

Planning, Zoning, and Building Department

BOARD ACTION REPORT - APPEALABLE ITEM

Project Name: AutoNation Delray
Project Location: 1311 Linton Boulevard

Request: Class V Site Plan, Landscape Plan, and Architectural Elevations.

Board: Site Plan Review and Appearance Board

Meeting Date: February 22, 2017

Board Actions: Approved the Class V Site Plan (2016-268) with a condition on a

5 to 0 vote (Chard/Kaub Absent); approved the Architectural Elevations on a 5 to 0 vote; approved the Landscape Plan on a 5

to 0 vote with a condition.

Project Description:

The subject property consists of Tract "A" of the Wallace Ford subdivision and consists of 10.51 acres. The subject property is located on the northwest corner of Linton Blvd. and Wallace Drive and is zoned Automotive Commercial (AC). The property consists of an existing automobile dealership (Wallace Ford) that was constructed in 1985, and annexed into the City in 1988 via Ordinance 10-88.

The development proposal consists of the demolition of all existing improvements on the property including multiple buildings, parking facilities, and landscaping. A new 105,400 square foot full service automobile dealership will be constructed on the property.

Board comments:

The Board was supportive of the project and had minimal comments.

Public Comments:

No members of the public spoke for or against the project.

Associated Actions:

All required actions were taken.

Next Action:

SPRAB action is final unless appealed by the City Commission.

SITE PLAN REVIEW AND APPEARANCE BOARD

CITY OF DELRAY BEACH

---STAFF REPORT---

MEETING DATE:

February 22, 2017

AGENDAITEM:

AutoNation Delray Beach (File #2016-268) - Class V Site Plan, Landscape Plan and Architectural Elevations associated with construction of a 105,400 square foot

automobile dealership at 1311 West Linton Boulevard.

Recommendation: Approve

GENERAL DATA:

Agent......Sol-Arch

Location...... Northwest corner of West Linton

Boulevard and Wallace Drive

Existing FLUM...... GC (General Commercial)

Existing Zoning...... AC (Automotive Commercial)

Adjacent Zoning......North: MIC (Mixed Industrial &

Commercial) and I (Industrial)

East: AC

South: PC (Planned Commercial)

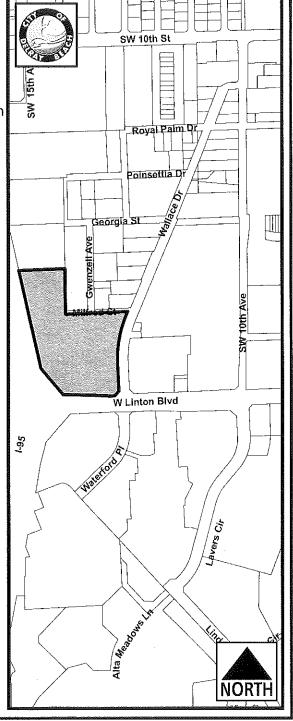
West: CD (Conservation District)

Existing Land Use...... Automobile Dealership

Proposed Land Use...... Automobile Dealership

Water..... Available on-site

Sewer..... Available on-site



ITEM BEFORE THE BOARD
The item before the Board is the approval of the following aspects of a Class V Site Plan request for AutoNation Delray Beach , pursuant to Land Development Regulations (LDR) Section 2.4.5(F):
☐ Site Plan; ☐ Landscape Plan; and ☐ Architectural Elevations;
The subject property is located at the northwest corner of West Linton Boulevard and Wallace Drive (1311 Linton Boulevard).
BACKGROUND ANALYSIS
The subject property consists of Tract "A" of the Wallace Ford subdivision and consists of 10.51 acres. The subject property is located on the northwest corner of Linton Blvd. and Wallace Drive and is zoned Automotive Commercial (AC). The property consists of an existing automobile dealership (Wallace Ford) that was constructed in 1985, and annexed into the City in 1988 via Ordinance 10-88. Now before the Board for consideration is a Class V Site Plan proposal for AutoNation Delray Beach.
PROJECT DESCRIPTION
The development proposal consists of the following:
Demolition of all existing improvements on the property including multiple buildings, parking facilities, and landscaping.
☐ Construction of a 105,400 square foot full service automobile dealership.
☐ Reconfiguration of the parking area to accommodate the redevelopment.
☐ Installation of associated landscaping.
SITE PLAN ANALYSIS

COMPLIANCE WITH THE LAND DEVELOPMENT REGULATIONS:

Items identified in the Land Development Regulations shall specifically be addressed by the body taking final action on the site and development application/request.

LDR Section 4.3.4(K) Development Standards Matrix - Nonresidential Zoning Districts:

The following table indicates that the site complies with LDR Section 4.3.4(K) as it pertains to the Automobile Commercial (AC) zoning district and LDR Section 4.4.10:

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		Standard:	Provided:
Minimum Lot Size		1.5 acres	10.51 acres
Minimum Lot Width		125'	330.73'
Minimum Lot Depth		200'	847'
Minimum Lot Frontage		125'	330.73'
Building Height:		48'	31'4"
Building Setbacks:	Front	15'	157.69'
	Rear	10'	80.36
	Side Street	15'	178.38'
	Side Interior	0'	96.31'
Open Space:		25%	26.5%

LDR Article 4.6 – Supplemental District Regulations:

Parking:

Per LDR Section 4.6.9(C)(f), the automobile dealership is required to provide 4 spaces per 1,000 square feet of total building gross floor area, except indoor display areas. Required parking spaces shall be designated for employee, customer, and/or service use at the standard of, at least, 1.5 spaces per service bay and 2 spaces per 1,000 sq.ft. of gross floor area (less indoor display area). Any remaining spaces may be used for display purposes. The gross floor area of the building minus the indoor display area is 95,605 square feet. Based on this, the required parking for the development is 382 parking spaces. The development complies with the minimum required parking since 811 spaces are proposed. The required parking for employee, customer, and/or service use is 184 spaces. The development proposal complies with this requirement since a total of 184 spaces are designated on the plan for these purposes. The remaining spaces (627) are designated for display or bull pen parking. Based on the above, the development proposal complies with the City's parking standards for automobile dealerships.

Handicap Accessible Parking:

Pursuant to LDR Section 4.6.9(C)(1)(b), special parking spaces designed for use by the handicapped shall be provided pursuant to the provisions of Florida Accessibility Code for Building Construction. Pursuant to the Florida Accessibility Code for Building Construction, six handicap accessible parking spaces are required for the 184 customer, employee, and service parking space. The proposed development complies with this requirement since eight handicap accessible parking spaces are provided.

Special Landscape Setback:

Pursuant to LDR Section 4.3.4(H)(6)(b)(4), a 30-foot special landscape area is required along Linton Boulevard. The required area has been provided and accommodated on the plans.

Right-of-way Dedications:

Pursuant to Table T-1 of the Transportation Element of the Comprehensive Plan, a 53' right-of-way exists on Linton Boulevard where sixty feet (60') is required along the north side of the road. A 7-foot right-of-way dedication will be required and is accommodated on the plans. The applicant will need to replat the subject property to accommodate the right-of-way dedications

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and associated easements for the proposed redevelopment and this is attached as a condition of approval.

Other Issues:

Outside Display:

Per LDR Section 4.4.10(G)(1)(a), outside display areas for sale, lease, or rental of vehicles shall be separated from all adjacent streets, asphalt, or vehicular use areas by a hedge or berm at least three feet high and trees planted at least 40 feet on center. However, the hedge may be planted at such a location that at least 24 inches of the hedge is above the finished grade of the adjacent parking area. The proposed development provides a 30" high Cocoplum hedge along Linton Boulevard and a 24" high Cocoplum hedge along Wallace Drive. Further, the plan provides a mixture of Royal Palms and Crepe Myrtle planted 40 feet along Linton Boulevard and Live Oak trees planted 20 feet to 40 feet along Wallace Drive. Based on the above, the development proposal complies with this requirement.

Lighting:

Per LDR Section 4.4.10(G)(6), exterior light fixtures cannot exceed 25 feet in height. Lighting within display areas cannot exceed 100 foot candles or 40 foot candles within all other areas. The lighting within the display areas shall be reduced to 50 foot candles after 11:00 p.m. The maximum lighting level in the display area is 25.5 foot candles and 17 foot candles in all other areas. Further the height of the light fixtures is 24.5 feet. Based on the above, the development proposal complies with these lighting requirements.

Bicycle Parking:

Per Policy D-2.2. of the Comprehensive Plan, Transportation Element, bicycle parking and facilities shall be required on all new development and redevelopment. The development complies with this requirement since a bike rack has been provided on the east side of the proposed building.

LANDSCAPE PLAN ANALYSIS

Pursuant to LDR section 4.6.16(C)(1)(a), prior to the issuance of a building permit for a structure or a paving permit, compliance with the requirements of Section 4.6.16 shall be assured through the review and approval of a landscape plan submitted pursuant to Section 2.4.3(C). A landscape plan has been submitted and evaluated by the City's Senior Landscape Planner and found that the landscape plan will be in compliance with applicable requirements of LDR section 4.6.16, subject to the following:

Landscape Islands:

Per LDR Section 4.6.16(H)(3)(i), landscape islands which contain a minimum of 135 square feet of planting area, with a minimum dimension of 9 feet, exclusive of the required curb, shall be placed at intervals of no less than one landscaped island for every 13 standard parking spaces. One shade tree shall be planted in every island with a minimum of 75 square feet of shrubs and groundcovers. Tree specifications shall adhere to those listed in Section 4.6.16(E)(5) and 4.6.16(E)(6). Where approval for the use of compact parking has been approved, islands may be placed at intervals of no less than one 1 island for every 15 compact parking spaces. This landscape island is required for the display parking spaces along Linton Boulevard, where 27

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and 14 parking spaces are provided in a row without the required landscape island. A condition of approval is attached that the plans be revised to provide the required interval of landscape islands and landscaping.

ARCHITECTURAL ELEVATIONS ANALYSIS

<u>LDR Section 4.6.18(E) – Criteria for Board Action:</u> The following criteria shall be considered, by the Site Plan Review and Appearance Board (SPRAB), in the review of plans for building permits. If the following criteria are not met, the application shall be disapproved:

- 1. The plan or the proposed structure is in conformity with good taste, good design, and in general contributes to the image of the City as a place of beauty, spaciousness, harmony, taste, fitness, broad vistas, and high quality.
- 2. The proposed structure, or project, is in its exterior design and appearance of quality such as not to cause the nature of the local environment or evolving environment to materially depreciate in appearance and value.
- 3. The proposed structure, or project, is in harmony with the proposed developments in the general area, with the Comprehensive Plan, and with the supplemental criteria which may be set forth for the Board from time to time.

The building is a contemporary architectural design. The southern elevation primarily consists of a glass curtain wall with flat canopies. The west, north, and east elevations also have small glass wall applications. The building will be painted Pure White and the curtain wall will be Sterling Gray.

All proposed architectural elevations will be compatible and harmonious with the surrounding properties and an enhancement to the area. Based upon the above, positive findings can be made with regard to the criteria listed in LDR Section 4.6.18(E).

REQUIRED FINDINGS

Pursuant to LDR Section 3.1.1, prior to the approval of development applications, certain findings must be made in a form which is part of the official record. This may be achieved through information on the application, written materials submitted by the applicant, the staff report, or minutes. Findings shall be made by the body which has the authority to approve or deny the development application. These findings relate to the following areas:

LDR Section 3.1.1(A) - Future Land Use Map:

The subject property has a zoning designation of Automotive Commercial (AC) and a General Commercial (GC) Future Land Use Map designation. Pursuant to the Land Use Designation/Zoning Matrix (Table L-7, Future Land Use Element), AC Zoning is consistent with GC Future Land Use Map designation. Pursuant to LDR Section 4.4.10(B)(1), full service automotive dealerships are permitted within the AC zoning district. Based upon the above, a positive finding can be made with respect to consistency with the Future Land Use Map designation.

LDR Section 3.1.1(B) - Concurrency:

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As described in Appendix "B", a positive finding of Concurrency can be made as it relates to water and sewer, streets and traffic, drainage, parks and recreation, open space, solid waste, and schools.

LDR Section 3.1.1(C) - Consistency:

As described in Appendix "C", a positive finding of Consistency can be made as it relates to Standards for Site Plan Actions.

LDR Section 3.1.1(D) - Compliance with the Land Development Regulations:

As described under the Site Plan Analysis of this report, a positive finding of compliance with the LDRs can be made when all outstanding items attached as conditions of approval are addressed.

Required Findings:

Pursuant to LDR Section 2.4.5(F)(5), the approving body must make a finding that development of the property pursuant to the site plan will be compatible and harmonious with adjacent and nearby properties and the City as a whole, so as not to cause substantial depreciation of property values.

The following table indicates the zoning and land use of the properties surrounding the subject property:

	Adjacent Zoning	Adjacent Land Use				
North	MIC (Mixed Industrial and Commercial) &	Single Family Residential and Industrial				
	I (Industrial)					
South	PC (Planned Commercial)	Linton Center Commercial Plaza				
East	AC (Automotive Commercial)	Wallace Nissan Automobile Dealership				
West	CD (Conservation District	Scrub Land (Across I-95)				

Comprehensive Plan Policies:

A review of the adopted Comprehensive Plan was conducted and the following applicable objectives and policies are noted:

Future Land Use Element Objective A-1: Property shall be developed or redeveloped in a manner so that the future use and intensity is appropriate and complies in terms of soil, topographic, and other applicable physical considerations, is complimentary to adjacent land uses, and fulfills remaining land use needs.

There are no known issues with respect to soil, topography, and other applicable physical consideration that prevent redevelopment of the existing automobile dealership to a new dealership. The new automobile dealership will be complimentary to the surrounding uses. A positive finding can be made with regard to Future Land Use Element Objective A-1.

REVIEW BY OTHERS

The development proposal is not in a geographic area requiring review by the Historic Preservation Board (HPB), Downtown Development Authority (DDA), or the Community Redevelopment Agency (CRA).

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<u>Courtesy Notices:</u> A special courtesy notice was provided to the following homeowners associations:

- Southridge HOA
- Woods of Southridge HOA

Letters of objection, if any, will be presented at the Site Plan Review and Appearance Board (SPRAB) meeting.

ASSESSMENT AND CONCLUSION

The development proposal is to construct a 105,400 square foot full service automobile dealership to replace the existing automobile dealership. The proposed development will be compatible with the surrounding uses.

ALTERNATIVE ACTIONS

- A. Move postponement of the Class V site plan, landscape plan and architectural elevations for **AutoNation Delray Beach**, by electing to continue with direction.
- B. Move approval of the Class V site plan, landscape plan and architectural elevations for **AutoNation Delray Beach**, by adopting the findings of fact and law contained in the staff report, and finding that the request is consistent with the Comprehensive Plan and meets the criteria set forth in Sections 2.4.5(F)(5) and Chapter Three of the Land Development Regulations, subject to the attached condition of approval.
- C. Move denial of the Class V site plan, landscape plan and architectural elevations for **AutoNation Delray Beach**, by adopting the findings of fact and law contained in the staff report, and finding that the request is inconsistent with the Comprehensive Plan and does not meet the criteria set forth in Sections 2.4.5(F)(5) and Chapter Three of the Land Development Regulations.

STAFF RECOMMENDATION

By Separate Motions:

Site Plan:

Move approval of the Class V site plan for **AutoNation Delray Beach**, by adopting the findings of fact and law contained in the staff report, and finding that the request is consistent with the Comprehensive Plan and meets the criteria set forth in Section 2.4.5(F)(5) and Chapter Three of the Land Development Regulations, subject to the condition that a recorded plat be submitted prior issuance of a building permit.

Landscape Plan:

Move approval of the landscape plan for **AutoNation Delray Beach**, by adopting the findings of fact and law contained in the staff report, and finding that the request is consistent with Section 4.6.16 of the Land Development Regulations, subject to the condition that the plans be revised to provide the required interval of landscape islands and landscaping prior to certification of the site plan.

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Architectural Elevations:

Move approval of the architectural elevations for AutoNation Delray Beach, by adopting the findings of fact and law contained in the staff report, and finding that the request meets the criteria set forth in Section 4.6.18(E) of the Land Development Regulations.

Attachments: Appendix "A"

Appendix "B"

Site Plan, Landscape Plan, Building Elevations

Location Map

Staff Report Prepared by:

Scott Pape, Principal Planner

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APPENDIX "A" CONCURRENCY FINDINGS

Pursuant to LDR Section 3.1.1(B), Concurrency, as defined pursuant to Objective B-2 of the Land Use Element of the Comprehensive Plan, must be met and a determination made that the public facility needs of the requested land use and/or development application will not exceed the ability of the City to fund and provide, or to require the provision of, needed capital improvements for the following areas:

Water and Sewer:

- Water service will be provided from an existing 12" main along Wallace Drive
- Sewer service will be provided from an existing 8" main along Milfred Street

Pursuant to the City's Comprehensive Plan, treatment capacity is available at the City's Water Treatment Plant and the South Central County Waste Water Treatment Plant for the City at build-out.

Streets and Traffic:

A traffic study has been provided for the development proposal indicating that the net increase in traffic is 949 average daily trips, 56 net new a.m. peak hour trips and 77 net new p.m. peak hour trips per day. The Palm Beach County Traffic Division has reviewed the traffic study for the proposed development and has provided a finding of concurrency.

Parks and Recreation Facilities:

Park dedication requirements do not apply for non-residential uses.

Solid Waste:

The proposed 105,400 automobile dealership will generate 484.84 tons of solid waste per year. The Solid Waste Authority has indicated that its facilities have sufficient capacity to accommodate all development proposals until 2047.

Schools:

School concurrency findings do not apply for non-residential uses.

Drainage:

Drainage will be accommodated on site via an exfiltration trench system with an outfall to dry retention areas along the south and west sides of the property. There should be no impact on drainage as it relates to this standard.

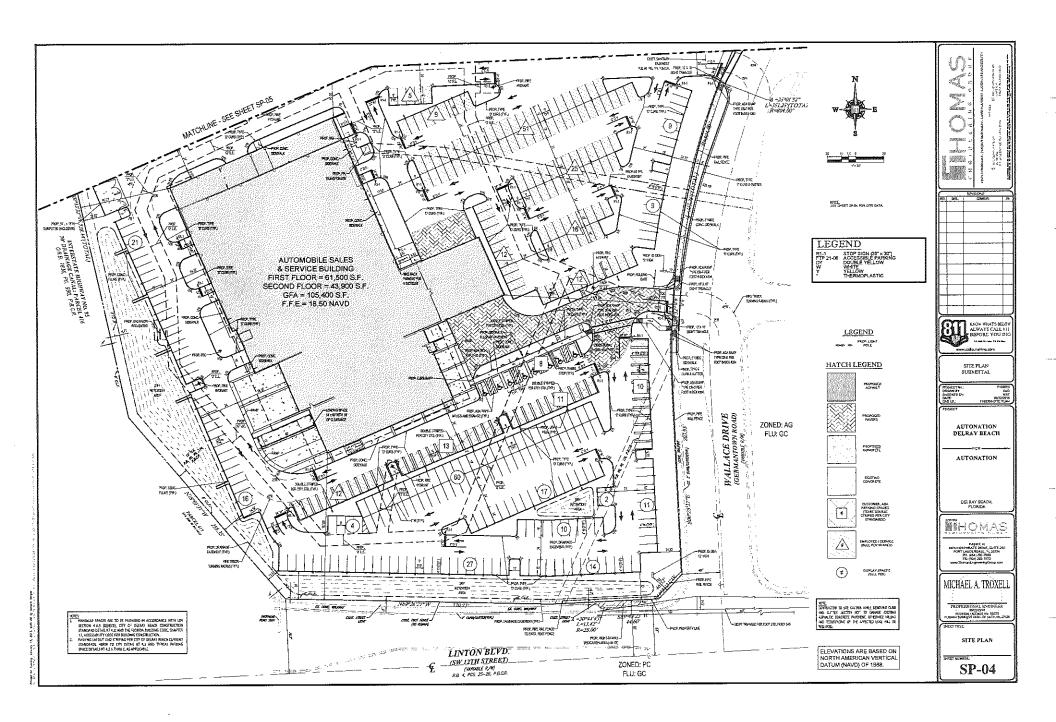
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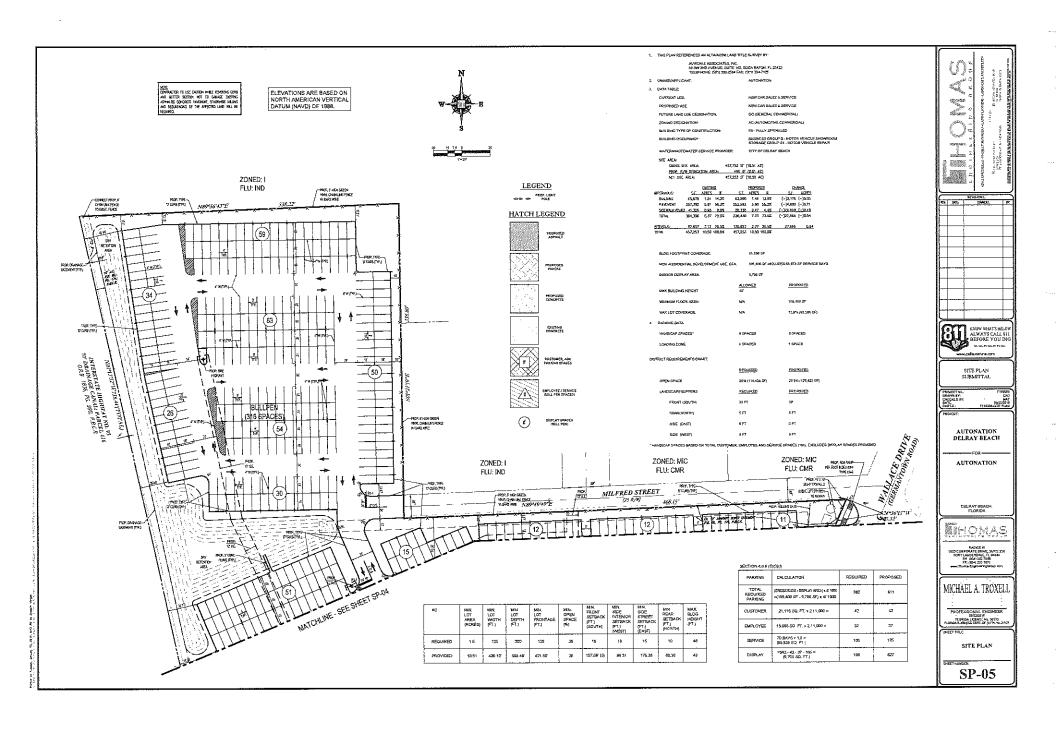
	APPENDIX "B" STANDARDS FOR SITE PLAN ACTIONS
A.	Building design, landscaping and lighting (glare) shall be such that they do not create unwarranted distractions or blockage of visibility as it pertains to traffic circulation.
	Not applicable Meets intent of standard X Does not meet intent
B.	Separation of different forms of transportation shall be encouraged. This includes pedestrians, bicyclists and vehicles in a manner consistent with policies found under Objectives D-1 and D-2 of the Transportation Element.
	Not applicable Meets intent of standard Does not meet intent
C.	Open space enhancements as described in Policies found under Objective B-1 of the Open Space and Recreation Element are appropriately addressed.
	Not applicable X Meets intent of standard Does not meet intent
D.	The City shall evaluate the effect that any street widening or traffic circulation modification may have upon an existing neighborhood. If it is determined that the widening or modification will be detrimental and result in a degradation of the neighborhood, the project shall not be permitted.
	Not applicable Meets intent of standard Does not meet intent X
E.	Development of vacant land which is zoned for residential purposes shall be planned in a manner which is consistent with adjacent development regardless of zoning designations.
	Not applicable X Meets intent of standard Does not meet intent

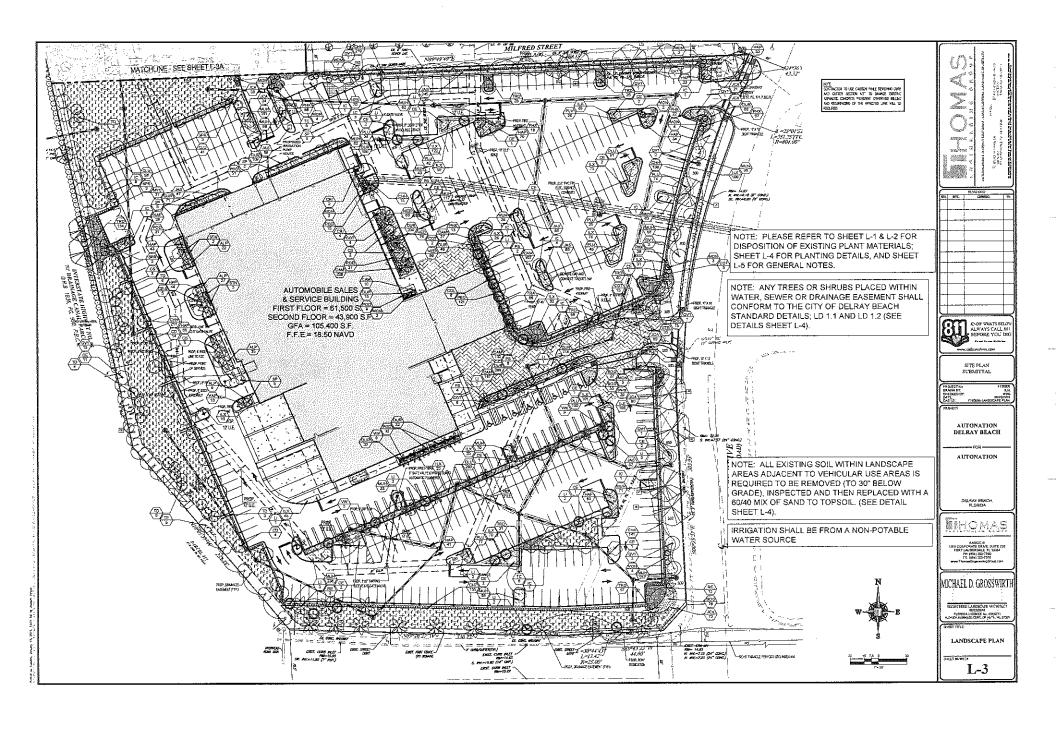
Page 10 F. Property shall be developed or redeveloped in a manner so that the future use and intensity are appropriate in terms of soil, topographic, and other applicable physical considerations; complementary to adjacent land uses; and fulfills remaining land use needs. Not applicable Meets intent of standard X Does not meet intent G. Redevelopment and the development of new land shall result in the provision of a variety of housing types which shall continue to accommodate the diverse makeup of the City's demographic profile, and meet the housing needs identified in the Housing Element. This shall be accomplished through the implementation of policies under Objective B-2 of the Housing Element. Not applicable X Meets intent of standard Does not meet intent H. The City shall consider the effect that the proposal will have on the stability of nearby neighborhoods. Factors such as noise, odors, dust, traffic volumes and circulation patterns shall be reviewed in terms of their potential to negatively impact the safety. habitability and stability of residential areas. If it is determined that a proposed development will result in a degradation of any neighborhood, the project shall be modified accordingly or denied. Not applicable Meets intent of standard Does not meet intent 1. Development shall not be approved if traffic associated with such development would create a new high accident location, or exacerbate an existing situation causing it to become a high accident location, without such development taking actions to remedy the accident situation. Not applicable Meets intent of standard Does not meet intent J. Tot lots and recreational areas, serving children from toddler to teens, shall be a feature of all new housing developments as part of the design to accommodate households having a range of ages. This requirement may be waived or modified for residential developments located in the downtown area, and for infill projects having fewer than 25 units. Not applicable Meets intent of standard Does not meet intent

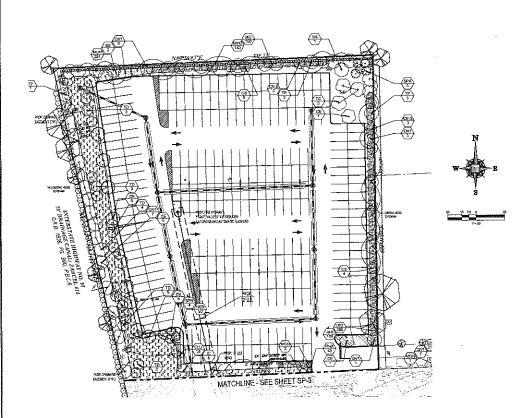
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NOTE: ALL EXISTING SOIL WITHIN LANDSCAPE AREAS ADJACENT TO VEHICULAR USE AREAS IS REQUIRED TO BE REMOVED (TO 30" BELOW GRADE), INSPECTED AND THEN REPLACED WITH A 60/40 MIX OF SAND TO TOPSOIL. (SEE DETAIL SHEET L-4).

NOTE: ANY TREES OR SHRUBS PLACED WITHIN WATER, SEWER OR DRAINAGE EASEMENT SHALL CONFORM TO THE CITY OF DELRAY BEACH STANDARD DETAILS; LD 1.1 AND LD 1.2 (SEE DETAILS SHEET L-4).

NOTE: PLEASE REFER TO SHEET L-1 & L-2 FOR DISPOSITION OF EXISTING PLANT MATERIALS: SHEET L-4 FOR PLANTING DETAILS, AND SHEET L-5 FOR GENERAL NOTES.

IRRIGATION SHALL BE FROM A NON-POTABLE WATER SOURCE

TREES	QTY	CONSUDIL NAME	POTANICAL NAME		CALADOH	HBGHT			XERIC	REWARKS
56	3	Gumba Limbo	Bursera simorusa	FG/94.8	2.5°Cal	12 Hr	6	Yes	Hgh	g.ct
D	72	Pigean Plum	Coccioba diversificia	FG/BAB	2.5°CN	sž Hr	e.	Yos	Hiph	e ct
Œ	a1	Buttonwood	Conocarpus arectus	FC/B&B	2.5°Cal	72' Hi	6	Yes	High	e e
Æ-5	io	Silver Button Vitagal	Conocarpus arectus tericevis	45 gas	5.2.CFI	12" Ht	45	Yes	High	4° CT
CR CR	1D	Balsom Apple	Chicke roses 'Prich Apple'	FG/BAR	25°C#	12 H	ā.	Yes	High	8°14
ĊS .	1P	Orange Geger Tree	Corda sobormes	42 am	2.5°C#	1T HI	5	Yes	High	4. Ct
.c	22	Dahorn Hoty	llex sasserie	-cress	2.5°C±	1Z Hi	5'	Yes	High	
1	28	Crepe Myrtis	Lagenstreents Indica "Tusknape"	45 gal	2.5°Cai	12' Ht		No	High	standard 4°d
DV:	51	Snottern Law Cak	Diserces virginard	FOREB	25°Cal	12 HL	6	Yes		e टा
YC	14	Yellow Trumper Toro	Tabebula caralte	FG/B&B	25°C#	12' Hi		No		0'd
TD	38	Sald Cypress	Taxoo um distrohum	45 gal	2.5°Cal	12 Hi	4.5	Yes		4a
TP			Tabebus padris	FGB4B	2.5 Cal	12 Ht	6.	No		FC.
IP.	16	Pink Tabebira	Actions brown	Palaca	TO ME	12 14		100	7.1-2-1	
PALM TREES	OTY	COMMON NAVE	BOTANICAL NAME	SPECIFICATIONS	CAUD6H	HEIGHT	SPREAD	NATIVE	XERIC	REMARKS
Ps .	10	Wild Date Palm		9.56	1	10 at		Yes	Medium	Wood to nut
rs RE	15	Flores Royal Palm		BSE	1	20' gw	20	Yes	High	metated
SP	18			886	1	10.14 ct. staggered		You	High	bocted
SP VM		Cabbone Palmetto	Satal palmetto Velirha serm	855	1	18-18" gw		No.	High	
VW.	1.9	Wiein Palm	vanctus weron	le z p		110-10 gw	L	1-2	. agii	
RELOCATIONS	laty	COMMON NAME	BCTANICAL NAME	SPECIFICATIONS	CAL/DSH	навит	SPREAD	NATIVE	XERIC	REMARKS
SPR	12	Cobbaga Parredo	Sahal palmetto	NA NA		varies		Yes	Kah	RFLOCATED
arn	112	CONTRACTOR ARELEGES		lian.	L			1.55	-9-	
	Taran.	COMMON NAME	BOTANICAL NAME	SPECIFICATIONS	Lenacinic	Тнеснт	SPREAD	NATIVE	XERIC	REMARKS
SHRUBS	αïν				SPACING	HE GHT	30T	Yes	High	ful to base
¢147	155	Red To Cocopum	Chrysotalanus kaco "Red Tip"	NA		36"				Full to base
(SA)	823	Horszontal Cottophym	Chrysobalenus kaco "Horizontal"	NA	26	1=-	15-24"	Yes	High	
CDL	6	Green Tanz	Colorea esculanta	NA	As Shown	200	24"	No	Madrum	Full to base
coti	30	Black Wagn: Ti Plent	Cordylme termenate Black Magic	7 gas	As Shown	30"	24"	No	High	3 200
CRU	3	"Due on Emma Othum	Crinism augustrim "Queen Emma"	15 gsi	As Shown	45	45	No.	Medium	ļ
MON	39	Spite Lead Philodendron	Monsters de konsa	PEA	30"	47	30-36	No	High	full fig base
MYC	160	Whit Myrtie	Myrica cerifere	eza	301	19.	307	Yes	High	full to base
P00	175	Shrubby Yew	Podecarpus macrophyllus mald	NEA	24	24	24	No	Mudhro	267/24"
STN	5 "	White But of Paradise	Streitza nkolar	15 ga	As Shown	57 Ht	3.	No		Full Fan spread
SHRUG AREAS	CTY	COMMON NAME	BOTANICAL NAME	SPECIFICATIONS	SPACING	HEIGHT	SPREAD	NATIVE	XERIC	RÉMARKS
ALP	B3	Purple Gloger	Alpinos purpurata	n/a	30-	30-36"	24-30	No	Medium	l
BCG	235	Pager Fower	Scugalnydes glabra	n/a	24	16"	18	No	High	Full to base
cm	374	Snot-Leaf Clusio	Çausia gustifora		20-	24	24-30"	No	Medium	
DUR	1,133	Sky Flower	Duranta I spens "Dwref Golden"	n/a	116	16"	16-12"	Yes	High	!
HAP	494	Fire Bush	Magnata patent	n/a	124"	18-24"	18-24"	Yes	High	Full to buse
					30"	24	74-37	330	Medium	
IXD	430	leon Necs Grant	Ixora x "Nore Grant"	NO NO	24*	(200-24*	18"	No	Mediton	Full
JAG	874	Southern Wax assuring	Jasonnum simplefollum				14-16			1. *"
MUK	1,783	Pink Muhly Grass	Muhiembergia caretara	n/a	18"	18-24*		Yes	rigit	-
NEB	120	(Bastor Fen)	Nephrolepis exaltata "Bostoniensis"	Na	24°	18"	28"	You	High	<u> </u>
PHX	315	Philadendron	Philodenatron x xanadu	jp/a	24"	20	24	No	Medium	<u> </u>
PLU	595	Plumbago	Plumbago surkutata	r/a	24"	ier	18"	衪	Spiritum Municipal Municip	Soft blue flower
STR	35	Yellow Bird of Faradise	Straittela reginee "Gold Ciest" TM	p/a	30"	30-36"	24"	No.	⊬gh	Full to base
TRES	378	Felahatches Grass	Tripsacum dadylokies	ln z	25	30"	13"	YUE	Hgh	Full to base
IPS)			12-27	I and and the state of	Teorem:-	Longe	Lenners	Lucene -	Evenic	REMARKS
		COMMON NAME	BOTANICAL NAME	SPECIFICATIONS	SPACING	неснт	EPREAD	NATIVE		REMARKS
GROUND COVERS			Селіва распосагра	rv/a	1.5"	14-16"	14-16	No	Medini	
GROUND COVERS	602	Dwarf Natel Plum			14*	8-107	8-10	Yes	High	
GROUND COVERS		Cucumberlest Sunfower	Heisanthus debis	r/û						
GROUND COVERS	602		Helianthus debits lies vernteus "Schlittigs Dwerf	n/a n/a	18"	14-16*	14-16	Yes	(Hlgh	
GROUND COVERS CAM HED	502 241	Cucumberlest Sunfower					14-16* 18*	Yes No	High Madhini	
GROUND COVERS CAM HED LX JCA	602 241 1,140	Cucumberlesi Sunflower Dwarf Schillings Holly	lies vernte a 'Schillings Dwarf	nfa	18"	14-16*				
GROUND COVERS CAM HED ILX JCA PEO	502 241 1,140 157 121	Cucumberlest Sunflower Dwarf Schillings Holly Stee Pacific vierper	lles vervious "Schillings Dwerf Juniorus boniets "Bive Pacific"	n/a n/a	16"	14-16"	18"	No	MadNh	Dwarf, Full to
GROUND COVERS CAM HED LX JCA	502 241 1,140 127	Cucumberlest Sunflower Dwarf Schillings Holly Stee Pacific Jumper Baby Rubber Plant	lika verntoi a "Scallings Dwarf Junise us bokieta "Bue Pacce;" Peperana obtustola	nta nta nta	18" 16"	14-16" 14-16" 8-10"	18* 8-10*	No Yes	Madwhi High	Dread, Full &
GROUND COVERS CAM HED ILX JCA PEO	502 241 1,140 157 121	Cucumberlest Sunflower Dwarf Schillings Holly Stee Pacific Jumper Baby Rubber Plant	lika verntoi a "Scallings Dwarf Junise us bokieta "Bue Pacce;" Peperana obtustola	nta nta nta	18" 16" 14" 24"	14-16" 14-16" 8-10"	18" 8-10" (16"	No Yes	Madwhi High	Dwarf, Fuller

HOPE CONTROL TO US CUITON WIRE REPORTED CAR AND DUTTER SECTION AND TO DANGE PRETING THE REPORT OF THE APPROVED DATE MILE REPORTED TO THE APPROVED DATE MILE REPORTED TO THE APPROVED DATE MILE REPORTED TO THE APPROVED DATE MILE REPORTED.

MI II T	IPLE FAMILY, COMMERCIAL AND INDUSTRIAL	
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CITY C	F DELRAY BEACH	
561-243	7200	
Ä	TOTAL LOT AREA	457,253 00 S.F
ė	STRUCTURES, PARKING, WALKWAYS, DRIVES, ETC	336,430.07 S.F
Ċ	TOTAL PERVIOUS LOT AREA	170,822.93 S.F
D	AREA OF SHRUBS AND GROUND COVERS REQUIRED	36,247 S.F
Ë	AREA OF SHRUBS AND GROUND COVERS PROVIDED	36,991.09 S.F
F	NATIVE VEGETATION REQUIRED (25%)	3,082_S,F
G	NATIVE VEGETATION PROVIDED	21,040.54 S.F
В	TOTAL PAVED VERICULAR AREA	280,269,00 S.F
	TOTAL INTERIOR LANDSCAPE REQUIRED	28,027 S.F
	TOTAL INTERIOR LANDSCAPE PROVIDED	97,47E S.F
×	TOTAL INTERIOR SHADE TREES REQUIRED	224 TK
L	TOTAL INTERIOR SHADE TREES PROVIDED	225 TR
7.3	TOTAL LINEAR FEET SURROUNDING PARKING OR VEHICULAR USE AREAS	0.0S2 47 L.F
N	TOTAL NUMBER OF PERIMETER TREES REQUIRED	102 TR
o	TOTAL NUMBER OF PERIMETER TREES PROVIDED	102 JTR
P	TOTAL NUMBER OF EXISTING TREES TO BE SAVED ON SITE	20 TR
Ö	TOTAL NUMBER OF NATIVE TREES REQUIRED (50%)	t€3 TR
R	TOTAL NUMBER OF NATIVE TREES PROVIDED	244 TR

AUTONATION DELRAY BEACH AUTONATION MICHAEL D. GROSSWIRTH REGISTERED LANDSCAPE ARCHITECT
SCORIDA LORGIST HA CHITECT
SLORIDA LORGIST HA CHICATI
SLORIDA LORGIST HA CHICATI LANDSCAPE PLAN

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