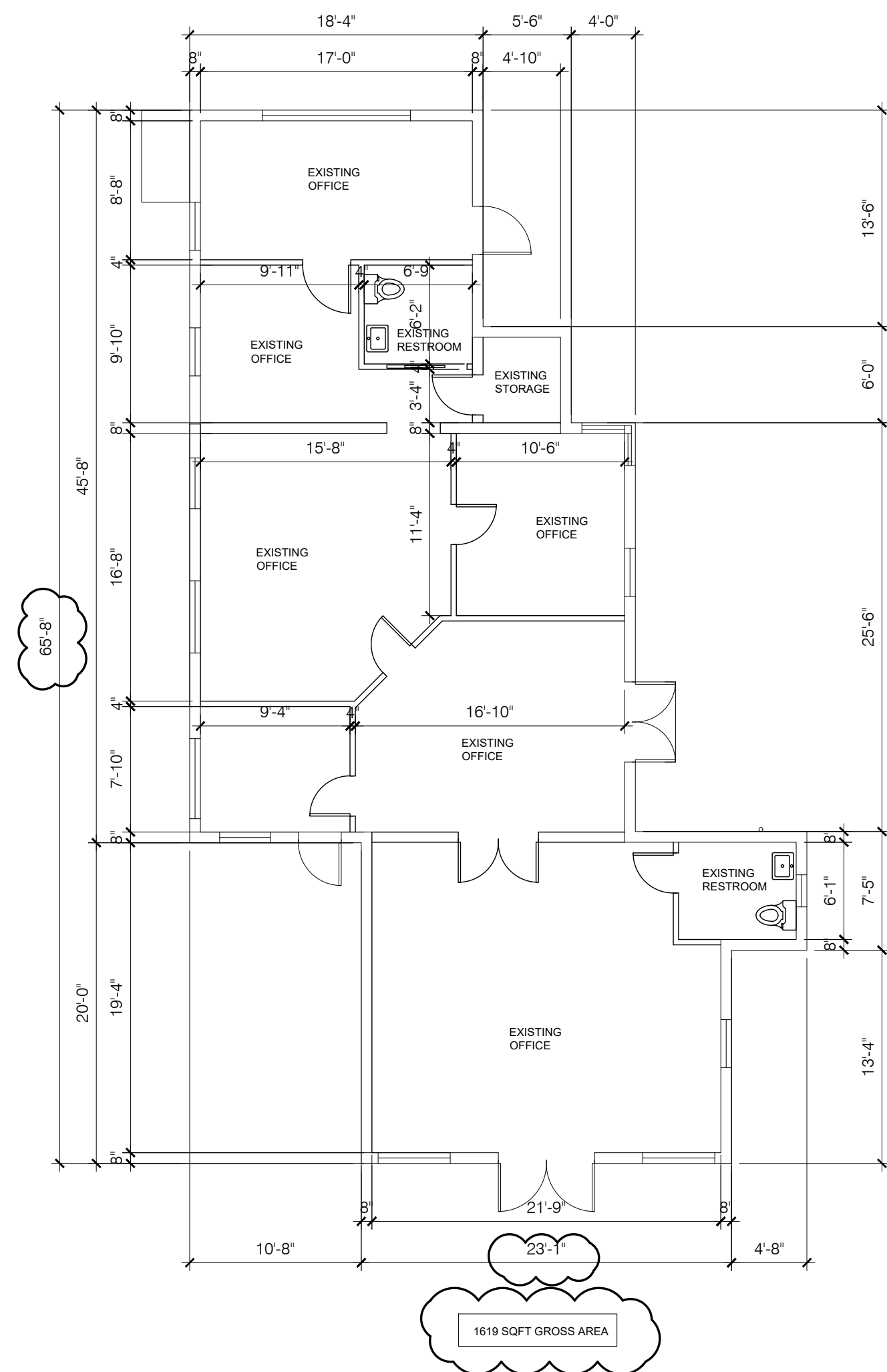
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L-3

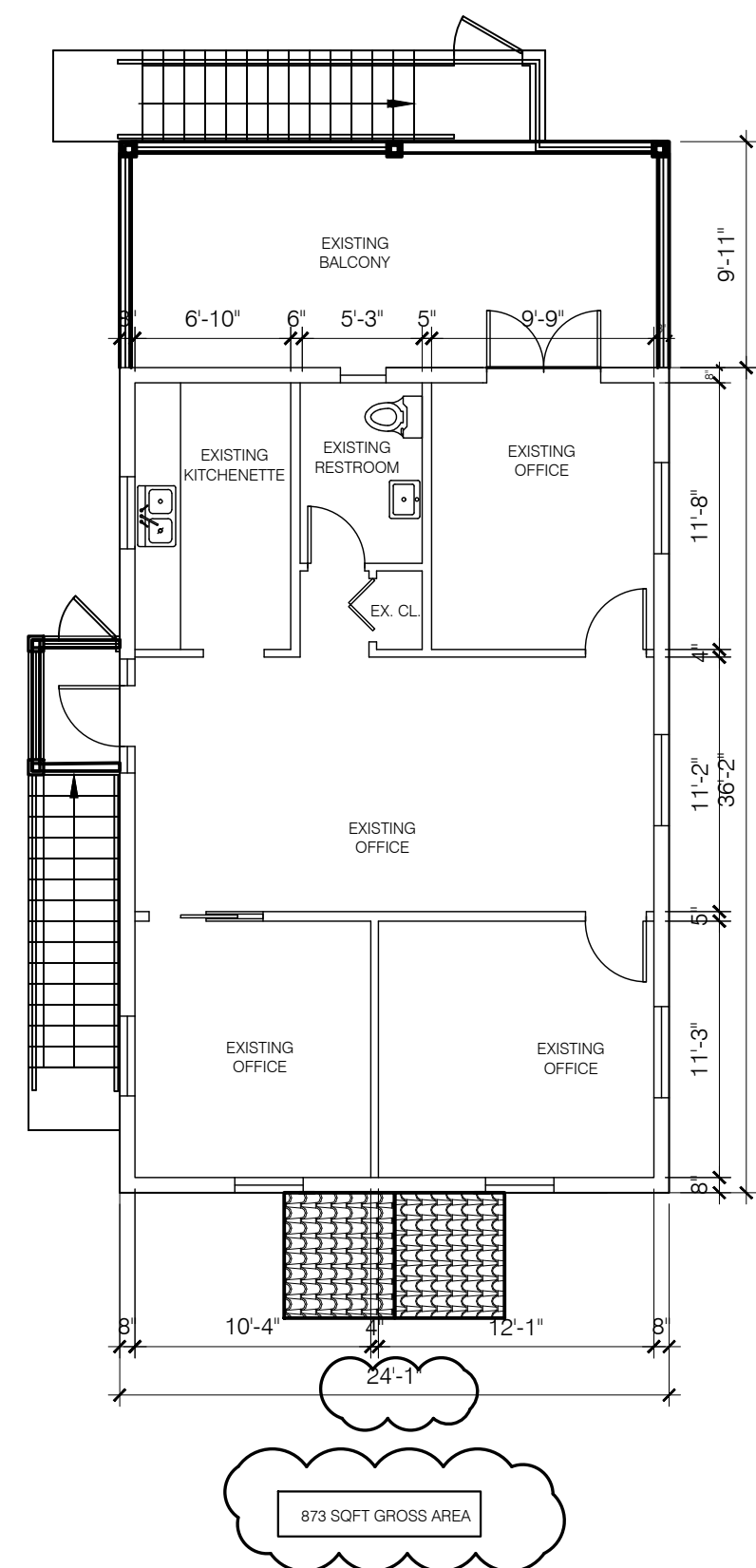
Sht. _____ of _____

© 2018 Murakami Landscape Architects

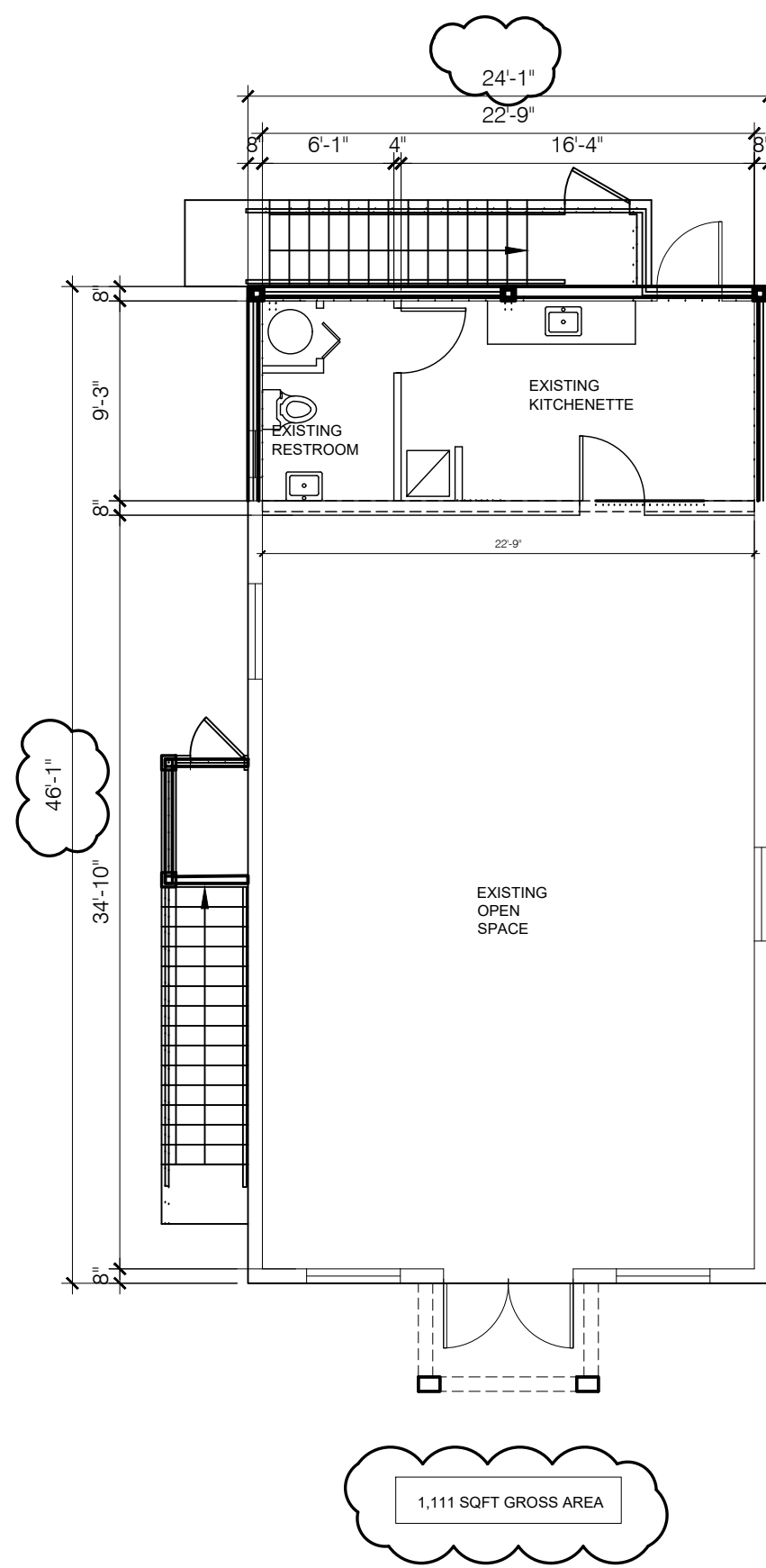




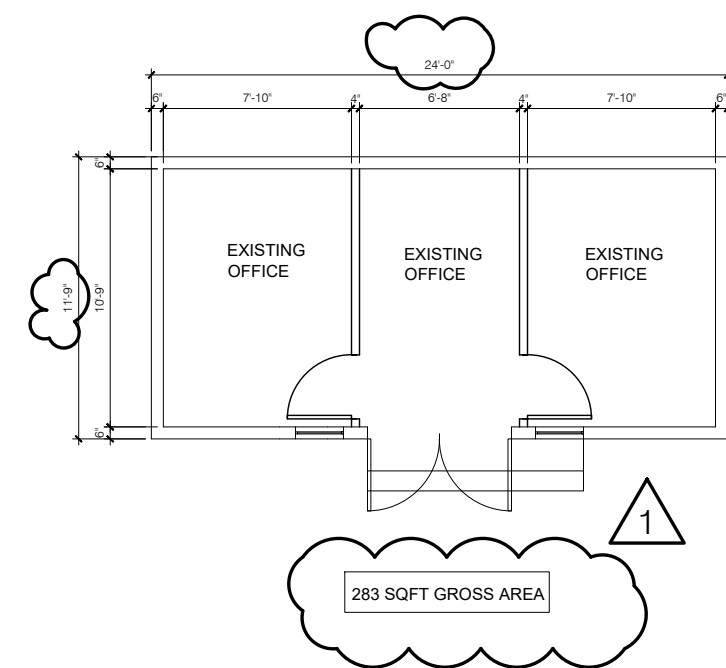
1235 Existing First Floor Plan
1/8" Sc.



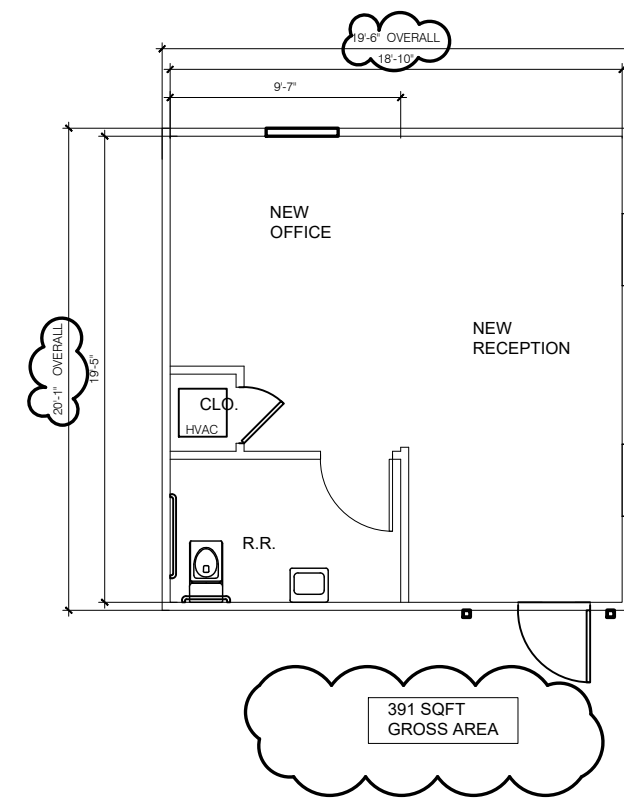
1205 Existing Second Floor Plan
1/8" Sc.



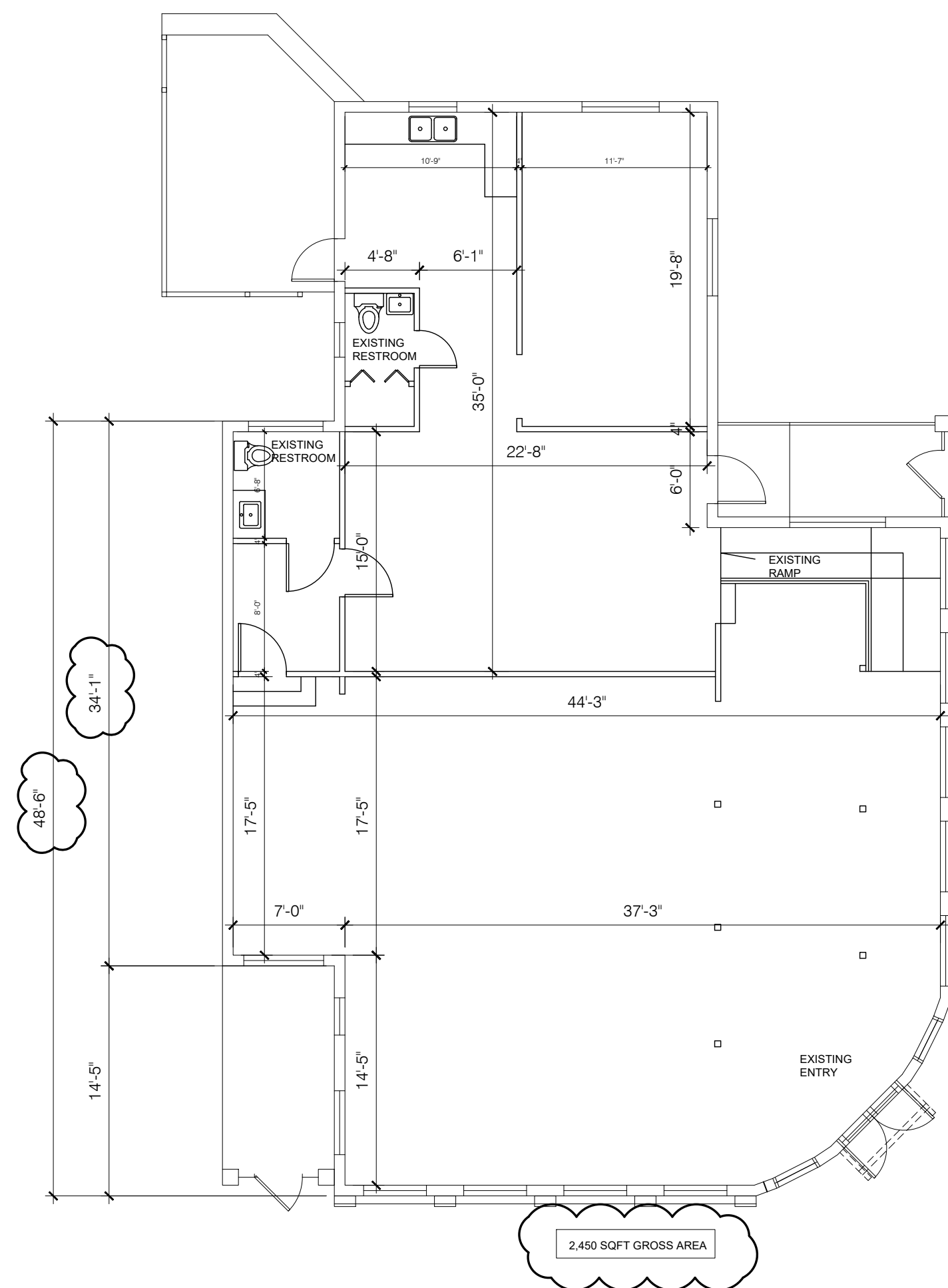
1205 Existing First Floor Plan
1/8" Sc.



1203 Existing First Floor Plan
1/8" Sc.

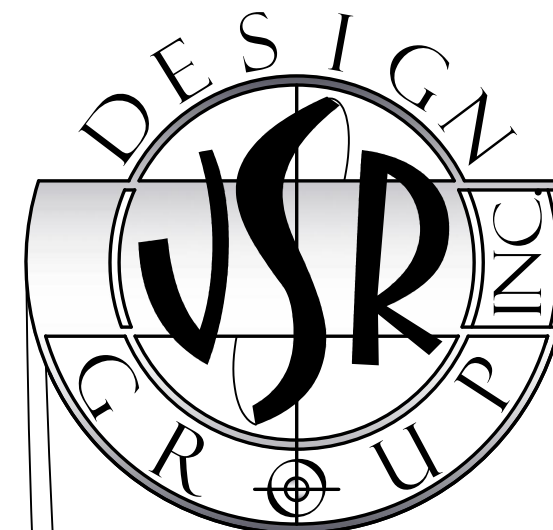


1237 Existing First Floor Plan
1/8" Sc.



1201 Existing Floor Plan
1/8" Sc.

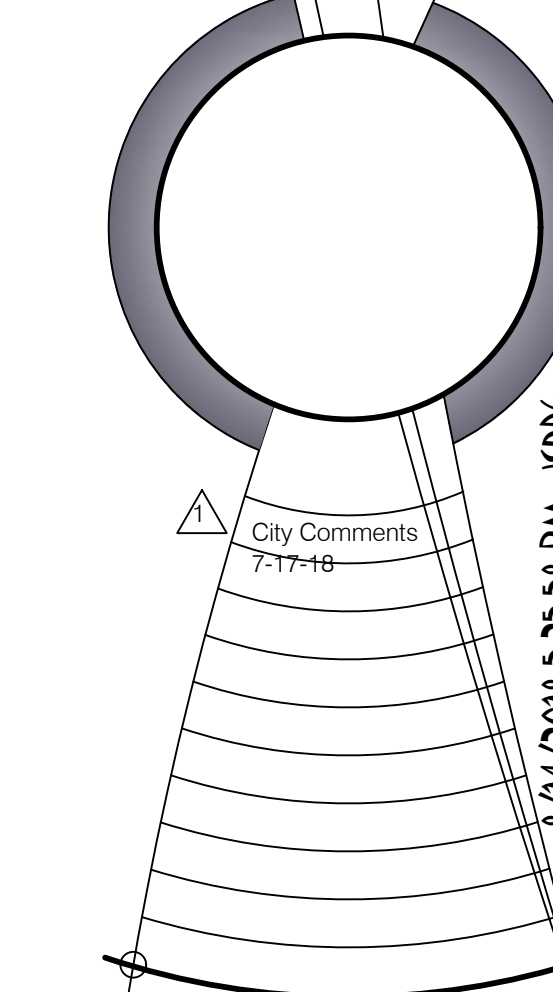
Existing floor plans, not interior remodeling will be done



LEVEL 2 ALTERATION
REMODELING FOR:
DELRAY OASIS BUSSINES PARK
1201 N. FEDERAL HWAY
DELRAY BEACH , FL.

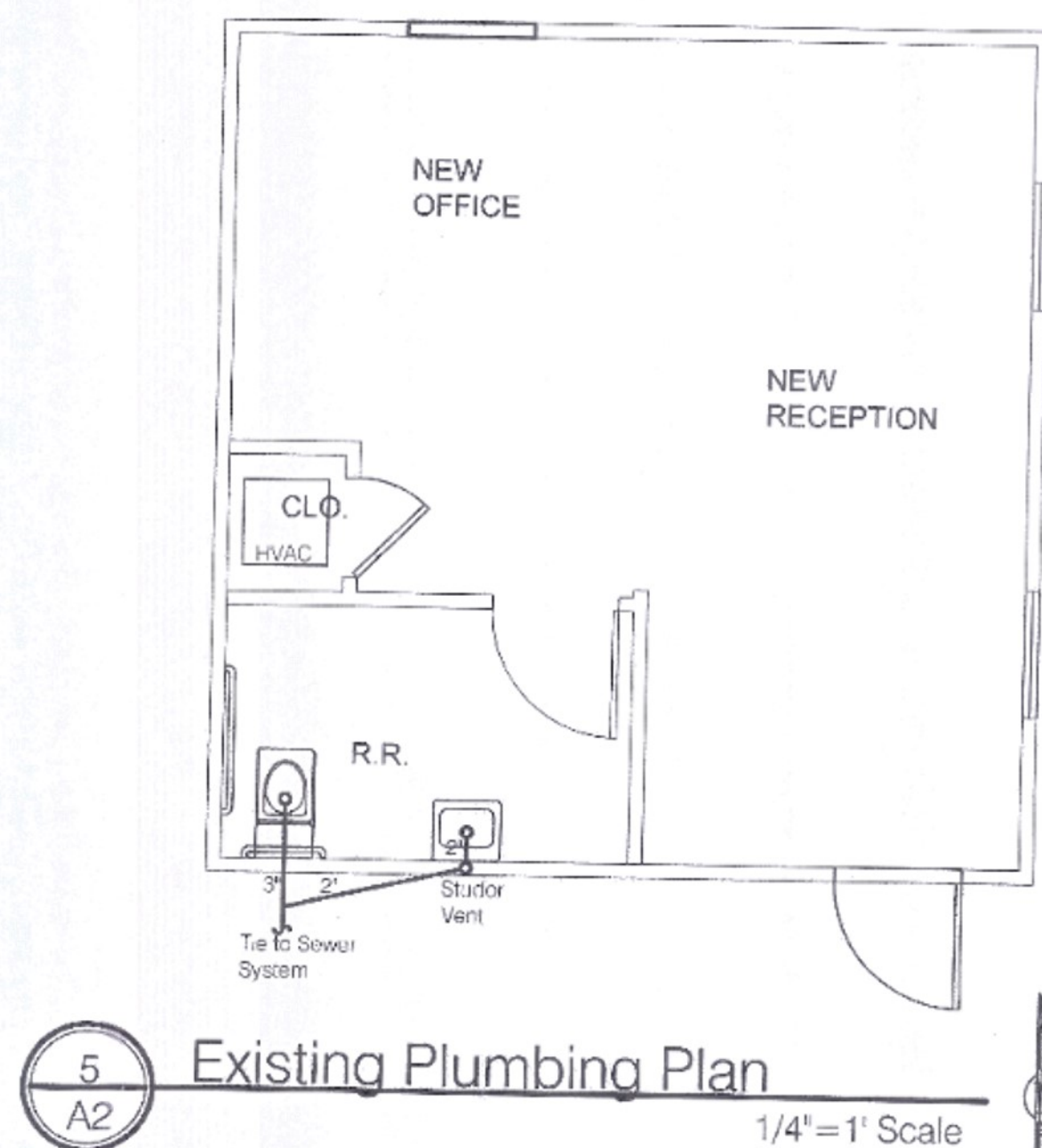
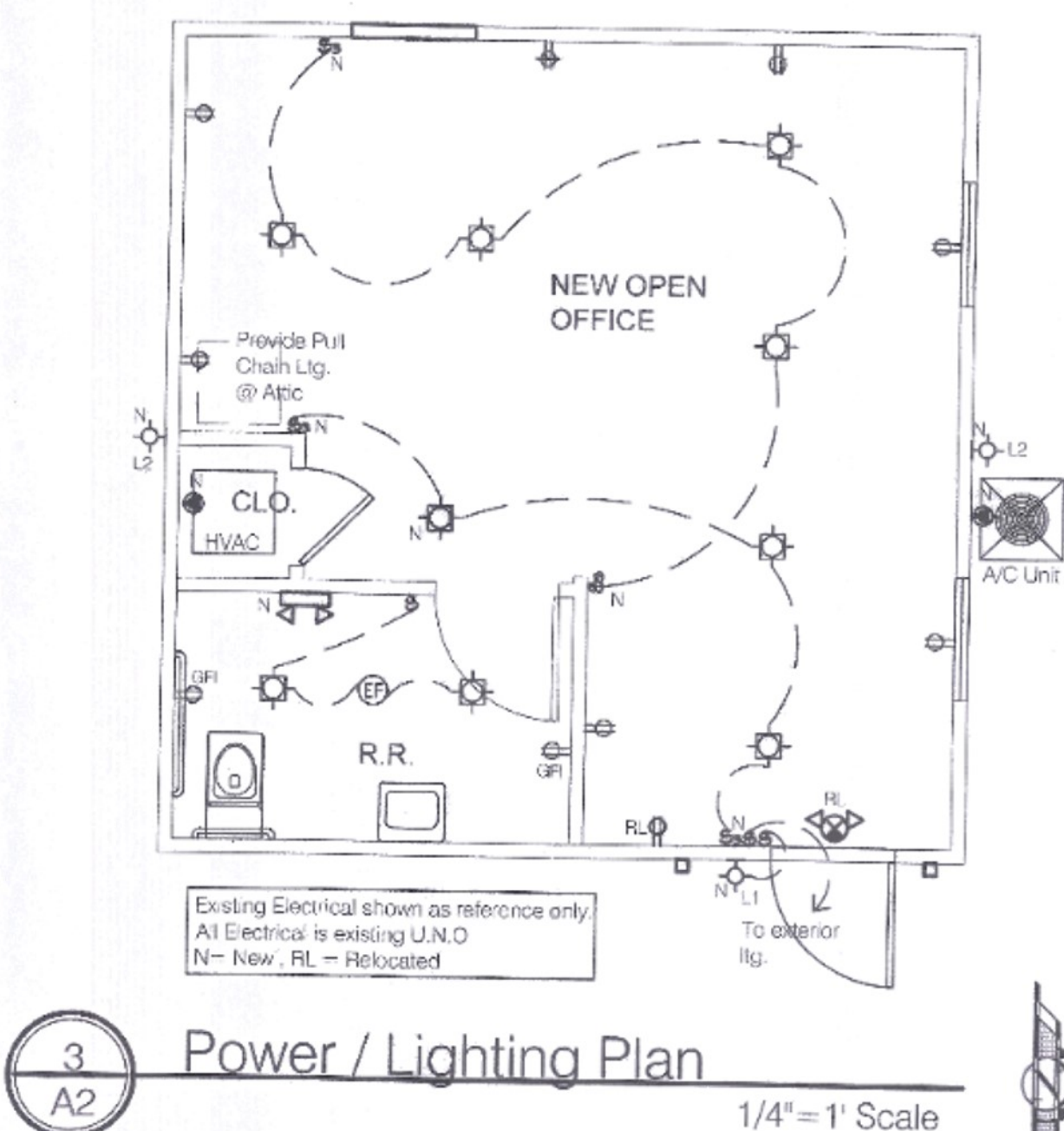
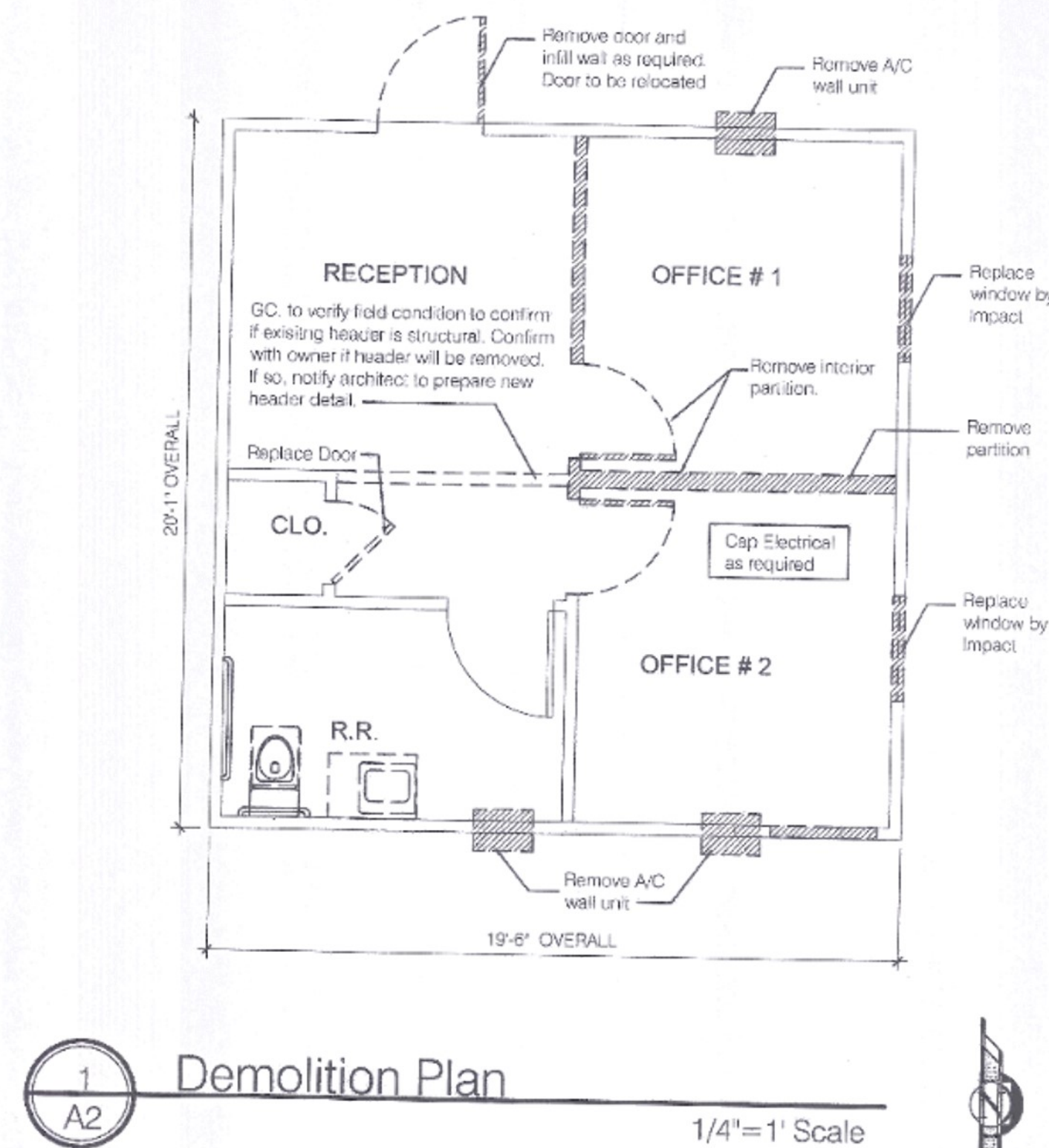
JOHN SHERMAN REED
ARCHITECT
FL LICENSE# AR95171

JSR DESIGN GROUP INC.
1 WEST CAMINO REAL SUITE 117E
BOCA RATON, FL 33432
PH. 561-362-7203
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16516
SP3



Wall Legend

	Existing wood framed wall. Apply Stucco outside as per typical wall section. Interior drywall to remain undamaged.
	Wood infill w/ exterior stucco finish and interior drywall to match existing













TYPICAL INSULATION NOTES:

exterior wood frame walls: Provide a min R-19 insulation.

DOOR & WINDOW SCHEDULE						
MARK	SIZE	TYPE	MATERIAL	REMARKS	WIND PRESS. (POS/NEG)	R.O. (n)
(D1)	3'-0" x 6'-8"	SWINGING	ALUM & GLASS	EXISTING RELOCATED	+ 36.1 / - 47.2	38"x 82"
(D2)	2'-4" x 6'-8"	SWINGING	LOUVERED	A/C CLOSET		
(W1)	3'-0" x 4'-0"	SINGLE HUNG	ALUMINUM	REPLACE EXISTING	+ 36.1 / - 47.2	39" x 49" (Extg.)
(W2)	3'-0" x 4'-0"	SINGLE HUNG	ALUMINUM	REPLACE EXISTING	+ 36.1 / - 47.2	38" x 49" (Extg.)

G.C. TO VERIFY R.O. WITH MANUFACTURER BEFORE PURCHASING
HARDWARE TO BE SELECTED BY OWNER
FRAME COLOR AND GLASS TINT BY OWNER.
DOORS QUANTITIES VERIFIED BY G.C. / WINDOW MANUFACTURER
PROVIDE WINDOW FLASHING CONSISTING OF WINDOWSEAL™ 40 OR EQUAL, WINDOWSEAL™ 40 IS A 40 MIL
SELF-ADHERING, SELF-SEALING CONSTRUCTION TAPE CONSISTING OF RUBBERIZED ASPHALT
WATERPROOFING COATING, LAMINATED TO AN EXTRA HIGH STRENGTH POLYETHYLENE FILM BACKING

LEGEND

	Illuminated Exit Sign		Switch
	Emergency Light		Three Way Switch
	110V Receptacle		Junction box
	42" High Receptacle		Recessed/surface mounted fixture
	GFI Ground Fault Circuit Interrupter		Incandescent Lighting Wall mounted fixture
	220V Receptacle		Exhaust fan

ELECTRICAL NOTES




1. Do not scale the electrical drawings. Refer to architectural plans and elevations for exact location of all equipment. Confirm with owners representative.
2. All work shall be done in accordance with National Electrical Code and shall comply with all local rules and ordinances.
3. It is not the intent of these plans to show every minor detail of construction. The Contractor is expected to furnish and install all items for a complete electrical system and provide all requirements necessary for equipment to be placed in proper working order. Equipment to be placed in proper working order. Electrical system shall be complete and effectively grounded as required by the latest edition of the N.E.C.
4. All materials shall be new and bear manufacturers labels, where applicable.
5. Work shall be performed by a licensed electrical contractor in a first class workmanlike manner.
6. Completed system shall be fully operative and accepted by Engineer/Architect.
7. All work shall be coordinated with other trades to avoid interference with the progress of construction.
8. Contractor shall guarantee all materials and workmanship free from defects from a period of not less than one year from date of acceptance.
9. Correction of any defects shall be provided for protection against public liability of property damage for the duration of the work.
10. All required insurance shall be provided for protection against public liability of property damage for the duration of the work.
11. Contractor shall pay for all permits, fees, inspections and testing.
12. The electrical installation shall meet all standard requirements of power and telephone companies.
13. Furnish and install disconnect switches and wiring for air conditioning system as per manufacturers recommendations. Controls are to be supplied by air conditioning contractor and connected by electrical contractor.

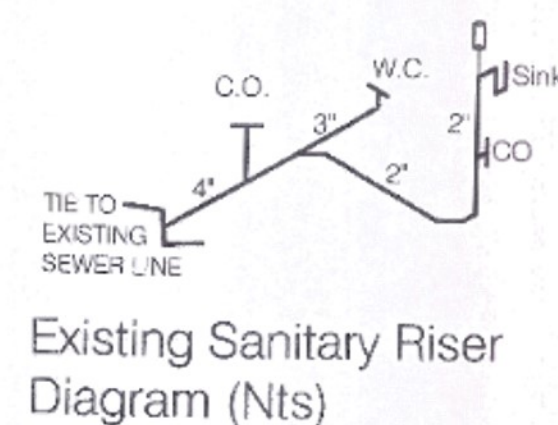
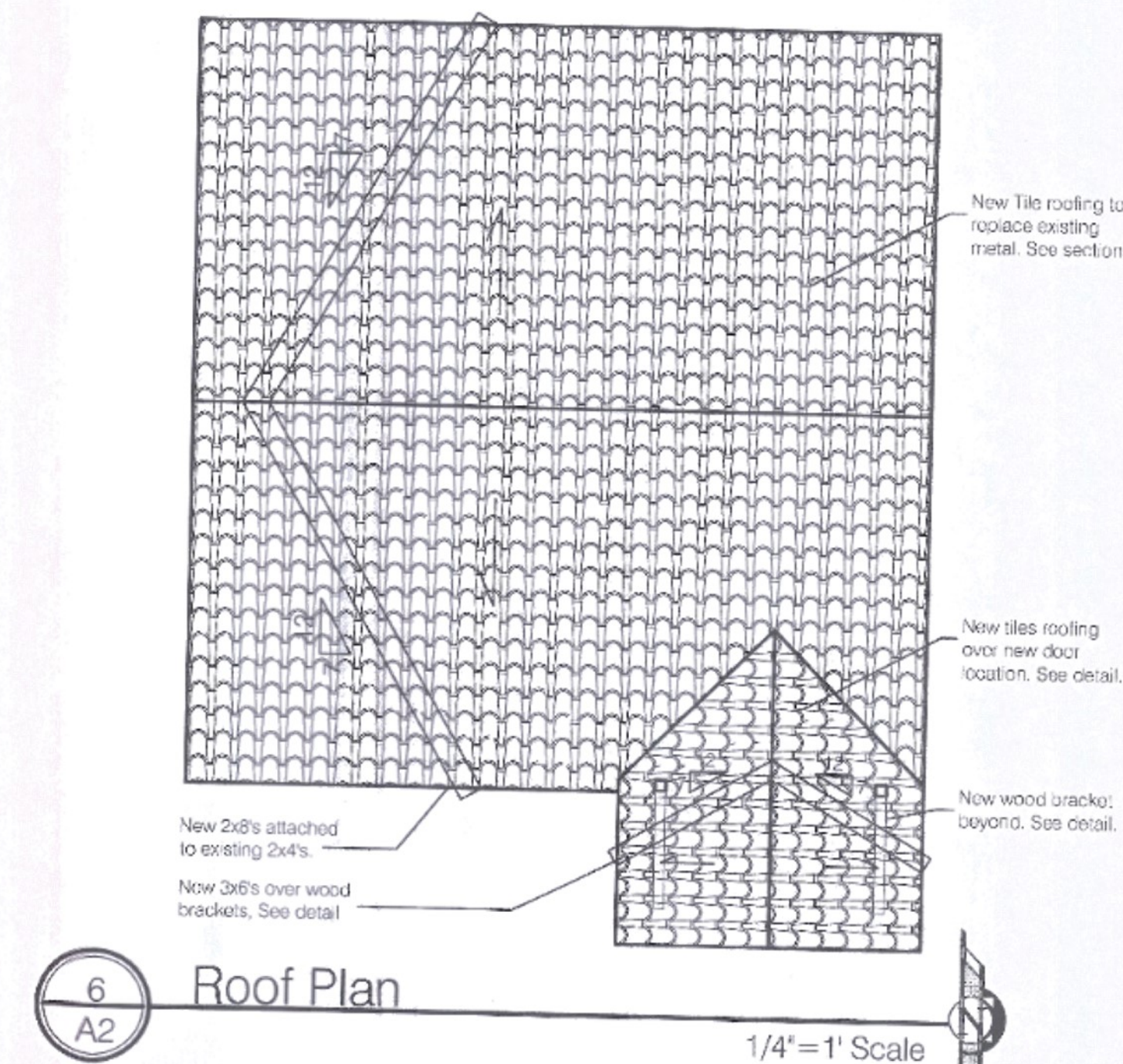
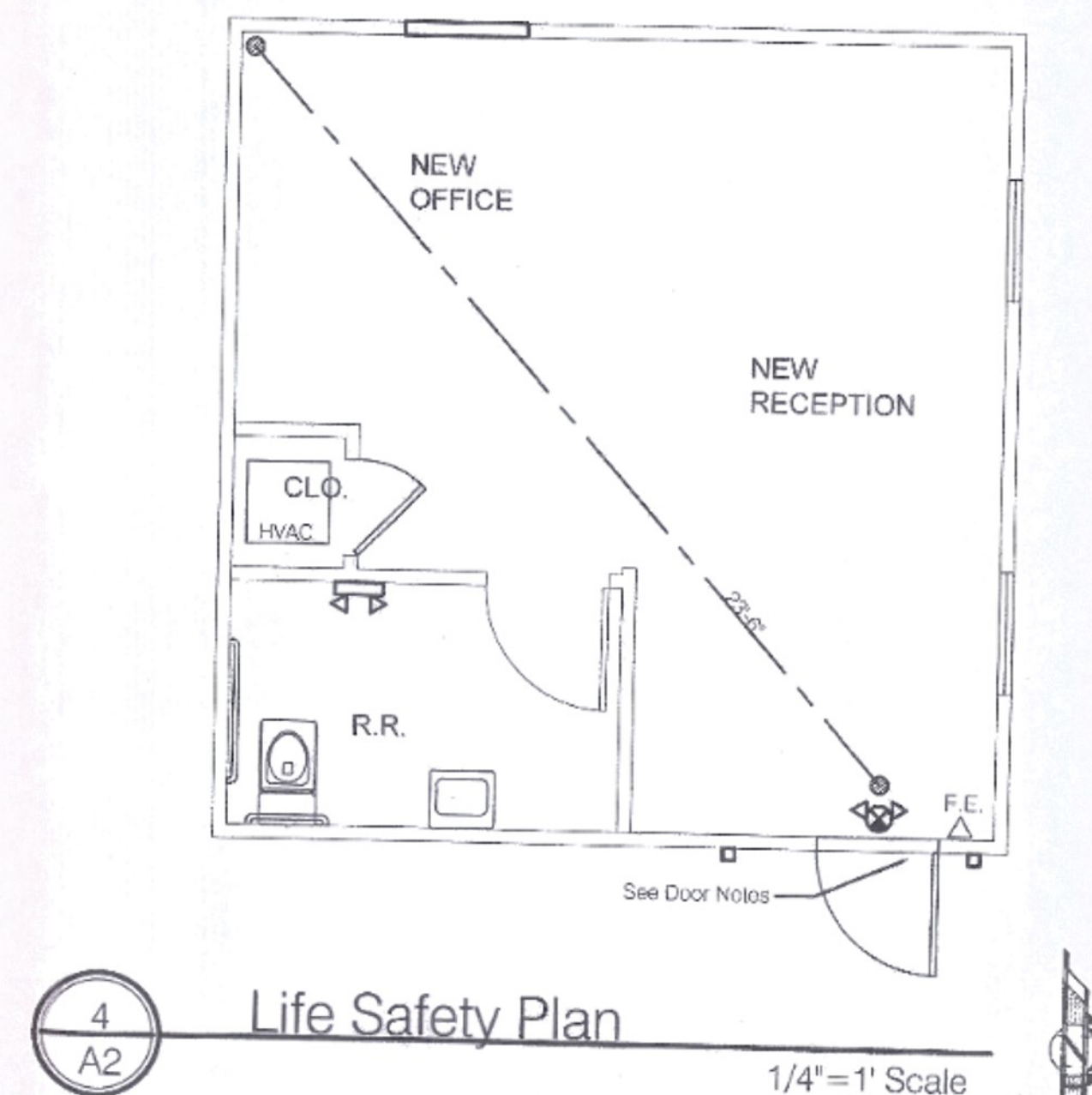
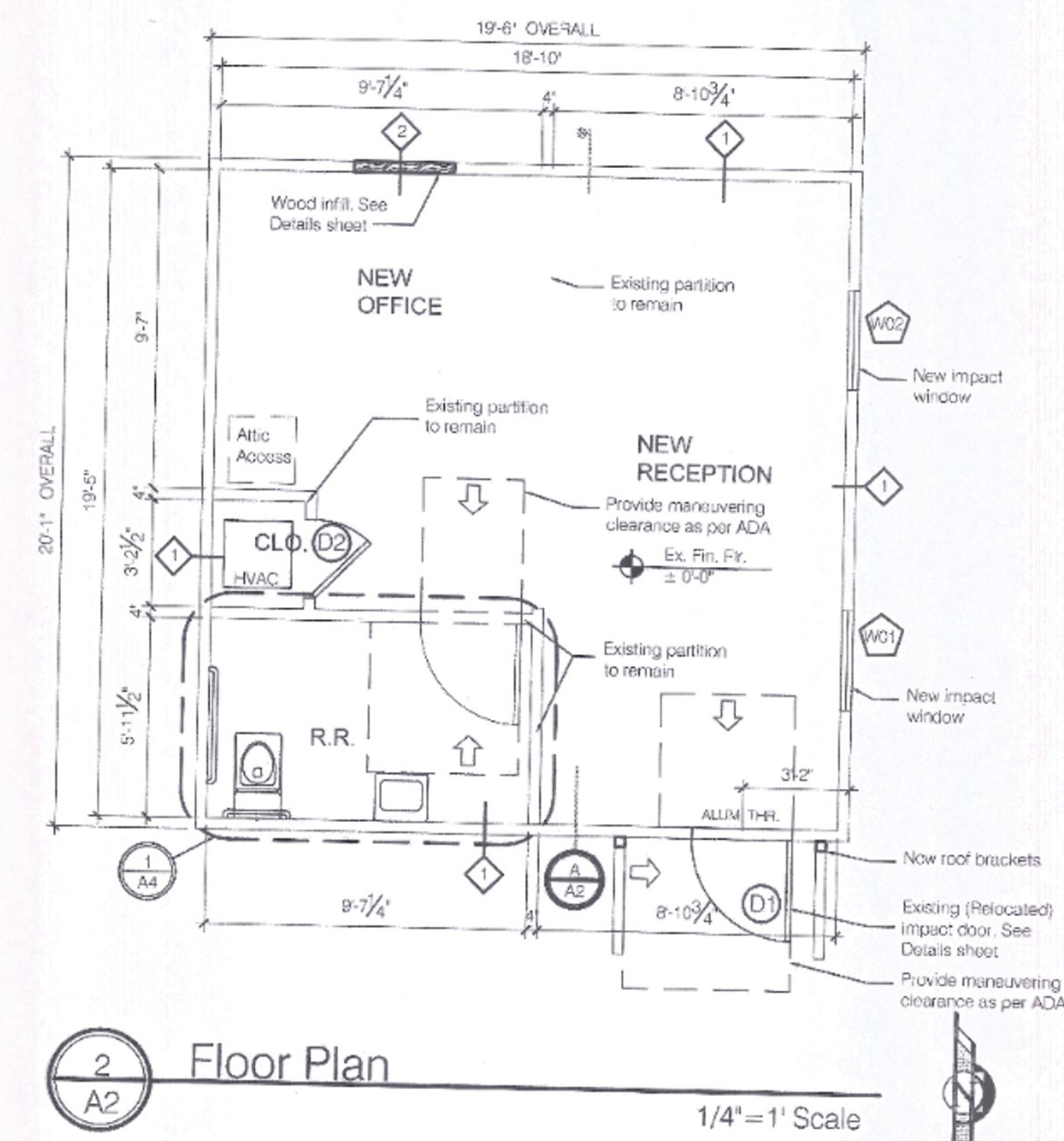
NOTES:

RECEPTACLES ARE TO BE MOUNTED AT 18" ABOVE FINISHED FLOOR, UNLESS OTHERWISE NOTED. SWITCHES ARE TO BE MOUNTED AT 48" ABOVE FINISHED FLOOR, UNLESS OTHERWISE NOTED.

EXTERIOR LIGHT FIXTURES:

1.1. Up & Downlight "DCC2WM" by DMF Lighting.
1.2. "Poseo Neck" AR14 - BZ. by Aluminite
Light fixtures to match other buildings as per approved S-3e plan on May 2017

<h1> LIFE SAFETY NOTES: </h1>	
<p>OCCUPANT LOAD Occupancy classification of this Tenant space classified as Business (B) 1 to 100 S.F., Gross (3rd B.C. Building, Table 1004.1.1 Maximum Floor Area Allowances per Occupant).</p>	<p>DOOR NOTES: 7.2.1.5.1 Door shall be opened readily from the egress side without the building is occupied. Lock should not require use of key, tool, or special knowledge or effort for operation from the egress side. 7.2.1.3 Threshold at doorway not to exceed 1/2" in height. If floor level changes in excess of 1/4" doorways shall be built with a slope not steeper than 1 in 2.</p>
<p>391 S.F./100 = 3.91 occupants Total Occupancy = 4 occupants One (1) Egress Door required</p>	
<p>NFPA 101 Life Safety Code Section 38.2.5 Common Path Limit - Unsprinklered Dead-End Limit Travel Distance Limit Proposed</p> <p style="text-align: right;">100 20 200 23'-0"</p>	<p>FIRE EXTINGUISHERS: Contractor to field verify all existing fire extinguishers, and replace (as/if required) or provide new certified fire extinguishers as per the current adopted Fire Prevention Code, G.C. to make sure that the maximum travel to any fire extinguisher is not more than 75 feet. Verify quantities and all final locations with fire official.</p>
<p>Egress routes illustrated are from the furthest travel distance</p>	
<p>A minimum of 36" Wide Path of Egress must be kept throughout the space.</p>	<p>LEGEND</p>
<p>Exit lights to have battery backup Contractor to verify that the final illumination for exit access will be at least 0.2 foot candles at floor level per F.B.C. 1016.1.4</p>	<p>F.E. FIRE EXTINGUISHERS TO 3E 5 LB ABC WITH TAG.</p>
	<p> LED EXIT SIGN W/BATTERY BACKUP</p>
	<p> LED EXIT SIGN and EMERGENCY LIGHT W/BATTERY BACKUP</p>
	<p> DOUBLE HEAD EMERGENCY LIGHT, W/ BATTERY BACKUP</p>
<p>NFPA Interior Business Finish Schedule per section 38.3.3 Floors: Class II Minimum Walls & Ceilings:</p> <p>(1) Exits and Corridors - Class B (2) Rooms - Class C</p>	



PLUMBING NOTES:

1. ALL WORKMANSHIP AND MATERIALS SHALL BE IN STRICT ACCORDANCE WITH THE FLORIDA BUILDING CODE.
2. CONTRACTOR SHALL VISIT THE JOB SITE AND THOROUGHLY FAMILIARIZE HIMSELF WITH ALL EXISTING CONDITIONS.
3. ALL MATERIALS SHALL BE NEW.
4. ALL WORK SHALL BE PERFORMED BY A LICENSED PLUMBING CONTRACTOR IN A FIRST CLASS WORKMANLIKE MANNER. THE COMPLETE SYSTEM SHALL BE FULLY OPERATIVE.
5. REQUIRED INSURANCES SHALL BE PROVIDED BY THE CONTRACTOR FOR PROTECTION AGAINST PUBLIC LIABILITY AND PROPERTY DAMAGE FOR THE DURATION OF THE WORK.
6. CONTRACTOR SHALL SECURE AND PAY FOR ALL PERMITS, FEES, INSURANCES AND TESTS.
7. DRAWINGS ARE DIAGRAMMATIC, DO NOT SCALE FOR THE EXACT LOCATIONS OF FIXTURES, PIPING, EQUIPMENT, ETC.
8. ALL WORK SHALL BE COORDINATED WITH OTHER TRADES TO AVOID INTERFERENCE WITH THE PROGRESS OF CONSTRUCTION.

PLUMBING CONTRACTOR NOTE:

1. Plumber may alter sanitary sewer connections once field conditions are exposed and verified.

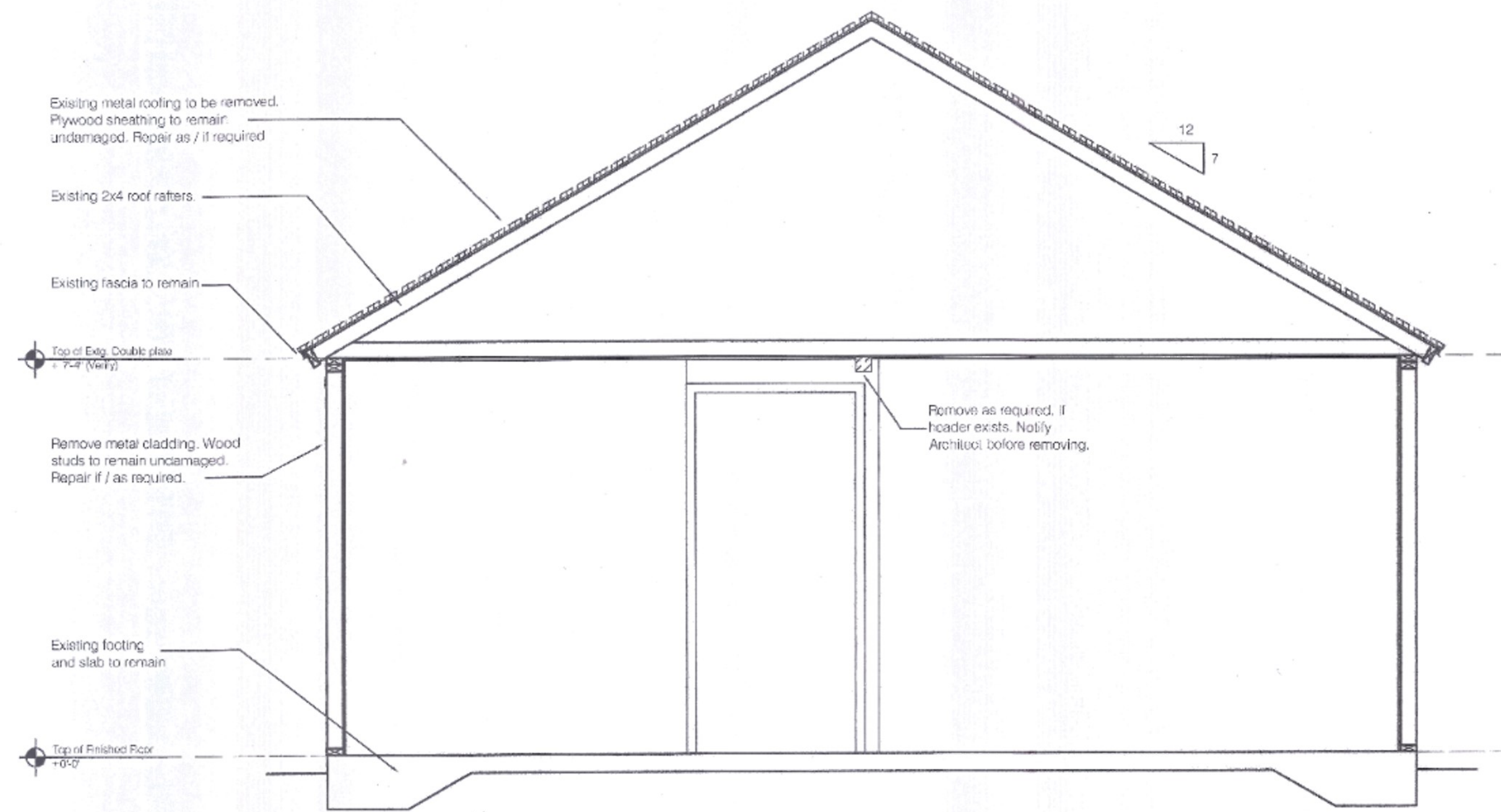
LEVEL 2 ALTERATION
REMODELING FOR:
DELRAY OASIS BUSINESS PARK
1237 N. FEDERAL HWY
DELRAY BEACH, FL

JOHN SHERMAN REED
ARCHITECT
FL LICENS# AR05171

JSR DESIGN GROUP INC.
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PH. 561-362-7203
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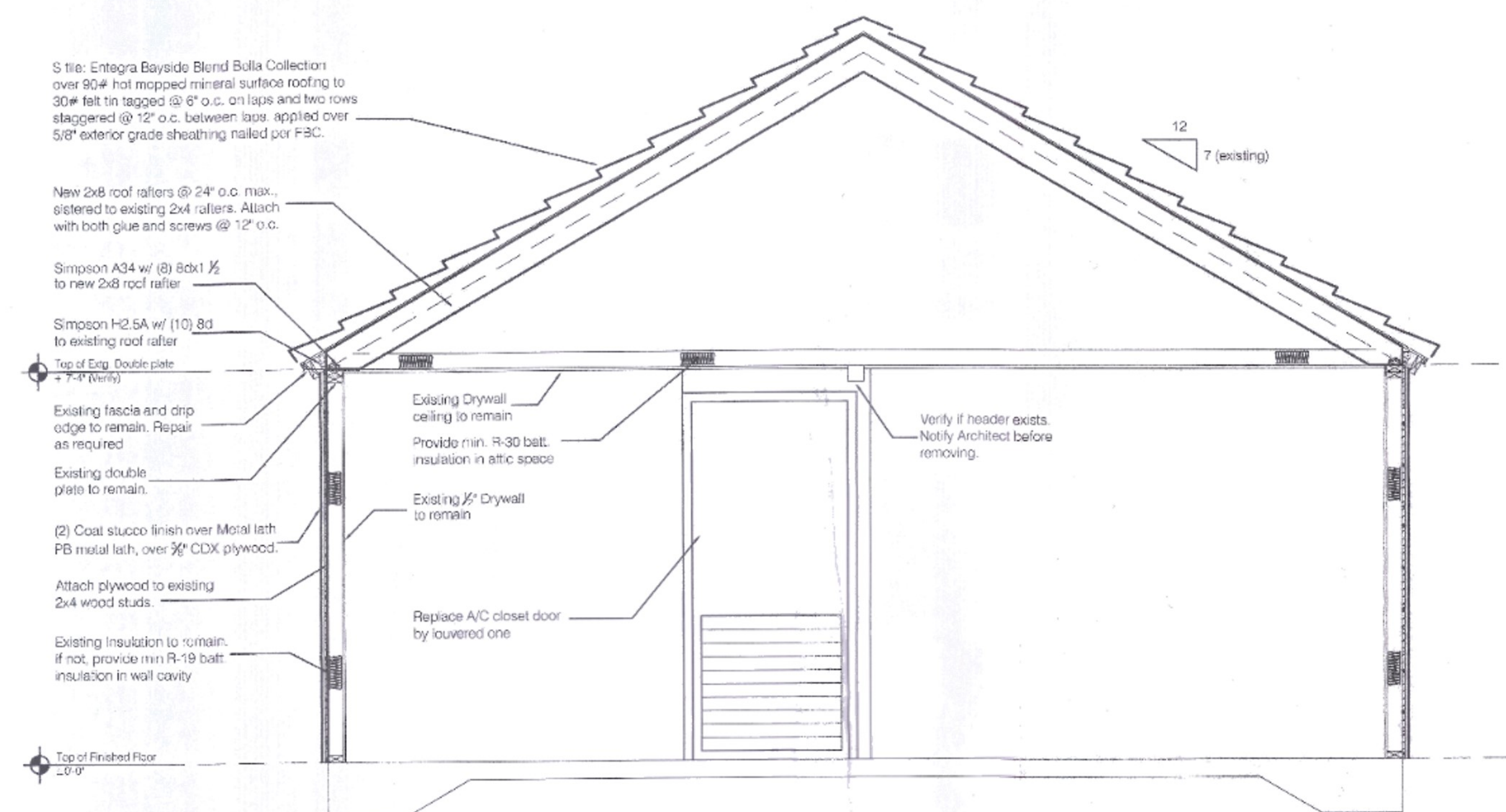
A line drawing of a skirt, likely a pleated skirt, showing the waistband, pleats, and a side seam. The drawing is simple and schematic, typical of a technical sketch for a garment.

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A-A Section Demo

1/2" = 1' Scale



A-A Section

1/2" = 1' Scale

Wood frame wall notes:

Moisture barrier

Wood shall be covered with 15-pound (9 kg) roofing felt, or other approved equally moisture-resisting layer, and metal reinforcement as set forth herein.

Metal reinforcement

All metal lathing shall be lapped not less than 1 inch (25 mm).

Metal reinforcement shall be furrowed out from the backing by an approved method.

Fastenings into wood sheathing or wood framing shall be by galvanized nails, with heads not less than 3/8 inch (9.5 mm) in diameter, driven to full penetration, using a minimum of two nails per square foot (0.093 m²), or by approved staples having equal resistance to withdrawal.

The fastening of rib lath to metal members shall be by #8 galvanized sheet-metal screws, using a minimum of two screws per square foot (0.093 m²).

Water resistive barriers:

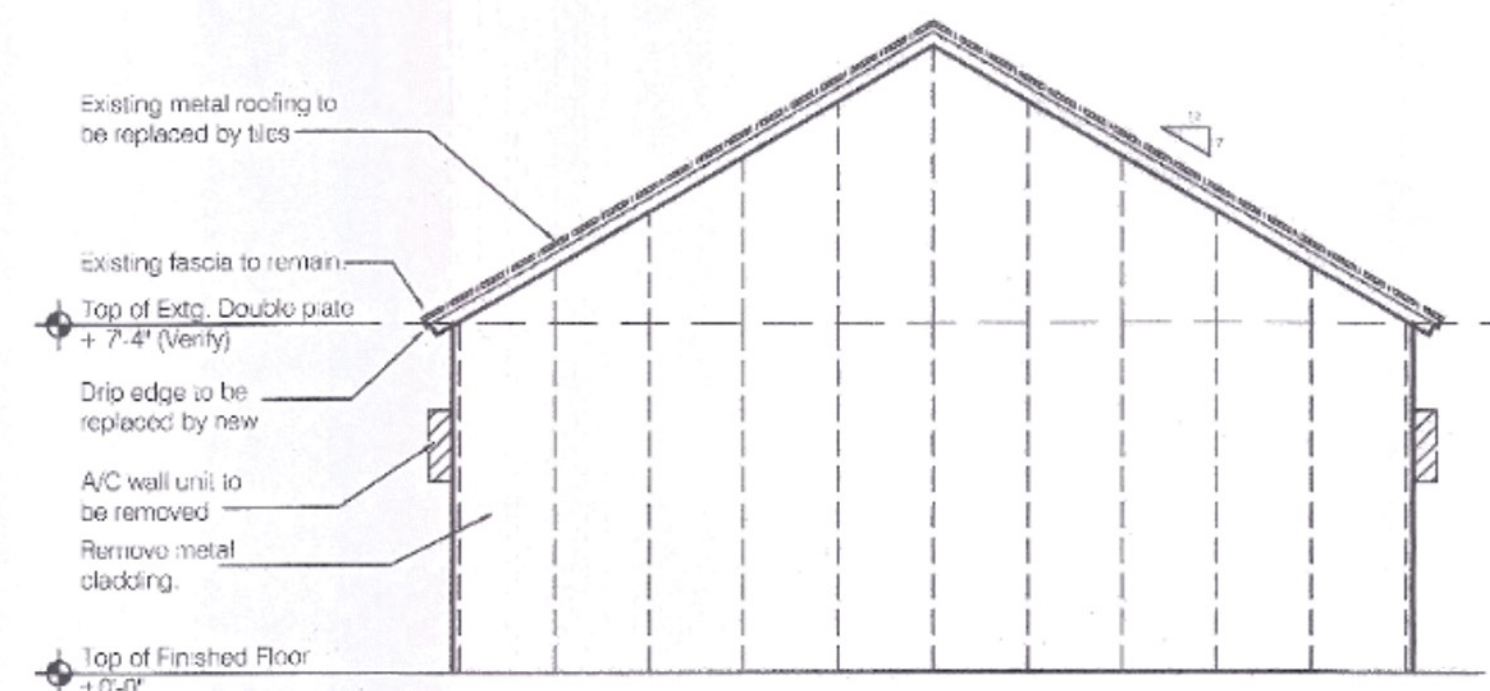
Walls (wood frame) with stucco
Tyvek "Home Wrap"

Window/Door Flashing System
Duroplast "Straight flash"

Tapcon Substitution Note:

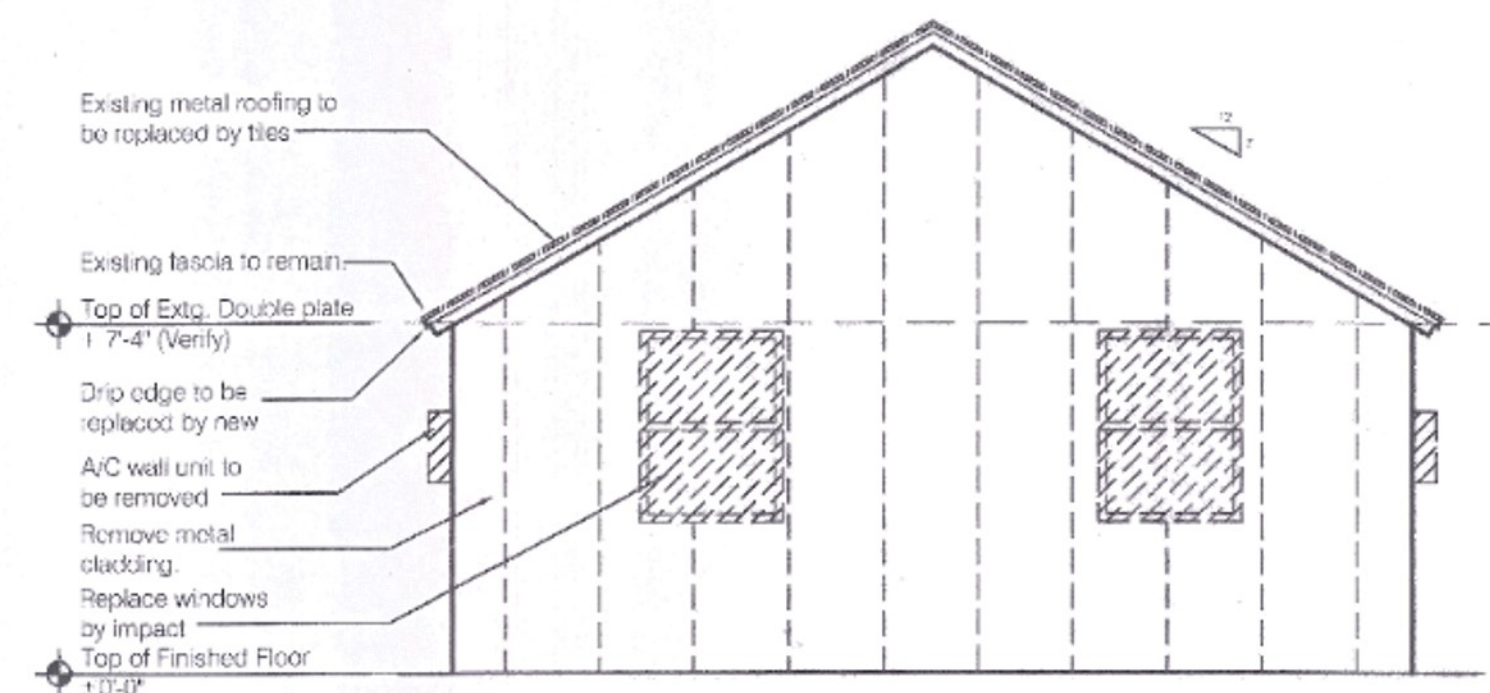
Anchoring required into existing tie beams can substitute (3) 1/4" Tapcons with a minimum 1-1/4" Embedment in place of rebar required

All exposed metal fasteners to be hot dipped galvanized or stainless steel.



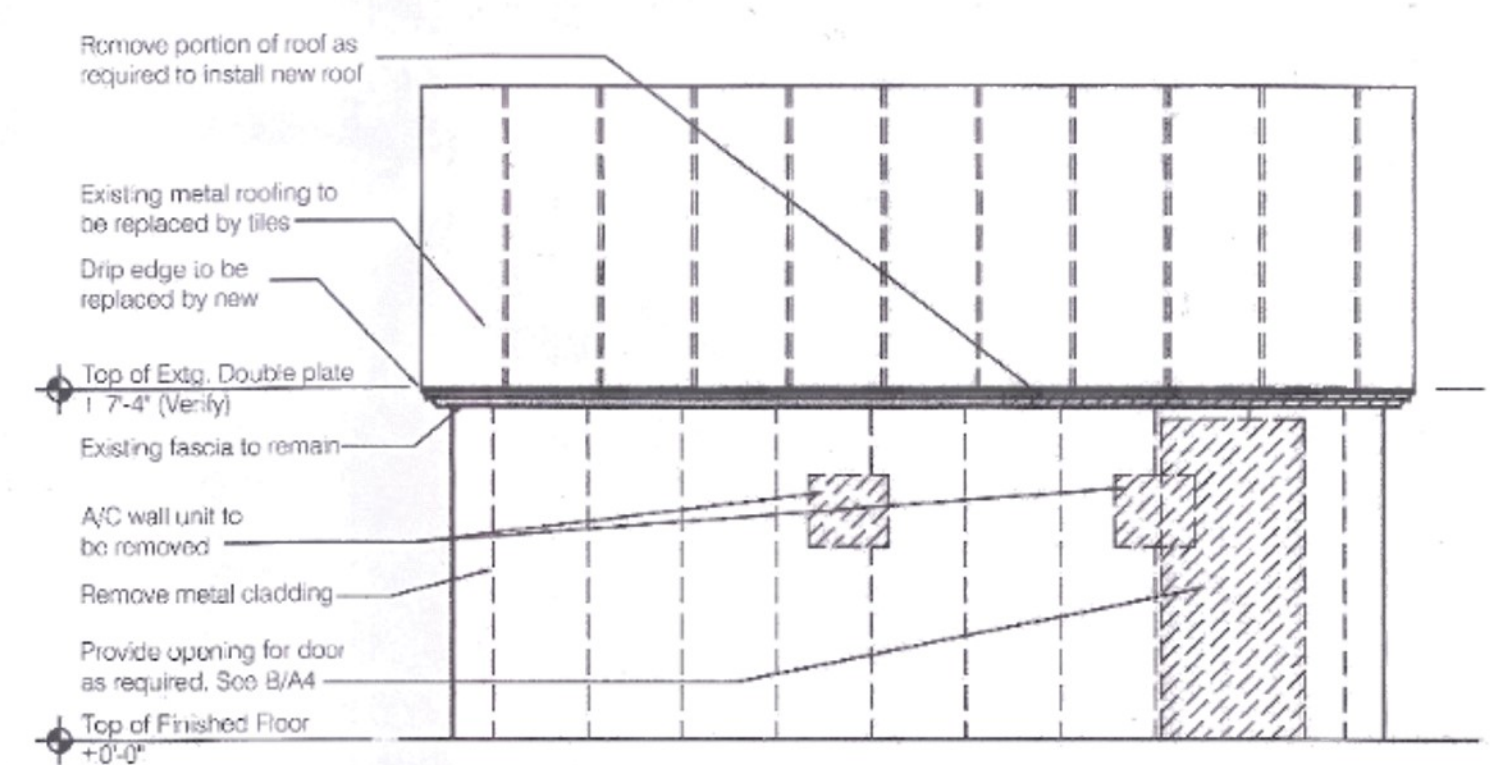
West Elevation Demo

1/4" = 1' Scale



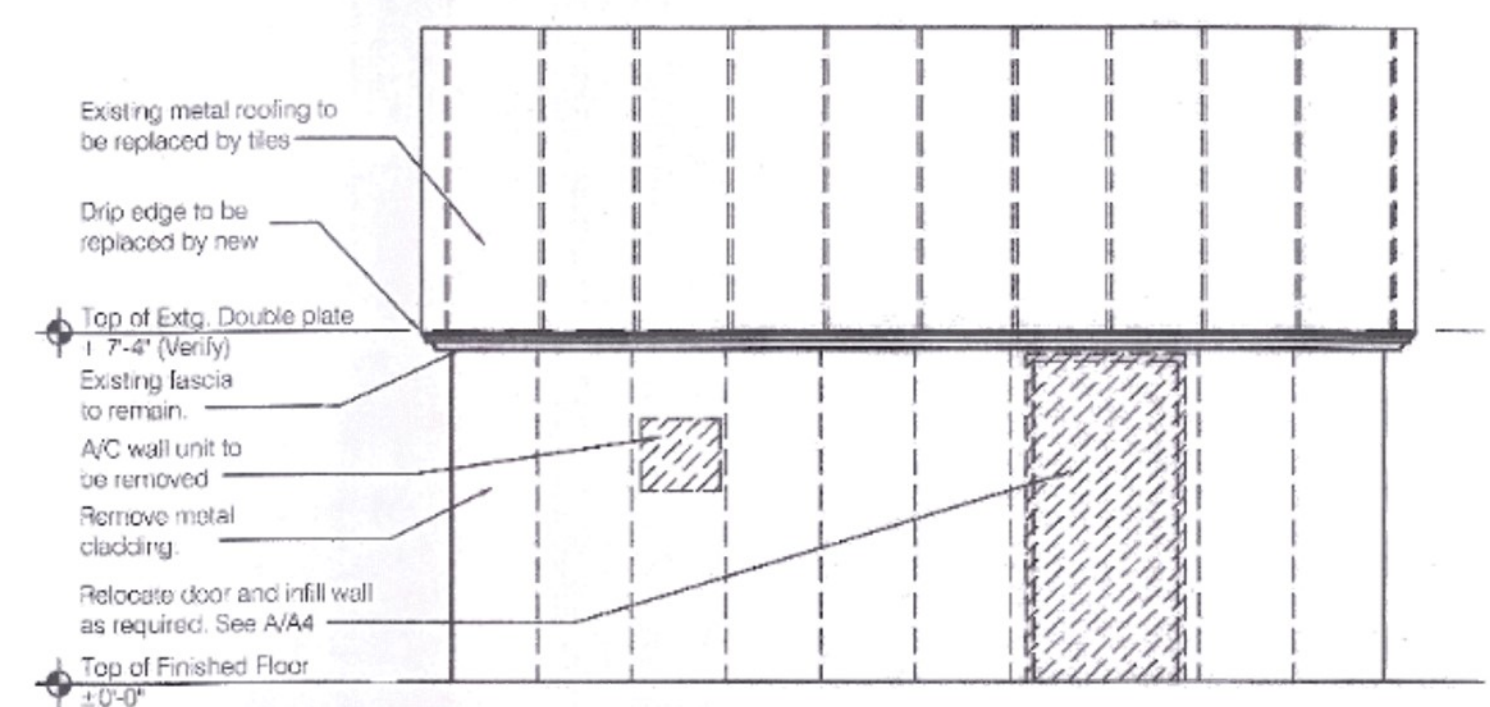
East Elevation Demo

1/4" = 1' Scale



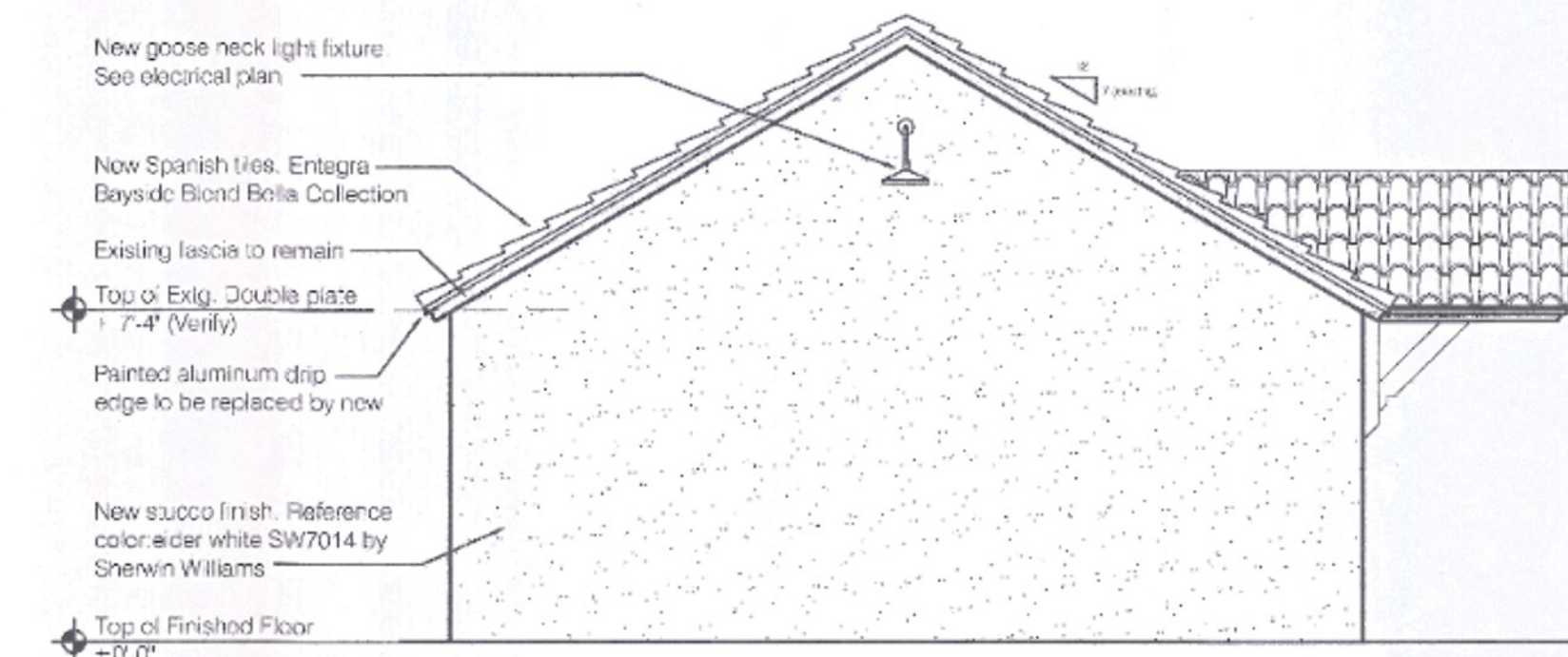
South Elevation Demo

1/4" = 1' Scale



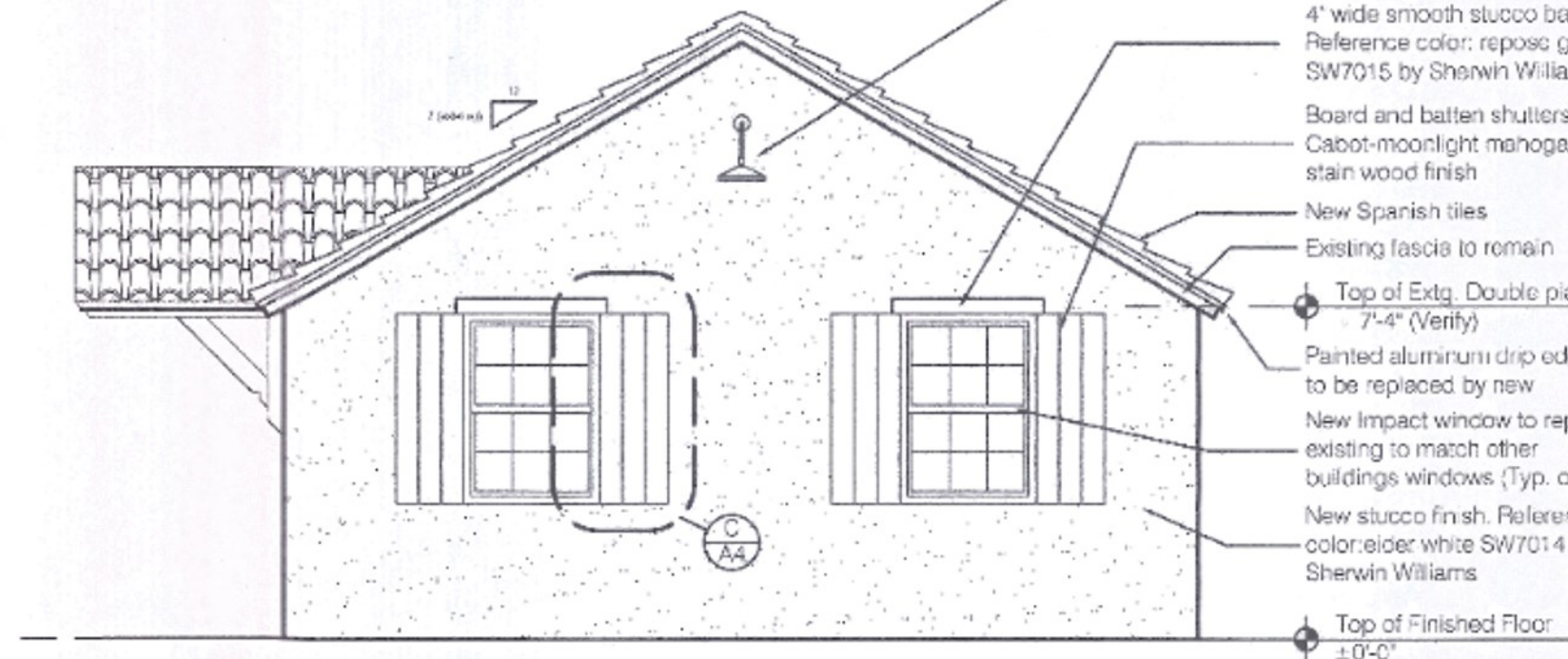
North Elevation Demo

1/4" = 1' Scale



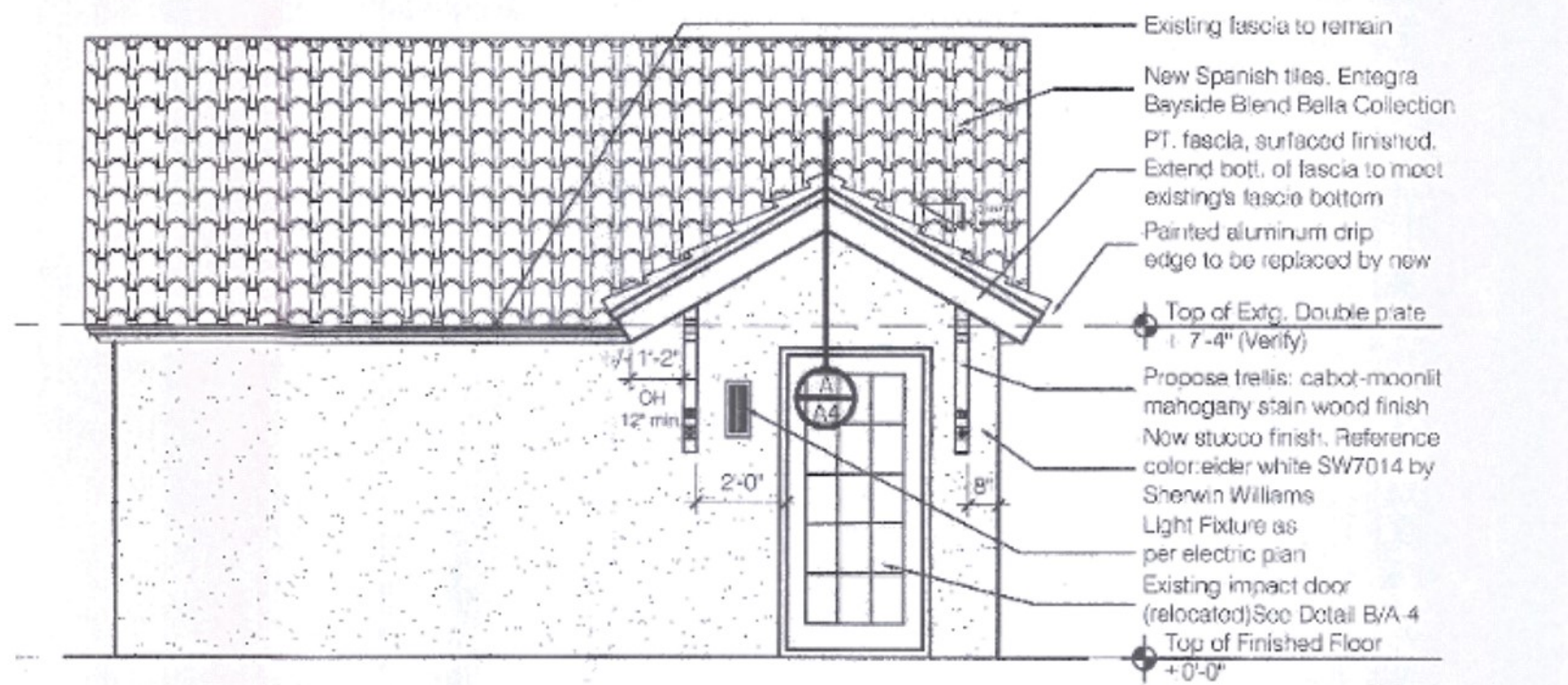
West Elevation

1/4" = 1' Scale



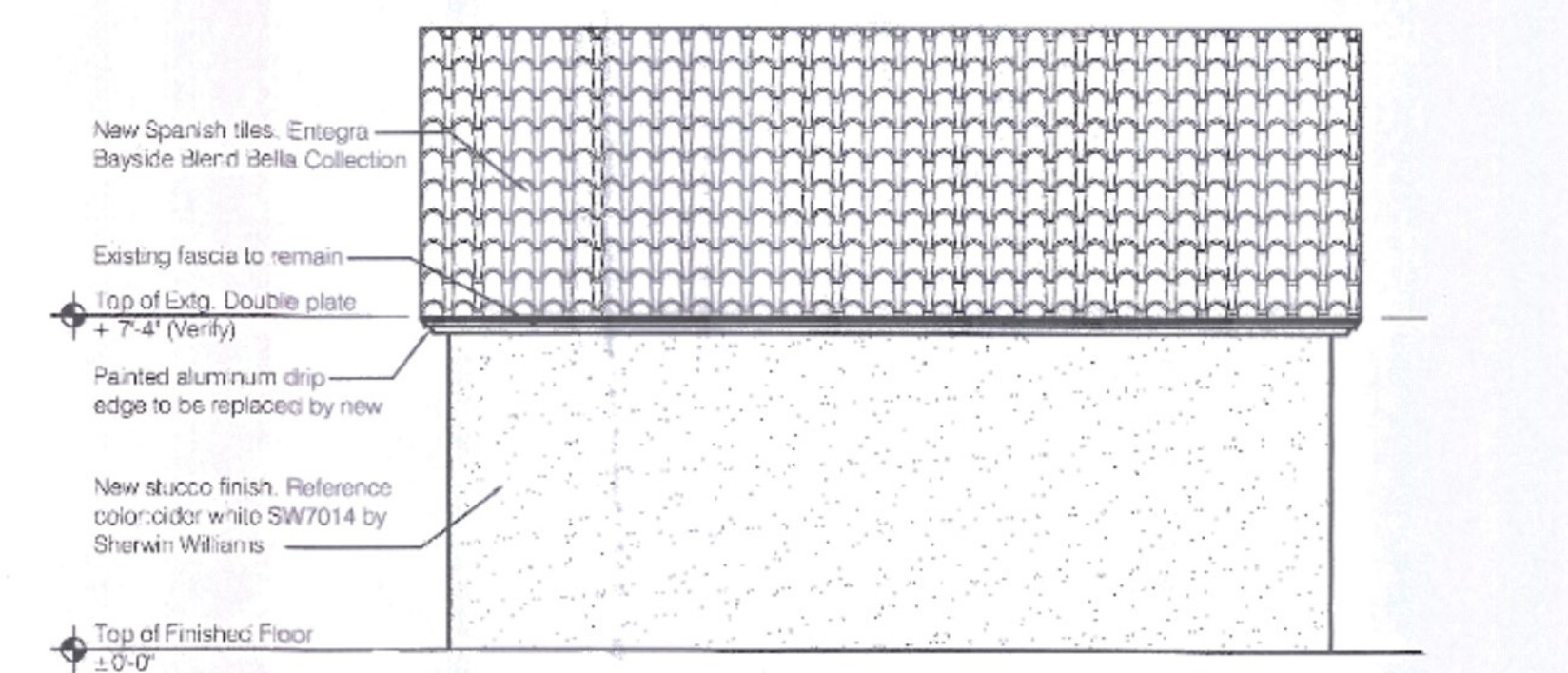
East Elevation

1/4" = 1' Scale



South Elevation

1/4" = 1' Scale



North Elevation

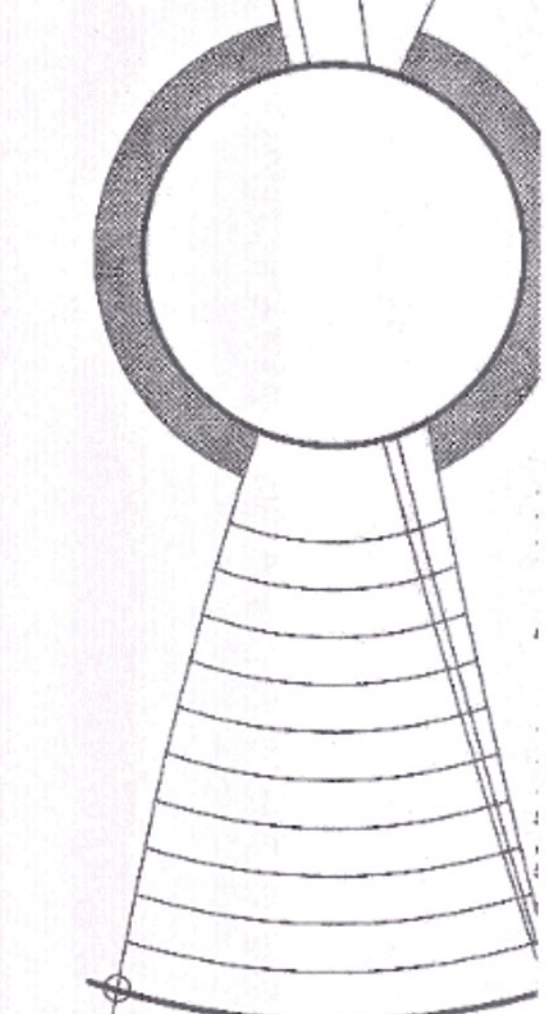
1/4" = 1' Scale



LEVEL 2 ALTERATION
REMODELING FOR:
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1237 N. FEDERAL HWY
DELRAY BEACH, FL.

JOHN SHERMAN REED
ARCHITECT
FL LICENSE # AR0517

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16516

A3