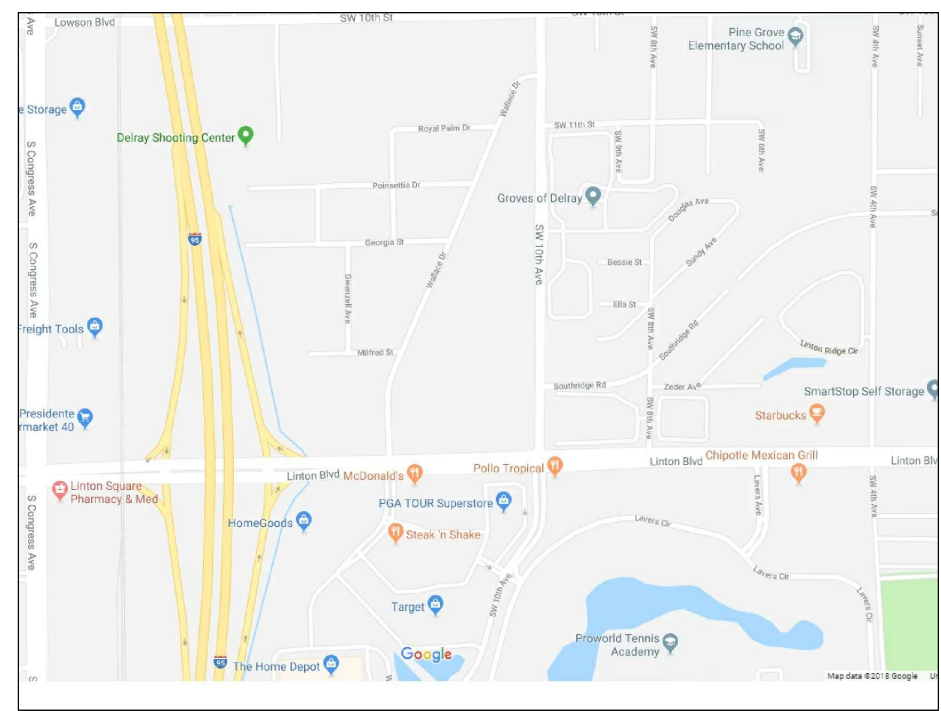


SEE SHEET 2 OF 2

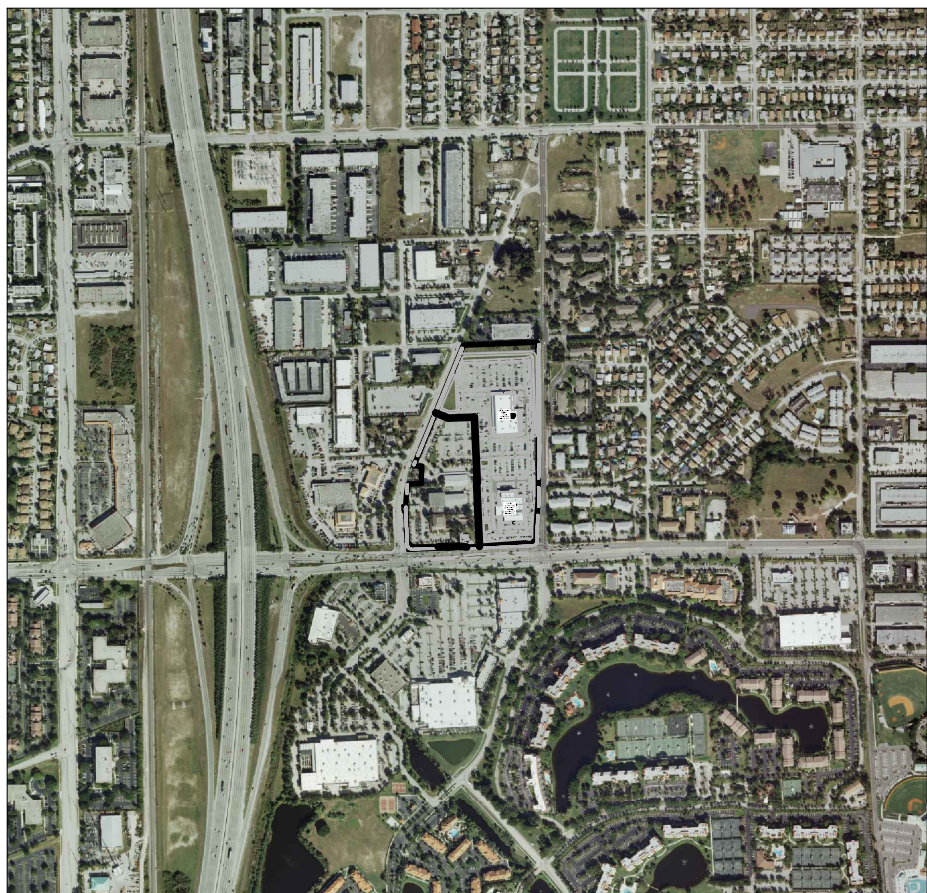
[illegible]

PARKING SPACE LEGEND

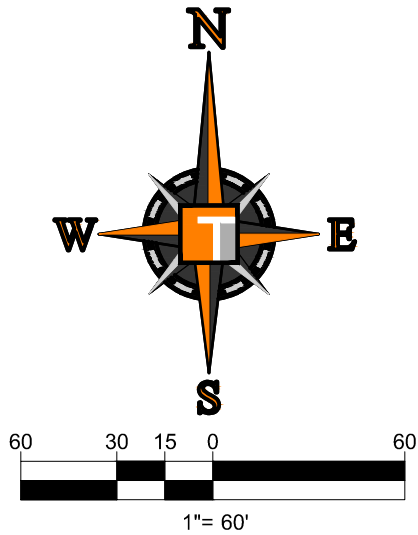
E = EMPLOYEES
I = INVENTORY
C = CUSTOMER
D = DISPLAY
S = SERVICE



LOCATION MAP
NOT TO SCALE



1/2 MILE RADIUS MAP
NOT TO SCALE



EAST PARCEL (MERCEDES BENZ)

ALL OF TRACT 'C', WALLACE DODGE, ACCORDING TO THE PLAT THEREOF, AS RECORDED IN PLAT BOOK 72, PAGES 54 AND 55 OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, TOGETHER WITH A PORTION OF TRACTS 'D' AND 'E', WALLACE DODGE REPLA, ACCORDING TO THE PLAT THEREOF, AS RECORDED IN PLAT BOOK 74, PAGES 9 AND 10, OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, TOGETHER WITH A PORTION OF PARCEL 'A', BILL WALLACE NISSAN, ACCORDING TO PLAT THEREOF AS RECORDED IN PLAT BOOK 56, PAGE 91, PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA BEING DESCRIBED AS FOLLOWS:

BEGIN AT THE THE NORTHEAST CORNER OF SAID TRACT 'C'; THENCE S01°13'44" E ALONG THE EAST LINE OF SAID TRACTS 'C' AND 'E', 150.00 FEET; THENCE S02°08'28" W ALONG THE EAST LINE OF SAID TRACTS 'E' AND 'D', 817.64 FEET; THENCE S06°20'07" W, 205.09 FEET; THENCE N08°06'12", 70.08 FEET (THE PREVIOUS CORNER BEING ALONG THE EAST LINE OF SAID TRACT 'A' TO THE NORTH RIVER); THENCE W01°00'00" E ALONG THE NORTH RIVER TO THE POINT OF BEGINNING; THENCE S89°42'15" E ALONG THE EAST LINE OF PALM BEACH COUNTY, FLORIDA; THENCE S51°12'28" W, 44.83 FEET; THENCE N79°23'39" W, 36.68 FEET; THENCE S88°07'54" W, 288.61 FEET (THE PREVIOUS THREE CALLS BEING ALONG THE SAID NORTH RIVER-OF-WAY OF LINTON BOULEVARD); THENCE N06°13'39" W, 406.86 FEET; THENCE N89°43'21" E, 5.36 FEET; THENCE N00°16'39" W, 409.82 FEET; THENCE N89°54'03" W, 155.99 FEET TO THE POINT OF CURVATURE OF A CURVE, CONCAVE TO THE NORTH, HAVING A RADIUS OF 50.67 FEET AND A CENTRAL ANGLE OF 24°26'37"; THENCE NORTHWESTERLY ALONG SAID ARC A DISTANCE OF 21.62 FEET TO A POINT OF TANGENCY; THENCE N05°27'26" W, 10.00 FEET; THENCE N00°00'00" E ALONG THE NORTH RIVER TO THE POINT OF BEGINNING; THENCE S89°42'15" E ALONG THE NORTH LINE OF SAID TRACT 'C', 466.92 FEET TO THE POINT OF BEGINNING.

WEST PARCEL (LAND ROVER / JAGUAR)

A PORTION OF PARCEL 'A', BILL WALLACE NISSAN, ACCORDING TO PLAT THEREOF AS RECORDED IN PLAT BOOK 56, PAGE 91, PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, TOGETHER WITH A PORTION OF TRACT 'D', WALLACE DODGE REPLY, ACCORDING TO THE PLAT THEREOF, AS RECORDED IN PLAT BOOK 74, PAGES 9 AND 10, OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, TOGETHER WITH A PORTION OF LOT 47, SANDS O'SEA, ACCORDING TO THE PLAT THEREOF, AS RECORDED IN PLAT BOOK 21, PAGE 27 OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA; TOGETHER WITH A PORTION OF THE SOUTH 25 FEET OF PRINCESS BOULEVARD, SANDS O'SEA, ACCORDING TO THE PLAT THEREOF AS RECORDED IN PLAT BOOK 21, PAGE 27, OF THE PUBLIC RECORDS OF PALM BEACH COUNTY, FLORIDA, BEING DESCRIBED AS FOLLOWS:

BEGIN AT THE NORTHWEST CORNER OF SAID PARCEL 'A'; THENCE N88°50'52"E ALONG THE NORTH LINE OF SAID PARCEL 'A', 85.14 FEET TO THE EASTERLY WEST LINE OF SAID TRACT 'D'; THENCE N00°09'08"W, 50.00 FEET; THENCE N06°13'52"E, 59.79 FEET (THE PREVIOUS TWO CALLS BEING ALONG THE EASTERLY WEST LINE OF SAID TRACT 'D'); THENCE N88°59'08"W, 51.60 FEET TO THE WEST LINE OF SAID TRACT 'D'; THENCE N24°45'29"E ALONG THE WEST LINE OF SAID TRACT 'D', 359.04 FEET; THENCE S65°27'26"E, 74.22 FEET TO THE POINT OF CURVATURE OF A CURVE, CONCAVE TO THE NORTH, HAVING A RADIUS OF 50.67 FEET AND A CENTRAL ANGLE OF 24°26'37"; THENCE SOUTHEASTERLY ALONG SAID ARC A DISTANCE OF 21.62 FEET TO A POINT OF TANGENCY; THENCE S89°54'03"E, 155.99 FEET; THENCE S00°16'39"E, 409.82 FEET; THENCE S89°43'21"W, 5.36 FEET; THENCE S00°16'39"E, 406.86 FEET TO THE NORTH RIGHT-OF-WAY LINE OF LINTON BOULEVARD, AS RECORDED IN OFFICIAL RECORDS BOOK 30076, PAGE 1959, OF THE PUBLIC RECORDS OF THE COUNTY OF PALM BEACH, FLORIDA; THENCE S00°16'39"E, 406.86 FEET ALONG THE NORTH RIGHT-OF-WAY LINE OF LINTON BOULEVARD, 39.94 FEET TO THE NORTH LINE OF THE SOUTH 30 FEET OF SAID LOT 47 OR THE RIGHT-OF-WAY OF LINTON BOULEVARD; THENCE S89°42'36"W ALONG THE NORTH LINE OF THE SOUTH 30 FEET OF SAID LOT 47 FOR THE RIGHT-OF-WAY OF LINTON BOULEVARD, 87.73 FEET; THENCE S89°26'12"W, 82.99 FEET; THENCE N78°03'37"W, 56.17 FEET; THENCE N88°53'57"W, 163.57 FEET; THENCE N44°15'11"W, 46.68 FEET (THE PREVIOUS FOUR CALLS BEING ALONG THE SAID NORTH RIGHT-OF-WAY LINE OF LINTON BOULEVARD PER OFFICIAL RECORDS BOOK 30076, PAGE 1959, PALM BEACH COUNTY RECORDS) TO THE WEST LINE OF SAID PARCEL 'A'; N00°17'24"W, 180.64 FEET TO THE POINT OF CURVATURE OF A CURVE, CONCAVE TO THE EAST HAVING A RADIUS OF 724.00 FEET AND A CENTRAL ANGLE OF 14°46'47"; THENCE NORTHERLY ALONG THE SAID ARC A DISTANCE OF 186.76 FEET (THE PREVIOUS TWO CALLS BEING ALONG THE WEST LINE OF SAID PARCEL 'A') TO THE POINT OF BEGINNING.

1. THIS PLAN REFERENCES AN ALTA/ACSM LAND TITLE SURVEY BY

AVIROM & ASSOCIATES, INC.
50 SW 2ND AVENUE, SUITE 102, BOCA RATON, FL 33432
TELEPHONE: (561) 392-2594 FAX: (561) 394-7125

2. DATA TABLE:

CURRENT USE:	NEW CAR SALES & SERVICE
PROPOSED USE:	NEW CAR SALES & SERVICE
FUTURE LAND USE DESIGNATION:	GC (GENERAL COMMERCIAL)
ZONING DESIGNATION:	AG (AUTOMOTIVE COMMERCIAL)
BUILDING TYPE OF CONSTRUCTION:	IIB - FULLY SPRINKLED
BUILDING OCCUPANCY:	BUSINESS GROUP B - MOTOR VEHICLE SHOWROOM
	STORAGE GROUP S1 - MOTOR VEHICLE REPAIR
WATER/WASTEWATER SERVICE PROVIDER:	CITY OF DELRAY BEACH

	REQUIREMENT	EXISTING MERCEDES	PROPOSED LAND ROVER JAGUAR
FRONTAGE (MINIMUM)	125'	361'	455.74'
WIDTH (MINIMUM)	125'	361'	455.74'
DEPTH (MINIMUM)	200'	1,272.93'	818.13'
AREA (MINIMUM)	1.5 ACRES	13.044 ACRES (568,198)	7.367 ACRES (319,620)
LOT COVERAGE	NONE	11.7% (66,380 S.F)	14.6% (46,551 SF)
OPEN SPACE (MINIMUM)	25%	25.8%	29.5%
HEIGHT (MAXIMUM)	48'	25'-0"	26' -0"
SETBACK (MAXIMUM):			
FRONT	15'	136.83'	93.42'
SIDE STREET	15'	79.40'	77.43'
REAR	10'	319.83'	465.57'
SIDE INTERIOR	15'	117.93'	126.68'

SITE DATA (LAND ROVER / JAGUAR)			ZONING DISTRICT: AC	
NET SITE AREA			319,620 SF	7.34 ACRES
SITE AREA CALCULATIONS			SF	% OF NET LAND AREA
PERVIOUS AREA (LANDSCAPED + OPEN SPACE)			82,380 SF	25.8
IMPERVIOUS AREA (PAVEMENT + BLDG. FOOTPRINT / SIDEWALKS)			237,240 SF	74.2
BUILDING AREA	PROPOSED SHOWROOM & SERVICE BLDG. (GROUND FLOOR + MEZZANINE)		59,214 SF	-
	TOTAL		59,214 SF	-
LANDSCAPE OPEN SPACE	REQUIRED = 25% OF SITE AREA (319,620)		79,905 SF	25.0
	PROVIDED	INTERIOR	34,686 SF	10.9
		PERIMETER BUFFER	47,694 SF	14.9
	TOTAL		82,380 SF	25.8
	BULLPEN LANDSCAPE REQUIRED; 10% OF BULLPEN PAVED AREA (75,852 SF) TRANSFERRED TO PERIMETER BUFFER		0 SF	0.0
GROUND COVERAGE	BUILDING	PROPOSED SHOWROOM & SERVICE	46,468 SF	14.5
		TOTAL BUILDING COVERAGE	46,468 SF	14.5
	LANDSCAPE		82,380 SF	25.8
	PAVEMENT		190,772 SF	59.7
	TOTAL		319,620 SF	100.0

SITE DATA (MERCEDES)			ZONING DISTRICT: AC	
NET SITE AREA			568,198 SF	13.04 ACRES
SITE AREA CALCULATIONS			SF	% OF NET LAND AREA
PERVIOUS AREA (LANDSCAPED + OPEN SPACE)			167,788 SF	29.5
IMPERVIOUS AREA (PAVEMENT + BLDG. FOOTPRINT / SIDEWALKS)			400,410 SF	70.5
BUILDING AREA	EXISTING BUILDINGS TO REMAIN (GROUND FLOOR + MEZZANINE)		72,318 SF	-
	TOTAL		72,318 SF	-
LANDSCAPE OPEN SPACE	REQUIRED = 25% OF SITE AREA (567,991)		142,050 SF	25.0
	PROVIDED	INTERIOR	147,601 SF	25.9
		PERIMETER BUFFER	20,187 SF	3.6
	TOTAL		167,788 SF	29.5
	BULLPEN LANDSCAPE REQUIRED: 10% OF BULLPEN PAVED AREA (143,970 S.F) TRANSFERRED TO PERIMETER BUFFER		0 SF	0.0
GROUND COVERAGE	BUILDING	EXISTING BLDG. TO REMAIN	66,380 SF	11.7
		TOTAL BUILDING COVERAGE	66,380 SF	11.7
	LANDSCAPE		167,788 SF	29.5
	PAVEMENT		334,358 SF	58.8
	TOTAL		568,198 SF	100.0

LAND ROVER JAGUAR REQUIRED PARKING (59,214 SF GROSS FLOOR AREA)						
	SQUARE FEET	BAYS	CALCULATION	REQUIRED	PROVIDED	SYMBOL
TOTAL PARKING: GROSS FLOOR AREA (LESS 7,050 SF OF INDOOR DISPLAY)	52,164	-----	4 SPACES / 1,000 SF	209	525	
REQUIRED PARKING DESIGNATION FOR EMPLOYEE, CUSTOMER AND/OR SERVICE USE						
GROSS FLOOR AREA (LESS 7,050 SF OF INDOOR DISPLAY)	52,164	-----	2 SPACES / 1,000 SF	104	70*	C
					34*	E
SERVICE BAYS (INCLUDES DETAILING)	-----	28	1.5 SPACES / SERVICE BAY	42	42	S
REMAINING (MAY BE USED FOR DISPLAY/INVENTORY)	-----	-----		63	63	D
					316	I

* ASSUME 1/3 OF REQUIRED FOR EMPLOYEE SPACES

LAND ROVER JAGUAR REQUIRED PARKING (72,318 SF GROSS FLOOR AREA)						
	SQARE FEET	BAYS	CALCULATION	REQUIRED	PROVIDED	SYMBOL
TOTAL PARKING: GROSS FLOOR AREA (LESS 8,649 SF OF INDOOR DISPLAY)	63,669	-----	4 SPACES / 1,000 SF	255	862	
REQUIRED PARKING DESIGNATION FOR EMPLOYEE, CUSTOMER AND/OR SERVICE USE						
GROSS FLOOR AREA (LESS 8,649 SF OF INDOOR DISPLAY)	63,669	-----	2 SPACES / 1,000 SF	127	85*	C
					42*	E
SERVICE BAYS (INCLUDES DETAILING)	-----	51	1.5 SPACES / SERVICE BAY	77	77	S
REMAINING (MAY BE USED FOR DISPLAY/INVENTORY)	-----	-----		51	64	D
					594	I



6300 NW 31ST AVENUE
FT. LAUDERDALE, FL 33309
P: 954-202-7000

4950 W. KENNEDY BLVD.
SUITE 600
TAMPA, FL 33609
P: 813-379-1100

125 W INDIANTOWN RD.
SUITE 206
JUPITER, FL 33458
P: 561-203-7503

THE INFORMATION, DESIGN AND CONTENT OF THIS PLAN ARE PROPRIETARY AND SHALL NOT BE COPIED OR USED FOR ANY PURPOSES WITHOUT PRIOR WRITTEN AUTHORIZATION FROM HOWES ENGINEERING GROUP, LLC. ONLY APPROVED, SIGNED AND SEALED PLANS SHALL BE VALID FOR CONSTRUCTION.

[illegible]

**KNOW WHAT'S BELOW
ALWAYS CALL 811
BEFORE YOU DIG**

It's fast. It's free. It's the law.

www.callsunshine.com

SPRAB SUBMITTAL

PROJECT No.: F170121
DRAWN BY: CAD
CHECKED BY: MAT
DATE: 08-16-19
CAD I.D.: F170121-SITE PLAN OVERALL

PROJECT:

**LAND ROVER/ JAGUAR
OF DELRAY BEACH
1111 W. LINTON BLVD.
DELRAY BEACH**

— FOR —
AUTONATION

DELRAY BEACH
FLORIDA



6300 NW 31ST AVENUE
FORT LAUDERDALE, FL 33309
PH: (954) 202-7000
FX: (954) 202-7070
www.ThomasEngineeringGroup.com

MICHAEL A. TRÖXELL

PROFESSIONAL ENGINEER
August 16, 2019
FLORIDA LICENSE No. 50572
FLORIDA BUSINESS CERT. OF AUTH. No. 27528

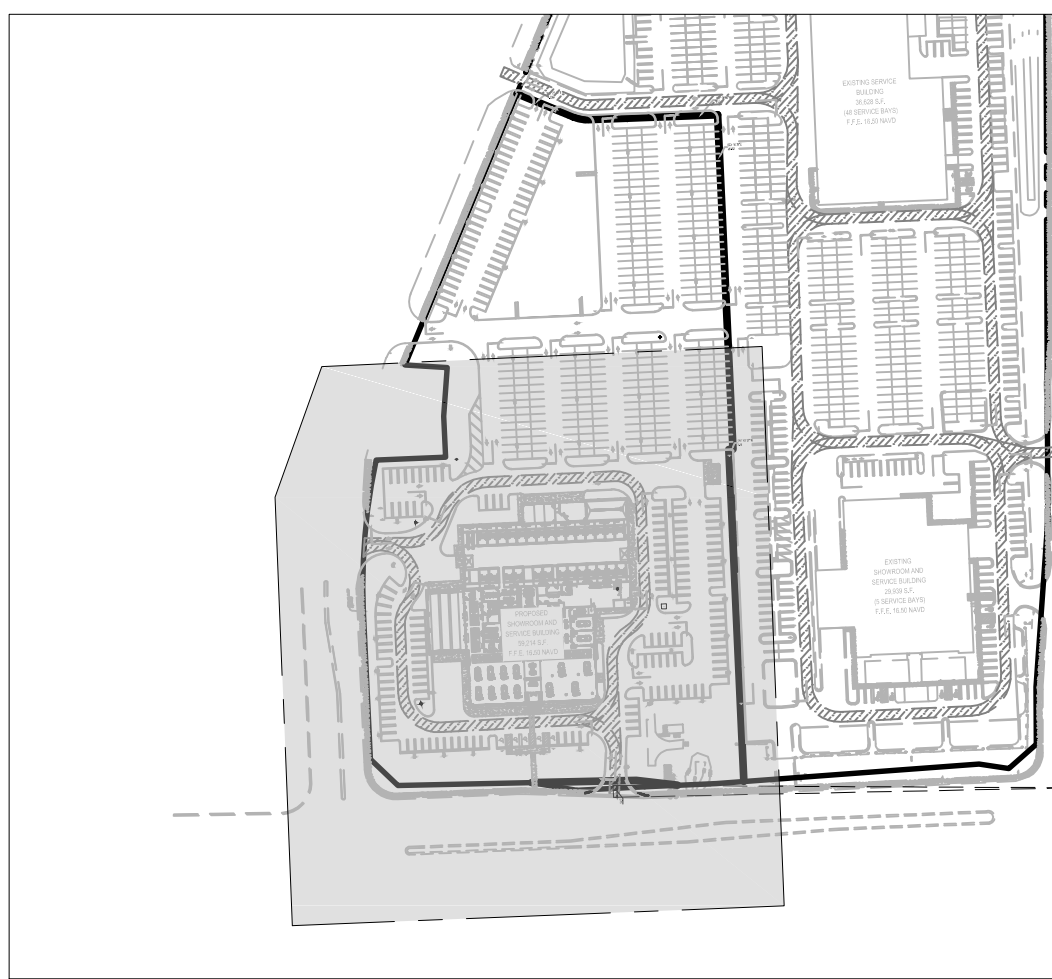
SHEET TITLE

OVERALL SITE PLAN

SHEET NUMBER

SP-01

MATCH LINE SEE SHEET SP-03



KEY

NOTE: CONTRACTOR TO USE CAUTION WHILE REMOVING CURB AND GUTTER SECTION NOT TO DAMAGE EXISTING ASPHALTIC CONCRETE PAVEMENT, OTHERWISE MILLING AND RESURFACING OF THE AFFECTED LANE WILL BE REQUIRED.

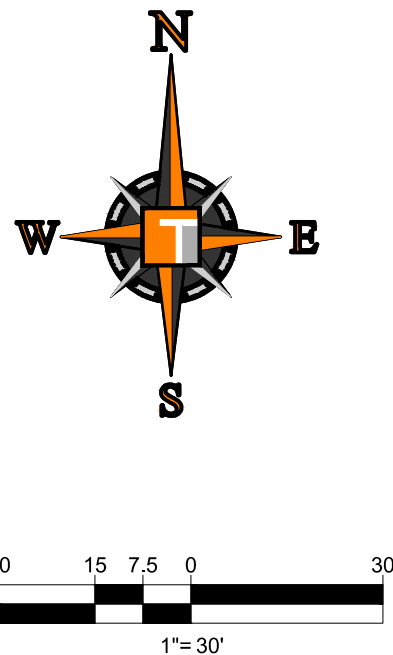
ELEVATIONS ARE BASED ON NORTH AMERICAN VERTICAL DATUM (NAVD) OF 1988.

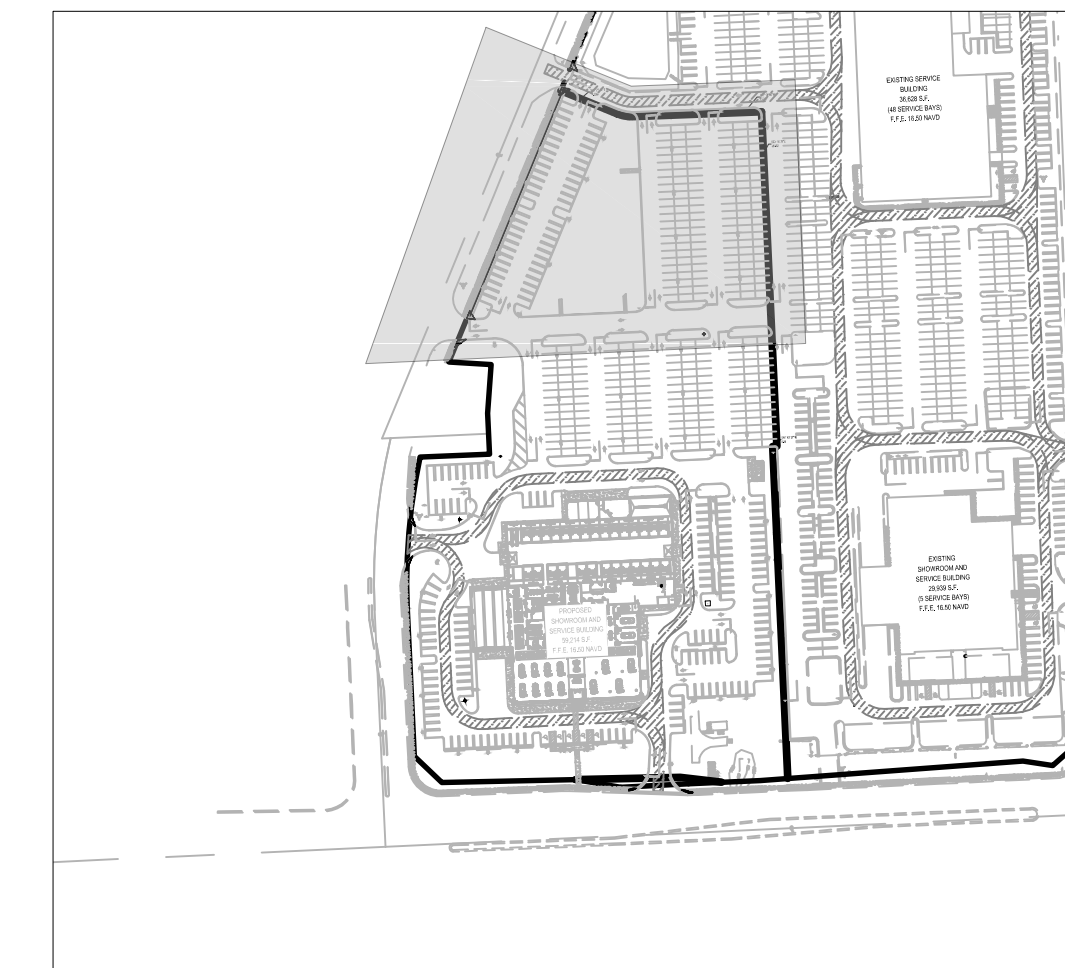
LEGEND

- PROPOSED LIGHT POLE
- E = EMPLOYEES
- I = INVENTORY
- C = CUSTOMER
- D = DISPLAY
- S = SERVICE

HATCH LEGEND

- PROPOSED CONCRETE
- EXISTING CONCRETE





PROP. LIGHT
POLE

<□□> <□>

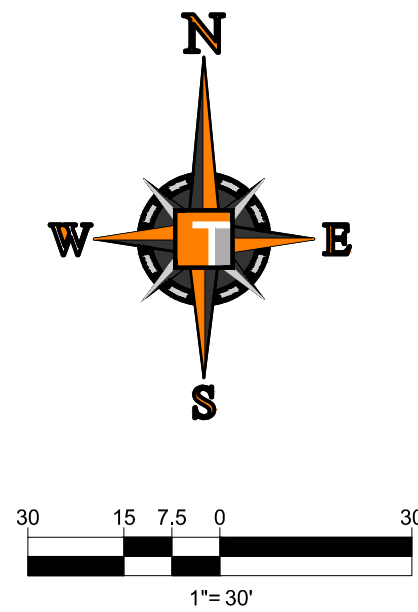
E = EMPLOYEES
I = INVENTORY
C = CUSTOMER / EMPLOYEE SPACE
D = DISPLAY
S = SERVICE

SHEET TITLE:

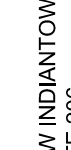
**LANDROVER JAGUAR
SITE PLAN**

SHEET NUMBER:

SP-03

DEMOLITION NOTES:

1. THOMAS ENGINEERING GROUP, LLC IS NOT RESPONSIBLE FOR JOB SITE SAFETY OR SUPERVISION.
2. ALL DEMOLITION ACTIVITIES ARE TO BE PERFORMED IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS AS WELL AS ALL FEDERAL, STATE AND LOCAL REGULATIONS. ANY DISCREPANCIES OR DEVIATIONS SHALL BE IDENTIFIED BY THE CONTRACTOR TO THOMAS ENGINEERING GROUP, LLC IN WRITING FOR RESOLUTION PRIOR TO INITIATION OF SITE ACTIVITY.
3. PRIOR TO STARTING ANY DEMOLITION CONTRACTOR IS RESPONSIBLE FOR:OL>
 - A. THE CONTRACTOR SHALL OBTAIN A SUNSHINE STATE ONE CALL LOCATION CERTIFICATION PRIOR TO ANY EXCAVATION OR DEMOLITION. THE NUMBER IS 1-800-432-4770.
 - B. ENSURING COPIES OF ALL PERMITS AND APPROVALS MUST BE MAINTAINED ON SITE AND AVAILABLE FOR REVIEW.
 - C. INSTALLING THE REQUIRED SOIL EROSION AND SEDIMENT CONTROL MEASURES PRIOR TO SITE DISTURBANCE.
 - D. LOCATE/CAP ALL UTILITIES AND SERVICES, INCLUDING BUT NOT LIMITED TO GAS, WATER, ELECTRIC, SANITARY AND STORM SEWER, TELEPHONE, CABLE, FIBER OPTIC CABLE, ETC. WITHIN THE LIMITS OF DISTURBANCE.
 - E. PROTECTING AND MAINTAINING IN OPERATION, ALL ACTIVE SYSTEM THAT ARE NOT BEING REMOVED DURING ALL DEMOLITION ACTIVITIES.
 - F. FAMILIARIZING THEMSELVES WITH THE APPLICABLE UTILITY SERVICE PROVIDER AND IS RESPONSIBLE FOR ALL COORDINATION REGARDING UTILITY DEMOLITION REQUIRED FOR THE PROJECT. THE CONTRACTOR SHALL PROVIDE THE OWNER WRITTEN NOTIFICATION THAT THE EXISTING UTILITIES AND SERVICES HAVE BEEN TERMINATED AND ABANDONED IN ACCORDANCE WITH JURISDICTION AND UTILITY COMPANY REQUIREMENTS.
 - G. COORDINATION WITH UTILITY COMPANIES REGARDING WORKING "OFF-PEAK" HOURS OR ON WEEKENDS AS MAY BE REQUIRED TO MINIMIZE THE IMPACT ON THE AFFECTED PARTIES.
 - H. A COMPLETE INSPECTION OF CONTAMINANTS BY A LICENSED ENVIRONMENTAL TESTING AGENCY, OF ALL BUILDINGS AND/OR STRUCTURES TO BE REMOVED. SAME SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE LOCAL, STATE, AND FEDERAL ENVIRONMENTAL REGULATIONS. ANY/ALL CONTAMINANTS SHALL BE REMOVED AND DISPOSED OF BY A FEDERALLY LICENSED CONTRACTOR IN ACCORDANCE WITH FEDERAL, STATE, AND LOCAL REGULATIONS, ALL ENVIRONMENTAL WORK INCLUDING HAZARDOUS MATERIAL, SOILS, ASBESTOS, OR OTHER REFERENCED OR IMPLIED HEREIN IS THE SOLE RESPONSIBILITY OF THE OWNER'S ENVIRONMENTAL CONSULTANT.
4. THE FIRM OR ENGINEER OF RECORD IS NOT RESPONSIBLE FOR JOB SITE SAFETY OR SUPERVISION. CONTRACTOR IS TO PROCEED WITH THE DEMOLITION IN A SYSTEMATIC AND SAFE MANNER, FOLLOWING ALL THE OSHA REQUIREMENTS, TO ENSURE PUBLIC AND CONTRACTOR SAFETY.
5. THE CONTRACTOR SHALL PROVIDE ALL THE "MEANS AND METHODS" NECESSARY TO PREVENT MOVEMENT, SETTLEMENT, OR COLLAPSE OF EXISTING STRUCTURES, AND ANY OTHER IMPROVEMENTS THAT ARE REMAINING ON OR OFF SITE. THE DEMOLITION CONTRACTOR IS RESPONSIBLE FOR ALL REPAIRS OF DAMAGE TO ALL ITEMS THAT ARE TO REMAIN AS A RESULT OF HIS ACTIVITIES. ALL REPAIRS SHALL USE NEW MATERIAL. THE REPAIRS SHALL RESTORE THE ITEM TO THE PRE-DEMOLITION CONDITION.
6. IN THE ABSENCE OF SPECIFICATIONS, THE CONTRACTOR SHALL PERFORM EARTH MOVEMENT ACTIVITIES, DEMOLITION AND REMOVAL OF ALL FOUNDATION WALLS, FOOTINGS, AND OTHER MATERIALS WITHIN THE LIMITS OF DISTURBANCE IN ACCORDANCE WITH DIRECTION BY OWNER'S STRUCTURAL OR GEOTECHNICAL ENGINEER.
7. EXPLOSIVES SHALL NOT BE USED WITHOUT PRIOR WRITTEN CONSENT OF BOTH THE OWNER AND APPLICABLE GOVERNMENTAL AUTHORITIES. ALL THE REQUIRED PERMITS AND EXPLOSIVE CONTROL MEASURES THAT ARE REQUIRED BY THE FEDERAL, STATE, AND LOCAL GOVERNMENTS SHALL BE IN PLACE PRIOR TO STARTING AN EXPLOSIVE PROGRAM. THE CONTRACTOR IS ALSO RESPONSIBLE FOR ALL INSPECTION AND SEISMIC VIBRATION TESTING THAT IS REQUIRED TO MONITOR THE EFFECTS ON ALL LOCAL STRUCTURES.
8. CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL AND GENERALLY ACCEPTED SAFE PRACTICES IN CONFORMANCE WITH: THE "MANUAL ON UNIFORM TRAFFIC CONTROL," AS WELL AS FEDERAL, STATE, AND LOCAL REGULATIONS WHEN DEMOLITION RELATED ACTIVITIES IMPACT ROADWAYS OR ROADWAY RIGHTS - OF - WAY.
9. CONDUCT DEMOLITION ACTIVITIES IN SUCH A MANNER TO ENSURE MINIMUM INTERFERENCE WITH ROADS, STREETS, SIDEWALKS, WALKWAYS, AND OTHER ADJACENT FACILITIES. STREET CLOSURE PERMITS MUST BE RECEIVED FROM THE APPROPRIATE GOVERNMENTAL AUTHORITY.
10. DEMOLITION ACTIVITIES AND EQUIPMENT SHALL NOT USE AREAS OUTSIDE THE DEFINED PROPERTY LINE WITHOUT WRITTEN PERMISSION OF THE OWNER AND/OR APPROPRIATE GOVERNMENT AGENCY.
11. USE DUST CONTROL MEASURES TO LIMIT AIRBORNE DUST AND DIRT RISING AND SCATTERING IN THE AIR IN ACCORDANCE WITH FEDERAL, STATE, AND/OR LOCAL STANDARDS. AFTER THE DEMOLITION IS COMPLETE, ADJACENT STRUCTURES AND IMPROVEMENTS SHALL BE CLEANED OF ALL DUST AND DEBRIS CAUSED BY THE DEMOLITION OPERATIONS. THE CONTRACTOR IS RESPONSIBLE FOR RETURNING ALL ADJACENT AREAS TO THEIR "PRE-DEMOLITION" CONDITION.
12. CONTRACTOR IS RESPONSIBLE TO SAFEGUARD SITE AS NECESSARY TO PERFORM THE DEMOLITION IN SUCH A MANNER AS TO PREVENT THE ENTRY OF UNAUTHORIZED PERSONS AT ANY TIME.
13. THIS DEMOLITION PLAN IS INTENDED TO IDENTIFY THOSE EXISTING ITEMS/CONDITIONS WHICH ARE TO BE REMOVED. IT IS NOT INTENDED TO PROVIDE DIRECTION OTHER THAN THAT ALL METHODS AND MEANS ARE TO BE IN ACCORDANCE WITH STATE, FEDERAL, LOCAL, AND JURISDICTIONAL REQUIREMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL OSHA AND OTHER SAFETY PRECAUTIONS NECESSARY TO PROVIDE A SAFE WORK SITE.
14. DEBRIS SHALL NOT BE BURIED ON THE SUBJECT SITE. ALL DEMOLITION WASTES AND DEBRIS (SOLID WASTE) SHALL BE DISPOSED OF IN ACCORDANCE WITH ALL CITY, COUNTY, STATE, AND FEDERAL LAWS AND APPLICABLE CODES.
15. ALL WATER MAINS TO BE ABANDONED IN PLACE WILL BE REMOVED WATER AND SEWER SERVICE LINES TO BE ABANDONED WILL NEED TO BE REMOVED BACK TO WATER MAIN. IN ACCORDANCE WITH APPLICABLE UTILITY STANDARDS.
16. CONTRACTOR TO REMOVE/DISCONNECT ALL UTILITIES SERVING BUILDINGS PRIOR TO DEMOLITION.
17. ANY EXISTING SITE IMPROVEMENTS NOT SHOWN TO REMAIN ON THE PROPOSED PLANS SHALL BE REMOVED UNLESS OTHERWISE NOTED.



ENGINEERING GROUP

CIVIL ENGINEERS • PROJECT MANAGERS • LAND PLANNING • LANDSCAPE ARCHITECTS

6300 NW 31ST AVENUE

FT. LAUDERDALE, FL 33309

P. 954-262-7000

SUITE 600

TAMPA, FL 33609

P. 813-574-1100

4850 W. KENNEDY BLVD.

SUITE 206

JUPITER, FL 33488

P. 561-743-7505

125 W. INDIANTOWN RD.

SUITE 208

JUPITER, FL 33488

P. 561-743-7505

THE ASSOCIATED ENGINEERS OF H. O. MAS & ASSOCIATES, INC. ARE LICENSED PROFESSIONAL ENGINEERS AND ARCHITECTS IN THE STATES OF FLORIDA AND ALABAMA. THE ASSOCIATED ENGINEERS OF H. O. MAS & ASSOCIATES, INC. ARE LICENSED PROFESSIONAL ENGINEERS AND ARCHITECTS IN THE STATES OF FLORIDA AND ALABAMA. THE ASSOCIATED ENGINEERS OF H. O. MAS & ASSOCIATES, INC. ARE LICENSED PROFESSIONAL ENGINEERS AND ARCHITECTS IN THE STATES OF FLORIDA AND ALABAMA.

[illegible]

KNOW WHAT'S BELOW
ALWAYS CALL 811
BEFORE YOU DIG

It's fast. It's free. It's the law.

www.callsunshine.com

SPRAB SUBMITTAL

PROJECT No.:	F170121
DRAWN BY:	CAD
CHECKED BY:	MAT
DATE:	08-16-19
CAD I.D.:	F170121-DEMO PLAN

PROJECT:

**LAND ROVER/ JAGUAR
OF DELRAY BEACH
1111 W. LINTON BLVD.
DELRAY BEACH**

— FOR —
AUTONATION



6300 NW 31ST AVENUE
FORT LAUDERDALE, FL 33309
PH: (954) 202-7000
FX: (954) 202-7070
www.ThomasEngineeringGroup.com

MICHAEL A. TROXELL

PROFESSIONAL ENGINEER
August 16, 2019
FLORIDA LICENSE No. 50572
FLORIDA BUSINESS CERT. OF AUTH. No. 27528

SHEET TITLE:

DEMOLITION PLAN

SHEET NUMBER:

CS-01

NOTE:
CONTRACTOR TO USE CAUTION WHILE REMOVING CURB
AND GUTTER SECTION NOT TO DAMAGE EXISTING
ASPHALTIC CONCRETE PAVEMENT; OTHERWISE MILLING
AND RESURFACING OF THE AFFECTED LANE WILL BE
REQUIRED

ELEVATIONS ARE BASED ON
NORTH AMERICAN VERTICAL
DATUM (NAVD) OF 1988.

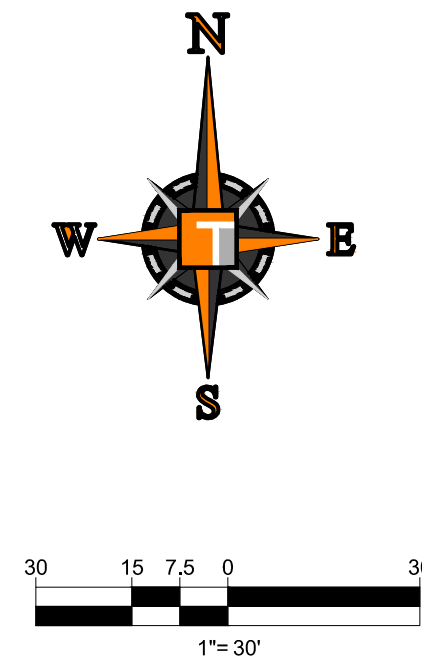
NOTE:

1. CONTRACTOR TO USE CAUTION WHILE REMOVING CURB AND GUTTER SECTION NOT TO DAMAGE EXISTING ASPHALTIC CONCRETE PAVEMENT, OTHERWISE MILLING AND RESURFACING OF THE AFFECTED LANE WILL BE REQUIRED.
2. ALL WATER MAINS TO BE ABANDONED IN PLACE WILL BE REMOVED SERVICE LINES WILL NEED TO BE REMOVED BACK TO THE MAIN.
3. ALL EXISTING WATER METERS NEED TO BE DISCONNECTED AT THE MAIN AND CORPORATION STOP.

HATCH LEGEND

Diagram illustrating existing pavement and concrete areas:


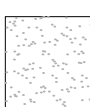
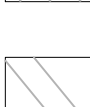
- EXISTING PAVEMENT (TO BE REMOVED)
- EXISTING CONCRETE (TO REMAIN)



NOTE:
CONTRACTOR TO USE CAUTION WHILE REMOVING CURB
AND GUTTER SECTION NOT TO DAMAGE EXISTING
ASPHALTIC CONCRETE PAVEMENT, OTHERWISE MILLING
AND RESURFACING OF THE AFFECTED LANE WILL BE
REQUIRED.

ELEVATIONS ARE BASED ON
NORTH AMERICAN VERTICAL
DATUM (NAVD) OF 1988.

HATCH LEGEND

	EXISTING PAVEMENT (TO BE REMOVED)
	EXISTING CONCRETE (TO REMAIN)
	EXISTING PAYMENT (TO BE MILLED AND RESURFACED)

- NOTE:**
1. CONTRACTOR TO USE CAUTION WHILE REMOVING CURB AND GUTTER SECTION NOT TO DAMAGE EXISTING ASPHALTIC CONCRETE PAVEMENT, OTHERWISE MILLING AND RESURFACING OF THE AFFECTED LANE WILL BE REQUIRED.
 2. ALL WATER MAINS TO BE ABANDONED IN PLACE WILL BE REMOVED SERVICE LINES WILL NEED TO BE REMOVED BACK TO THE MAIN.
 3. ALL EXISTING WATER METERS NEED TO BE DISCONNECTED AT THE MAIN AND CORPORATION STOP.